



## THURSTON COUNTY COURTHOUSE & CIVIC CENTER | FINAL REPORT

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# XVI.

THURSTON COUNTY COURTHOUSE FINAL REPORT

## EXECUTIVE SUMMARY

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# **Thurston County Courthouse Comprehensive Comparative Feasibility Study**

December 6, 2018

## **EXECUTIVE SUMMARY**

Since the project start in early September, the Thomas Architecture Studios (TAS)/HOK Team has reviewed previous studies; interviewed both Thurston County, Judiciary and City of Olympia stakeholders; held meetings with the Citizen's Advisory Group and held Open House sessions with the community throughout the County.

We have updated the space program to reflect changes that occurred since the 2015 study was completed. Future space projections were revised based on these changes plus the goal to project thirty years ahead, moving the target date from 2045 to 2050. Thus, construction approved in a 2019 referendum would provide the space needs through the term of the bond.

The program is organized to reflect building location. The courthouse includes the lobby and security screening, Superior and District Courts, Clerk, Pre-trial Services, the Sheriff's court security operation and building support. The Civic Center includes court-related offices for the Prosecutor, Office of Public Defense, Drug Court and the County offices, including: County Administration, Assessor, Auditor, Treasurer, Environmental Health, Development Review, Community Planning, General Information Services, Human Resources and Central Services. The Sheriff's Administration and Field Operations Bureau is listed separately for planning and funding purposes but ultimately, it may not be a part of the Thurston County Courthouse and Civic Center Complex.

The Olympia Justice Center is also listed separately. It includes the departments currently located in the Creighton Justice Center. This includes the Municipal Court, City Prosecutor, Community Court, Probation Services, Public Defense Coordinator, Police Holding facility and building support.

The full Program Space Summary and all supporting documentation can be found Section V of this report.

## **OPTIONS**

The County pre-selected the following three sites for further analysis:

- A. Hilltop – Existing site of Thurston County government and the courts. This site includes Buildings 1-6. Buildings 1-3 were constructed in 1978 and are original to the complex. Over the past 40 years, the County has purchased Buildings 4-6 which are located across the street to the west of the original site.
- B. Plum Street – Formerly the original site of Olympia City Hall and currently the location of the City of Olympia's Creighton Justice Center. This site contains the Municipal Courts, related departments and the police booking facility. The one-story Justice Center sits on the northwest corner of the parcel. The Yashiro Japanese Garden, a 0.72-acre park dedicated to the Olympia's sister city of Yashiro, Japan, is located on the southwest corner of the site.
- C. Harrison West – A currently undeveloped parcel in a rapidly developing area on the west side of the city. This site is zoned for multi-use, high density development.



Multiple options were developed for each site. The options test their suitability for the new Courthouse & Civic Center and do not represent a final solution.

## **A. HILLTOP**

Five different options were developed for the Hilltop site. This is a complex site with steep slopes around the hilltop, substantial grade change and limited developable site area and access. Maintaining existing operations during construction requires phased development. Each Option offers different implementation phasing and planning opportunities.

### **OPTION A1**

This option utilizes both the original Hilltop Campus and the block immediate to the southwest. It is the only option not concentrating all construction on the original site. The courthouse will include both the Superior and District Court and a 600-space parking structure will be constructed on the block to the southwest. The courthouse is six-stories tall with a lower level for vehicle sallyport, central holding and secure parking.

The original campus will be dedicated to County Administrative and Court-Related offices. Additional structured parking will be available on the site of Building 3 which currently houses the District Court, Sheriff's Administration and some probation offices. Buildings 1 and 2 can then be renovated or replaced at the County's discretion.

### **OPTION A2**

Option A2 constructs the new courthouse and a parking structure on the site of Building 3. When the courts are relocated from Building 2 it can be demolished for a new County Administration Building. The two buildings will be joined by a common lobby. Building 1 can then be demolished and the north part of the Hilltop site redeveloped for parking and public space.

### **OPTION A3**

The goal of this option is to construct the courthouse, administrative offices and lobby together in one phase. It sits on the existing surface parking on the north portion of the Hilltop site and curves around Building 1. It provides dramatic views from the new building and creates a visible landmark visible from the isthmus and Capitol. Parking is divided into two structures, one that can be constructed on the footprint of Building 3 and the other on the footprint of Building 2 after the courts move into the new structure. Building 1 can be demolished after construction and retained as plaza and a site for future expansion. Existing Buildings 4 thru 6 can be sold.

### **OPTION A4**

Option 4 constructs a six-story tower on the green space between Buildings 1 and 2. It is taller than the other schemes to compensate for a smaller footprint. Parking will be provided by a parking structure on the site of Building 3. This scheme offers dramatic views from the new building. Once the courts move into the new structure Buildings 2 and 3 can be renovated or new buildings constructed in their place. Existing Buildings 4 thru 6 can be sold.

### **OPTION A5**

The Courthouse and Administration building in this scheme are placed to focus the view toward Budd Bay. Located on the north end of the Hilltop site, the Administration building would be built in phase one on the site of existing surface parking. Once Building 1 is emptied, it would be demolished, and the new Courthouse would be constructed over its footprint. Parking structures would be phased on the sites of Buildings 2 and 3. Together the buildings form a dramatic public plaza. The scheme includes a two-level plaza with a vehicle drop off, lobby with views of the bay and connection to the parking structures on one level. Above would be an open public walk leading between the buildings to an overlook of the bay.



## **B. PLUM STREET**

Two options were developed for the west side of the Plum Street site. Both schemes use the existing city grounds maintenance yard for construction of a new parking structure. These schemes are approximately 45,000 SF larger to incorporate the City of Olympia's Justice Center requirements into a unified facility.

### **OPTION B1**

This option locates the new courthouse and the administrative office building along the east side of Plum Street. The buildings are at least 40 feet east of the west property line providing a pleasant walk and landscaping while maintaining secure vehicle stand-off distance.

Municipal Court functions integrated onto the first floor of the courthouse. There is a shared lobby for security screening and below grade secure parking and central holding. New site development is based on the Japanese Garden with cherry trees lining the entry drive and a new contemporary garden lining the walkway between the parking structure and the lobby. The existing Hilltop site can be sold to recover cost of new Courthouse & Civic Center.

### **OPTION B2**

This scheme constructs the courthouse and office buildings parallel to each other on the northwest quadrant of the site forming a secure, ground level courtyards. Functionally the scheme is similar to Option B1 except construction is concentrated in the northwest corner of the site creating an internal private garden and retaining the existing Yashiro Japanese Garden.

## **C. HARRISON WEST**

Two options were developed for the Harrison West site. Both schemes incorporate mixed use development incorporating both civic and commercial structures providing complimentary services and sharing development costs. City streets will be extended through the site from both Harrison Avenue and Kaiser Road per Olympia requirements. The schemes vary by the size and quantity of development. The site is currently vacant allowing the County Courthouse and Civic Center to be constructed in one phase and earlier completion than options on the other sites. The existing Hilltop site can be sold to recover cost of a new Courthouse & Civic Center.

### **OPTION C1**

This scheme has the lowest density of any of the schemes. The buildings are lower in keeping with the suburban location with paths, storm water detention ponds and native plantings. Surface lots provides all public and staff parking.

One and two-story commercial development along west entry drive defrays cost for extending roads and utilities while providing retail to support the Courthouse & Civic Center.

### **OPTION C2**

This scheme develops a higher density neighborhood/city center development with taller buildings. Office buildings opposite the Courthouse & Civic Center provide opportunities for law firms and can house a portion of County space such as the office of Public Defense. A combination of parking structures and surface lots are shown for initial construction phases.

Separate buildings for the Courthouse and Administrative office building connected by a shared lobby. A pedestrian plaza created in center of development that can provide outdoor dining, community/festival space and outdoor art. This option provides increased opportunity to share site development costs.



## SITE OPTIONS COMPARISON

		OPTIONS COMPARISON SUMMARY							
		Site Area	Building Area	Number of Phases	Courthouse Height	Office Height	Construction Time	Total Cost (Program Driven)	Total Cost (Budget Driven)
Hilltop Site	A1	460,000sf	335,000sf	4	82 feet	60 feet	5-6 Years	\$302,397,559	\$218,149,000
	A2	380,000sf	335,000sf	4	99 feet	84 feet	5-6 Years	\$276,945,261	\$214,946,505
	A3	390,000sf	335,000sf	3	99 feet	99 feet	4 Years	\$288,993,968	\$234,170,372
	A4	459,250sf	335,000sf	4	116 feet	60 feet	5-6 Years	\$303,600,783	\$214,568,834
	A5	390,000sf	335,000sf	3	99 feet	84 feet	5-6 Years	\$307,570,595	\$231,750,460
Plum Street	B1	486,420sf	335,000sf	1	116 feet	70 feet	2 years	\$315,709,655	\$237,117,203
	B2	486,420sf	335,000sf	1	116 feet	70 feet	2 years	\$313,455,701	\$236,793,078
Harrison West	C1	908,750sf	335,000sf	1	68 feet	54 feet	2 years	\$260,286,371	\$198,799,888
	C2	772,300sf	335,000sf	1	68 feet	68 feet	2 years	\$275,596,864	\$208,537,042



## **COST ESTIMATES**

### **CONSTRUCTION COST ESTIMATES**

The TAS/HOK Team selected Bill Acker Consulting to prepare a Rough Order of Magnitude construction cost estimate for each of the nine options studied for the three sites. All nine options are based on the same program area of 335,000 SF. For the Plum Street sites, an additional 45,000 SF of program area is added to accommodate the City of Olympia's needs for the replacement of the Creighton Justice Center.

The construction cost estimate is prepared in the context of 2018 costs. It then multiplies these costs by a 25% contractor's markup which covers the contractor's general requirements, overhead and profit, bond & insurance, B&O tax, modest design contingency and a sustainability premium. See Report Section XIII for the complete cost estimate report.

### **TOTAL PROJECT COST – PROGRAM DRIVEN**

The TAS/HOK Team took the construction costs from the Acker estimates and prepared a Total Project Cost Summary that is based on the 2050 program requirements as-is. To these construction numbers, we add all other project related soft costs which are unique to each option studied. See Report Section XIV for the Program Driven Total Project Costs for each option.

### **TOTAL PROJECT COST – BUDGET DRIVEN**

Once again, the TAS/HOK Team took the construction costs from the Acker estimates and prepared a Total Project Cost Summary; however, for these Budget Driven Total Project Costs, the TAS/HOK Team worked with County staff to modify the project program, selectively cap (reduce) construction costs where possible, and reduce the amount of time to start construction and shorten the phasing, where possible. The later of these steps, to reduce the time to start construction and shorten the phasing time frames had the greatest impact on reducing costs. See Report Section XV for the Budget Driven Total Project Costs for each option.

### **THIRD PARTY REVIEW OF COSTS**

With the assistance of County staff, the TAS/HOK Team reached out to a respected, large Pacific Northwest contractor, Mortenson Construction, and asked them to review both Bill Acker Consulting's Rough Order of Magnitude construction cost estimate and the TAS/HOK Team's Total Project Cost summaries. Mortenson Construction tested both documents against their past experiences with Courthouse and Civic Center projects and current marketplace pricing here in the Pacific Northwest. Edits to both sets of documents were made based on their input.

### **CONSULTANT REVIEW**

The TAS/HOK Team engaged Civil, Structural, Mechanical, Electrical, and Plumbing consultants to examine each of the three potential sites as well as the existing buildings located on the hilltop. Following is a summary of their findings.

The Hilltop site has development limitations due to the steep slope to the north and east, and some schemes may require slope stabilization. The site has good access to utilities and resides within the City of Olympia's stormwater flow "exempt" region. Traditional footings may be used for the foundation, concrete walls and slab on grade for the bottom level with a steel structural framing system above. Parking structures would be of post-tensioned concrete resting on concrete columns. The existing buildings are of wood framed construction and face a list of potential upgrade issues if they are to be reused.

The Plum Street site has soils that will require piles for the foundation. This site has good access to utilities and is also located in the City of Olympia's stormwater flow "exempt" region. A small wetland area exists on the Plum Street site and would require mitigation to fully utilize the site for development. Structured parking would be required for this site, due to the smaller size, compared to Hilltop which could use a mixture of both surface- and structured parking, and the Harrison West site which could potentially allow for ample surface parking only.

The Harrison West site has good bearing soil and would also use traditional concrete footings; however, the soil has poor draining capabilities and would require stormwater detention ponds or underground storm cells for retention and slow release of storm water. The City of Olympia would likely require the extension of two connector streets and corresponding street improvements through this site (one north-south, and one east-west).

All three sites would follow Low Impact Development (LID) standards for site development.

## **PUBLIC OUTREACH**

Public outreach efforts included 15 Open House sessions in Olympia, Lacey, Yelm, Tenino, Rainier and Rochester, at which the TAS/HOK Team presented a PowerPoint presentation and answered questions. Several 24" x 36" presentation boards and 60" x 24" site boards of the three site options were arranged for public viewing at each Open House. Self-administered surveys were completed by 74 participants, and their responses were documented, compiled, and analyzed to produce a report of stakeholder preferences.

Additional public outreach included social media engagement via Facebook, Instagram & Twitter, direct emails, and direct mailing of flyers. Flyers were also posted at local spots around Olympia, Yelm, Rainier, Tenino, Grand Mound, and Rochester.

Thurston County maintained an up-to-date project website containing scheduled meeting dates, session recordings, information about each of the three sites including images, and surveys. A direct link to the project website is available on the County homepage. Two surveys were posted on the project website which generated 57 responses.

## **CITIZENS ADVISORY COMMITTEE (CAC)**

The TAS/HOK Team benefitted from the regular input (meetings held every three weeks for the duration of this study) from the Citizens Advisory Committee. This diverse group of citizens served as a sounding board for all activities over the duration of the study. Valuable insights and constructive critiques resulted in meaningful course-corrections on many topics.

## **DATA ANALYSIS**

When testing the three sites and nine concept options, our team created a survey with a four-point rating system ranging from Strongly Disagree, Disagree, Agree, to Strongly Agree. The results of the public feedback can be found in Report Section XII.

## **OUTREACH BIAS**

Outreach participants, overall, had a high level of awareness of the need for a new and/or updated courthouse. General observations indicate that the high level of awareness is due to long and frequent occupation or visits to the existing courthouse and many participants were direct stakeholders in the outcome of the project. This level of awareness and interest is not likely to be held by most of the voting public. A significant level of effort will be required to achieve corresponding level of awareness by voters and non-stakeholders in the outcome.



Based on our experience with the CAC and outreach participants, arguably some of the most informed individuals on the issue and those with a significant stake in the outcome, a significant investment of time and resources will be required to fully inform the public so that they understand the current problem, cost drivers, and long-term impacts. A poorly informed population will not likely vote in the affirmative for the planned solution.

The single biggest challenge for a successful outcome is establishing a high level of trust concerning this project and those who are leading it. We believe that time will be the biggest and most important resource to build public trust.

Including the term “Civic Center” appears to be confusing to some participants and seems to imply something more than “county administrative functions and services.” In many communities, a civic center is an auditorium or community center that provides public meeting and recreational space. Preliminary design does not include these types of public amenities. Future public awareness activities will be more effective if the term “civic center” were excluded from the name of the project.

## **NEXT STEPS**

Following is our recommendation for steps to be followed leading up to a public vote in the later part of 2019.

1. **Confirmation of program feasibility.**

This step is largely complete in that the TAS/HOK Team has confirmed that all three sites are viable and can accommodate the full 2050 program requirements. Final review of this report and confirmation by The Board of County Commissioners will complete this step.

2. **Select a preferred site.**

Based on the results of this report, The Board of County Commissioners now have a significant amount of objective information and the results of the public outreach effort completed to-date to decide on the preferred site to proceed forward with. Acknowledging that this is a challenging decision to make, it is of paramount importance that the decision be made in a timely manner to allow the greatest amount of time possible to inform the public prior to the vote.

3. **Determine a budget for selected site.**

Program Driven vs. Budget Driven. The TAS/HOK Team has provided you with a significant amount of cost data to review. Ultimately, a decision needs to be made that will be supported by the greatest number of voters and will provide the citizens of Thurston County with a facility that will serve them well for the next century.

4. **Determine election timing.**

There was considerable discussion regarding an August ballot vs. a November ballot to allow the appropriate amount of time to inform voters.

Placing the funding measure on the August primary election ballot presents several positive and offsetting negative aspects. The single greatest positive aspect is that the Courthouse measure would likely be the only countywide item on the August ballot and, for about 30 percent of the voters, it would be the only item on their ballot.

However, the remaining 70 percent of voters would receive a ballot that also includes primary elections (three or more candidates) in the incorporated areas, such as the City of Olympia and Port Commissioner District 1. Voter turnout is traditionally lower in off-year elections. In the 2015 August primary election, voter participation was about 25 percent compared to the 37 percent participation in the November countywide election (2015 was when the District 1 Port Commissioner was elected). In addition, an August vote significantly reduces the amount of time to educate and inform voters. The additional period between the primary and general election typically presents a more timely and meaningful engagement opportunity with likely voters. This opportunity is leveraged, and voter awareness of issues increases, by the other elections for City Council, Mayor, and Port Commissioner and competing ballot items.

With all these factors taken into consideration, a November ballot is strongly recommended. While having the Courthouse measure be the only countywide item on the ballot and not competing with other local and statewide items has some merit, the additional time gained with a November ballot can be used to better inform the public and voter.

5. **Determine a 2019 Scope of Work for the TAS/HOK Team.**

With County staff input, determine how the TAS/HOK Team can best support the County's needs through further site analysis and preferred concept graphics associated with the preferred site.

6. **Develop public outreach and communications plan.**

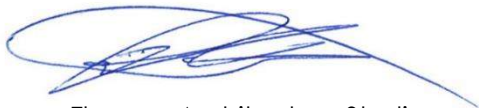
Again, with County staff input, determine how the TAS/HOK Team can best support the County's public outreach and communications plan during 2019 to best inform the public about the project on the preferred site.

7. **Inform the public prior to election.**

Put all the above into action!

On behalf of all members of the TAS/HOK Team, it has been an honor and our pleasure to serve you over these past four months. We look forward to assisting the county in further site analysis and public outreach efforts in 2019.

Respectfully,



Thomas Architecture Studios  
Ron S. Thomas, AIA  
President



Hellmuth, Obata & Kassabaum  
Bob Schwartz, FAIA  
Group VP | Sr. Justice Planner



# INTRODUCTION

I.

THURSTON COUNTY COURTHOUSE FINAL REPORT

## INTRODUCTION

DRAFT



# **Thurston County Courthouse Comprehensive Comparative Feasibility Study**

## **Introduction**

Thurston County government has occupied its current facilities since 1978. Many building systems are reaching the end of their life span. Major facility deficiencies are presently being experienced and are expected to worsen. The Board of County Commissioners (BoCC) is considering the most prudent course of action for the future and whether it is best to reconstruct and expand the existing campus and buildings or build a new Courthouse and Administration complex elsewhere within the city limits of Olympia. The Thurston County Board of County Commissioners commissioned the Thomas Architecture Studios/HOK team in 2015 to complete preliminary space needs programming, building use review and analysis of alternate sites. A Facility Condition Assessment Final Report was completed by MENG Analysis in 2016.

In 2018, the Thurston County Board of County Commissioners selected the Thomas Architecture Studios/HOK team, including architects, engineers, cost estimators and public relations experts, to provide on-call services including a Comprehensive Comparative Feasibility Study. The goal for this study is to: (a) document current and future space needs and (b) develop a comparison of alternative concepts including renovation, expansion, and adaptive reuse of the existing Buildings as well as new construction options. The services include:

- A. Planning, analysis, assessment, evaluation, investigation, development of recommendations, scoping, cost estimating;
- B. Preparing procedures, plans, studies, reports, correspondence, and meeting notes;
- C. Support public outreach, including preparing and participating in briefings to staff, citizens, and elected officials, as well as organizing, coordinating, and staffing public meetings;
- D. Coordination with regulatory agencies, other County departments, consultants, and contractors;
- E. Other types of professional services requested by the County that are consistent with the intent of our contract.

## **PROJECT GOAL**

The study will provide the County with a detailed analysis of each potential site to be used by the Board of County Commissioners to determine the scope, cost and location of a future project.

## **PROJECT APPROACH**

The Thomas Architectural Studios /HOK team worked with the County's Special Projects Coordinator, Rick Thomas, and the City of Olympia's designated representatives in the development of the study as well as user groups including the judiciary, elected officials and county staff.

## **SCOPE OF WORK**

This Comprehensive Comparative Feasibility Study includes: (1) Renovation / Expansion of the Courthouse and Civic Center at its current location Hilltop location, (2) Construct a new Courthouse and Civic Center on the existing City of Olympia property on Plum Street where the existing Creighton Justice Center is located and (3) Construct a new Courthouse and Civic Center on the Harrison West property located at Harrison Avenue NW and Kaiser Road SW.

## **COST ESTIMATES**

This study addresses project options with the following budget scenarios: (1) program driven cost estimate based on full space program projected through 2050 as compared to 2045 in the 2015 Comparable Feasible Study and (2) a budget driven cost estimate based on maximum space utilizing space planning best practices criteria with a project budget ceiling of \$200M - \$235M assuming a Design Build contract is awarded and construction begins in the summer of 2021.

## **PUBLIC OUTREACH**

The TAS/HOK team engaged Doug Mah & Associates, LLC & JayRay to augment County staff and support public outreach efforts, conduct surveys, and document their findings. Anticipating an August 2019 ballot measure, the TAS/HOK team supported County efforts to inform the public through email distribution and direct mailings about upcoming community open house sessions throughout Thurston County, site surveys, and dissemination of up-to-date project information via County website and social media.

Public input regarding each of the three pre-selected sites and corresponding site options were solicited in the following areas: compatibility for surrounding area, use of property, feasibility, economic development driver, promote civic pride, and areas needing improvement. Documentation and analysis of findings was completed by JayRay.

## **STUDY PARTICIPANTS**

Thurston County Commissioners, Thurston County Staff, the Thomas Architecture Studios (TAS) / Hellmuth, Obata & Kassabaum (HOK) Team, and a Citizens Advisory Committee (CAC) comprised of 20 volunteer Thurston County citizen representatives from a broad range of public and private sectors.

## PARTICIPANTS

# II.

THURSTON COUNTY COURTHOUSE FINAL REPORT

## PARTICIPANTS

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## THURSTON COUNTY COURTHOUSE & CIVIC CENTER | PARTICIPANTS

### **THURSTON COUNTY**

Rick Thomas, Special Projects Manager  
Meghan Porter, Public Information Supervisor

### **PRIME / ARCHITECT**

#### **THOMAS ARCHITECTURE STUDIOS**

Ron Thomas, AIA, President  
Amos Callender, Project Manager

### **CONSULTANTS**

#### **HOK**

Bob Schwartz, FAIA, Senior Justice Planner

#### **JAYRAY**

Kathleen Deakins

#### **DOUG MAH & ASSOCIATES**

Doug Mah

#### **SCJ ALLIANCE**

Amy Head

#### **PCS STRUCTURAL SOLUTIONS**

Jeff Klein

#### **HULTZ | BHU**

Rick Hultz

#### **ACKER CONSULTING SERVICES**

Bill Acker

### **PUBLIC**

#### **CITIZENS ADVISORY COMMITTEE**

(Thurston County Citizens)

Angela White  
Ann Freeman-Manzaneres  
Aslan Meade  
Christy Reynolds  
Dave Platt  
David Schaffert  
Dean Foster  
Doug Mah  
Jonathan Sprouffske  
Juanita Taurman  
Kyle Cronk  
Larry Whitaker  
Matt DeBord  
Michael Cade  
Monica Crawford  
Rick Nelson  
Scott Spence  
Shauna Stewart  
Virgil Clarkson  
Whitney Bowerman





# III.

THURSTON COUNTY COURTHOUSE FINAL REPORT

## PROJECT MANAGEMENT

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TASKS		2018		AUGUST				SEPTEMBER				OCTOBER					NOVEMBER				DECEMBER			
		DATES	6	13	20	27	3	10	17	24	1	8	15	22	29	5	12	19	26	3	10	17	24	
		WEEKS			1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	17	18	19	20	
INFORMATION GATHERING	INFORMATION GATHERING										CONCEPTS					TEST OPTIONS & DOCUMENTS								
	1. Project Kick-Off																			Symbol Key				
	Kick-Off Meeting																			Thurston County Staff				
	Common Understanding of all Prior Studies																			TAS Design Team				
	Team Tours of Sites and Buildings																			TAS Cost Estimating				
	Overview of Land Use Requirements (All Sites)																							
	2. Program Confirmation																							
	All Departments Meeting																							
	Review and Update Programming																							
	Prioritize Programming for Fixed Budget Options																							
	Programming for Creighton Center Uses																							
	3. Analysis of Three Sites																							
	Natural Attributes, Utilities Analysis																							
Site Circulation, Transportation, Civil Req's																								
Diagram Site Opportunities																								
4. Needs Assessment																								
Confirm Departmental Relationships																								
Updates to Courtroom Diagrams																								
Update Department Space Diagrams																								
CONCEPTS	5. Concept Alternatives																							
	Site Concepts for all Three Sites																							
	Concept Floor Plans for each Building Option																							
	Overview of Building Systems for Each Option																							
	Security Analysis & Phasing Plans for Each Option																							
6. Concept Features																								
Summary of Concept Features																								
List of Advantages and Disadvantages																								
OUTREACH	7. Outreach Plan																							
	County Project Representatives																							
	County Staff, Electeds, Dept. Heads, BoCC, City																							
	Citizens Advisory Committee																							
	Thurston County Citizens																							
TEST OPTIONS & DOCUMENT	8. Cost Estimate																							
	Construction Cost Estimates																							
	Phasing Costs																							
	Life Cycle & Operational Costs																							
	Total Project Costs																							
	9. Concept Comparison																							
	Assemble Documentation for Draft Report																							
	Concept Comparison Analysis																							
	10. Document & Presentation																							
	Final Report																							
BoCC and Project Stakeholder Presentations																								



building no 2

V.

THURSTON COUNTY COURTHOUSE FINAL REPORT

PROGRAMMING

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# PROGRAMING



# Thurston County Courthouse and Civic Center Space Needs Assessment

Olympia, Washington

December 4, 2018

## Space Summary

Including Olympia Justice Center

		Existing				Existing to Standard				Current Need to Standard				2050 Space Projection			
No.	Room Name/ Position	Staff No.	Area (SF)	DGSF Area	TOTAL BGSF	Staff No.	DGSF Area	TOTAL BGSF	Staff No.	DGSF Area	TOTAL BGSF	Staff No.	DGSF Area	TOTAL BGSF	Staff No.	DGSF Area	TOTAL BGSF
Courthouse																	
1.0	Courthouse Lobby + Public Space	5	2,164	3,153	3,941	5	2,796	1,30	3,635	8	8,657	8	8,873	11,535	8	8,873	11,535
2.0	Superior Court	23	15,318	21,132	26,415	27	35,161	1,30	45,709	31	42,298	31	58,990	76,687	41	58,990	76,687
2.5	Pre-Trial Services	3	286	343	429	5	491	1,30	638	8	1,587	8	2,035	2,646	13	2,035	2,646
3.0	Clerk	29	4,523	5,021	6,276	26	6,002	1,30	7,803	35	6,658	35	7,425	9,653	46	7,425	9,653
4.0	District Court	33	9,266	10,341	12,926	32	18,808	1,30	24,451	36	28,025	36	39,385	51,200	57	39,385	51,200
5.0	Sheriff (court support area)	6	2,292	6,228	7,785	7	4,698	1,30	6,107	7	4,941	7	5,130	6,689	8	5,130	6,689
SUBTOTALS		99	33,849	46,218	57,773	102	67,957		88,344	125	92,166	125	121,838	158,390	173	121,838	158,390
Sheriff																	
9.0	Sheriff Administration	37	6,020	9,150	11,438	37	8,437	1,25	10,546	39	9,009	39	9,955	12,444	45	9,955	12,444
10.0	Sheriff Field Operations Bureau (FOB)	36	28,370	31,491	39,364	36	19,864	1,25	24,830	41	20,597	41	21,975	27,469	60	21,975	27,469
SUBTOTALS		73	34,390	40,641	50,801	73	28,301		35,376	80	29,606	80	31,931	39,913	105	31,931	39,913
Civic Center																	
6.0	Prosecutor	81	12,555	15,470	19,338	80	11,252	1,25	14,065	89	16,462	89	21,322	26,652	127	21,322	26,652
7.0	Thurston County Public Defense	39	6,139	8,969	11,211	39	8,502	1,25	10,628	46	9,569	46	13,159	16,449	72	13,159	16,449
8.0	Drug Court (outside courthouse)	9	3,425	5,032	6,290	9	4,833	1,25	6,042	8	5,031	8	5,031	6,289	8	5,031	6,289
11.0	Assessor	33	3,704	4,457	5,571	33	5,184	1,25	6,480	35	5,715	35	6,136	7,670	38	6,136	7,670
12.0	Auditor	23	15,097	19,174	23,968	23	17,368	1,25	21,710	25	17,794	25	18,174	22,717	27	18,174	22,717
13.0	Treasurer	15	1,521	1,886	2,358	15	3,047	1,25	3,809	17	3,110	17	3,235	4,043	19	3,235	4,043
14.0	Environmental Health	20	3,461	4,113	5,141	15	2,272	1,25	2,841	15	2,272	15	2,564	3,205	15	2,564	3,205
15.0	Commissioners	14	3,814	4,173	5,216	14	5,143	1,25	6,429	17	5,452	17	6,716	8,395	21	6,716	8,395
16.0	Development Review	5	472	627	784	6	603	1,25	754	6	603	6	920	1,151	10	920	1,151
17.0	Community Planning & Economic Development (CPED)																
	Water Resources & Resource Stewardship	71	9,304	10,514	13,143	61	14,310	1,25	17,888	62	14,654	62	14,479	18,099	66	14,479	18,099
18.0	Information Technology (IT)	4	328	42	53	4	48	1,25	60	4	48	5	5,757	7,196	5	5,757	7,196
19.0	Human Resources	17	3,570	5,648	7,060	17	5,699	1,25	7,124	19	5,782	19	6,360	7,950	21	6,360	7,950
20.0	Central Services	50	4,289	9,112	11,390	50	5,184	1,25	6,481	51	5,314	51	6,339	7,924	72	6,339	7,924
SUBTOTALS		381	67,679	89,217	111,521	366	83,447		104,309	394	91,806	394	110,191	137,738	501	110,191	137,738
Olympia Justice Center																	
22.0	Justice Center																
22.1	Municipal Court	8	0	0	0	8	9,261	1,25	11,576	10	9,828	10	10,125	12,656	13	10,125	12,656
22.2	City Prosecutor	5	0	0	0	5	1,679	1,25	2,099	8	1,939	8	2,074	2,592	9	2,074	2,592
22.3	Community Court	0	0	0	0	1	4,104	1,25	5,130	0	4,509	0	4,806	6,008	0	4,806	6,008
22.4	Probation Services	2	0	0	0	2	2,479	1,25	3,098	2	2,479	2	2,803	3,503	4	2,803	3,503
22.5	Public Defense Coordinator	2	0	0	0	2	545	1,25	682	2	1,112	2	1,112	1,391	2	1,112	1,391
22.6	Olympia City Police/Holding	9	0	0	0	9	11,997	1,25	14,997	12	12,078	12	12,078	15,098	12	12,078	15,098
22.7	Building Support	1	0	0	0	2	3,048	1,25	3,809	2	3,048	2	3,048	3,809	2	3,048	3,809
SUBTOTALS		27	0	0	0	29	33,113		41,392	36	34,993	36	36,046	45,057	42	36,046	45,057
TOTALS		580	135,918	176,076	220,095	570	212,818		269,421	635	248,571	635	300,005	381,098	821	300,005	381,098



# Thurston County Courthouse and Civic Center Space Needs Assessment

Olympia, Washington

December 4, 2018

## Space Summary

		Existing				Existing to Standard				Current Need to Standard				2050 Space Projection			
No.	Room Name/ Position	Staff No.	Area (SF)	DGSF Area	TOTAL BGSF	Staff No.	DGSF Area	TOTAL BGSF	Staff No.	DGSF Area	TOTAL BGSF	Staff No.	DGSF Area	TOTAL BGSF	Staff No.	DGSF Area	TOTAL BGSF
Courthouse																	
1.0	Courthouse Lobby + Public Space	5	2,164	3,153	3,941	5	2,796	1.30	3,635	8	8,657	11,254	8	8,873	11,535		
2.0	Superior Court	23	15,318	21,132	26,415	27	35,161	1.30	45,709	31	42,298	54,988	41	58,990	76,687		
2.5	Pre-Trial Services	3	286	343	429	5	491	1.30	638	8	1,587	2,063	13	2,035	2,646		
3.0	Clerk	29	4,523	5,021	6,276	26	6,002	1.30	7,803	35	6,658	8,656	46	7,425	9,653		
4.0	District Court	33	9,266	10,341	12,926	32	18,808	1.30	24,451	36	28,025	36,433	57	39,385	51,200		
5.0	Sheriff (court support area)	6	2,292	6,228	7,785	7	4,698	1.30	6,107	7	4,941	6,423	8	5,130	6,669		
SUBTOTALS		109	37,274	51,250	57,773	112	73,645		88,344	125	92,166	119,816	173	121,838	158,390		
Civic Center																	
6.0	Prosecutor	81	12,555	15,470	19,338	80	11,252	1.25	14,065	89	16,462	20,577	127	21,322	26,652		
7.0	Thurston County Public Defense	39	6,139	8,969	11,211	39	8,502	1.25	10,628	46	9,569	11,961	72	13,159	16,449		
8.0	Drug Court (outside courthouse)	9	3,425	5,032	6,290	9	4,833	1.25	6,042	8	5,031	6,289	8	5,031	6,289		
11.0	Assessor	33	3,704	4,457	5,571	33	5,184	1.25	6,480	35	5,715	7,143	38	6,136	7,670		
12.0	Auditor	23	15,097	19,174	23,968	23	17,368	1.25	21,710	25	17,794	22,243	27	18,174	22,717		
13.0	Treasurer	15	1,521	1,886	2,358	15	3,047	1.25	3,809	17	3,110	3,887	19	3,235	4,043		
14.0	Environmental Health	20	3,461	4,113	5,141	15	2,272	1.25	2,841	15	2,272	2,841	15	2,564	3,205		
15.0	Commissioners	14	3,814	4,173	5,216	14	5,143	1.25	6,429	17	5,452	6,815	21	6,716	8,395		
16.0	Development Review	5	472	627	784	6	603	1.25	754	6	603	754	10	920	1,151		
Community Planning & Economic Development (CPED)																	
Water Resources & Resource Stewardship		71	9,304	10,514	13,143	61	14,310	1.25	17,888	62	14,654	18,317	66	14,479	18,099		
18.0	Information Technology (IT)	4	328	42	53	4	48	1.25	60	4	48	60	5	5,757	7,196		
19.0	Human Resources	17	3,570	5,648	7,060	17	5,699	1.25	7,124	19	5,782	7,228	21	6,360	7,950		
20.0	Central Services	50	4,289	9,112	11,390	50	5,184	1.25	6,481	51	5,314	6,643	72	6,339	7,924		
SUBTOTALS		381	67,679	89,217	111,521	366	83,447		104,309	394	91,806	114,758	501	110,191	137,738		
Sheriff - not included in Courthouse + Civic Center																	
9.0	Sheriff Administration	37	6,020	9,150	11,438	37	8,437	1.25	10,546	39	9,009	11,261	45	9,955	12,444		
10.0	Sheriff Field Operations Bureau (FOB)	36	28,370	31,491	39,364	36	19,864	1.25	24,830	41	20,597	25,747	60	21,975	27,469		
SUBTOTALS		73	34,390	40,641	50,801	73	28,301		35,376	80	29,606	37,008	105	31,931	39,913		
TOTAL					220,095	551			228,029			271,582			336,041		

Notes:

1. BGF = Building Grossing Factor: (defined as the increase in area due to exterior walls, circulation outside department, and mechanical/electrical spaces)
2. Sheriff's Field Operations Bureau requires 12,400 NSF of covered secure exterior space NOT included in total.
3. BGF for existing Justice Facility spaces is 1.25.

# Thurston County Courthouse and Civic Center Space Assessment

Olympia, Washington

## Space Summary

Courthouse									
No.	Department	Existing		Existing to Standard		Current Need to Standard		2050 Space Projection	
		Staff	Net Area	DGSF Area	Standard Net Area	Staff	DGSF Area	Staff	DGSF Area
1.0	Courthouse Lobby + Public Space	5	2,164	3,153	2,330	8	8,657	8	8,873
2.0	Superior Court	23	15,318	21,132	27,047	31	42,298	41	58,990
2.5	Prettrial Services	3	286	343	372	8	1,587	13	2,035
3.0	Clerk	29	4,523	5,021	4,446	35	6,658	46	7,425
4.0	District Court	33	9,266	10,341	14,468	36	28,025	57	39,385
5.0	Sheriff Court Support	6	2,292	6,228	3,480	7	4,941	8	5,130
6.0	Prosecutor	Temporary Workspace Included within Courthouse Lobby Program							
7.0	Thurston County Public Defense	Temporary Workspace Included within Courthouse Lobby Program							
SUBTOTAL		99		46,218		125	92,166	173	121,838
Building Grossing Factor		0.25		11,555	0.30		20,093		36,551
TOTAL BGFS				57,773			119,816		158,390

Court-Related Agencies									
No.	Department	Existing		Existing to Standard		Current Need to Standard		2050 Space Projection	
		Staff	Net Area	DGSF Area	Standard Net Area	Staff	DGSF Area	Staff	DGSF Area
6.0	Prosecutor	81	12,555	15,470	11,252	89	12,194	127	21,322
7.0	Thurston County Public Defense	39	6,139	8,969	6,441	46	9,569	72	13,159
8.0	Drug Court (outside courthouse)	9	3,425	5,032	3,718	8	5,031	8	5,031
SUBTOTAL		129		29,471		143	26,794	207	39,512
Building Grossing Factor		0.25		7,368	0.25		7,131		9,878
TOTAL BGFS				36,839			33,492		49,390

County Administrative Offices									
No.	Department	Existing		Existing to Standard		Current Need to Standard		2050 Space Projection	
		Staff	Net Area	DGSF Area	Standard Net Area	Staff	DGSF Area	Staff	DGSF Area
9.0	Sheriff's Administrative Office	37	6,020	9,150	6,490	39	9,009	45	9,955
10.0	Sheriff's Field Operations Bureau	36	28,370	31,491	15,280	41	20,597	60	21,975
11.0	Assessor	33	3,704	4,457	3,988	35	5,715	38	6,136
12.0	Auditor	23	15,097	19,174	13,672	25	17,794	27	18,174
13.0	Treasurer	15	1,521	1,886	2,344	17	3,110	19	3,235
14.0	Environmental Health	20	3,461	4,113	1,748	15	2,272	15	2,564
15.0	Commissioners	14	3,814	4,173	3,956	17	5,452	21	6,716
16.0	Development Review	5	472	627	464	6	603	10	920
17.0	Community Planning & Economic Development	71	9,304	10,514	11,008	62	14,654	66	14,479
18.0	GIS	4	328	42	256	4	48	5	48
19.0	Human Resources	17	3,570	5,648	4,384	19	5,782	21	6,360
20.0	Central Services	50	4,289	9,112	3,988	51	5,314	72	6,339
SUBTOTAL		325		100,387		331	90,351	399	96,900
Building Grossing Factor		0.25		25,097	0.25		22,588		24,225
TOTAL BGFS				125,484			112,939		121,125

## Space Summary

Notes:

1. BGF = Building Grossing Factor (defined as the increase in area due to exterior walls, circulation outside department, and mechanical/electrical spaces)
2. Sheriff's Field Operations Bureau future space needs include 12,400 SF of covered secure exterior space.
3. BGF for existing Justice Facility spaces is 1.25. For standard and future spaces, the BGF is 1.38 (as shown).





# Thurston County Courthouse and Civic Center Space Assessment

## 1.0 Court Lobby and Common Spaces

Olympia, Washington

Future Space Summary

No.	Room Name/ Position	Existing		Existing to Standard		Current Need to Standard		2050 Space Projection		Additional Comments
		No.	Area (SF)	Total Area	No.	Standard Area	Total Area	No.	Total Area	
1.01	Entry Vestibule	0	0	0	1	80	80	1	80	Gun lockers prior to security screening
1.02	Security Queuing	0	0	0	1	800	800	1	800	
1.03	Security Screening	2	300	600	2	80	160	2	160	Two staff per screening station
1.04	Information Counter/Kiosk	0	0	0	1	150	150	1	150	
1.05	Lobby	1	1,564	1,564	1	900	900	1	900	
1.06	Security Office	0	60	0	0	120	0	1	120	Building security and screening staff
1.07	Media Work Room	0	100	0	0	180	0	0	180	
1.08	Attorney Work Area	0	161	0	1	240	240	1	240	
1.09	AV/IT Staff Work Area	0	0	0	0	180	0	1	180	2 workstations, workbenches, storage
	<i>Law Enforcement Waiting</i>									
1.10	Law Enforcement Waiting	0	350	0	0	300	0	1	300	include 2 officer workstations
1.11	Officer Restrooms	0	350	0	0	50	0	2	100	
	<i>Prosecutor Satellite Workspace</i>									
1.12	Victim Advocate				0	120	0	0	0	When office is not located in courthouse
1.13	Attorney Client Conference				0	120	0	0	0	
1.14	Attorney Work Area				0	120	0	0	0	
	<i>Thurston County Public Defense</i>									
1.15	Attorney Client Conference				0	120	0	0	0	When office is not located in courthouse
1.16	Attorney Work Area				0	120	0	0	0	
	<i>Children's Center</i>									
1.17	Reception/Waiting Area				0	80	0	1	80	When office is not located in courthouse
1.18	Activity Area				0	480	0	1	480	
1.19	Staff Office				0	120	0	1	120	
1.20	Children's Toilet				0	64	0	1	64	
	<i>Food Service</i>									
1.21	Counter/ Work Area				0	300	0	1	300	
1.22	Vending				0	300	0	1	300	includes microwave, condiments
1.23	Storage				0	120	0	1	120	
1.24	Seating				0	800	0	1	800	
1.25	Staff Toilet				0	50	0	1	50	
	<i>Building Support</i>									
1.30	Loading Dock	0	0	0	0	300	0	1	300	Assumes vehicles parked up to and outside building footprint
1.31	Staging/Screening	0	0	0	0	250	0	1	250	
1.32	Recycling Staging	0	0	0	0	80	0	1	80	
1.33	Trash	0	0	0	0	60	0	1	60	
1.34	Fire Control Center	0	0	0	0	80	0	1	80	
1.35	Building Storage	0	0	0	0	400	0	1	400	
1.36	Facility Maintenance Area	0	0	0	0	300	0	1	300	
1.37	Staff Locker/Shower Rooms	0	0	0	0	140	0	2	280	
1.38	Bicycle Storage	0	0	0	0	120	0	1	120	

# Thurston County Courthouse and Civic Center Space Assessment

## 1.0 Court Lobby and Common Spaces

### Future Space Summary

No.	Room Name/ Position	Existing			Existing to Standard		Current Need to Standard		2050 Space Projection		Additional Comments
		No.	Area (SF)	Total Area	No.	Standard Area	Total Area	No.	Total Area	Total Area	
	Overall Building Grossing Factor Includes										
	Public toilets including a family toilet on each court floor										
	Janitors Closets										
	Electrical rooms										
	IDF rooms on each floor										Quantity varies with size of floor plate
	Stairs										Quantity varies with size of floor plate
	Elevators										
	Mechanical shafts and chases										
	Mechanical, electrical, plumbing, fire protection and main distribution frame										
SUBTOTAL NET AREA		5	0.46	2,164	5		2,330	8	7,214	8	7,394
Circulation and Internal Walls				989		0.20	466		1,443		1,479
TOTAL DGFS				3,153			2,796		8,657		8,873



# Thurston County Courthouse and Civic Center Space Assessment

## Olympia, Washington

## 2.0 Superior Courts

### Future Space Summary

No.	Room Name/ Position	Existing			Existing to Standard		Current Need to Standard		2050 Space Projection		Additional Comments	
		No.	Area (SF)	Total Area	No.	Standard Area	Total Area	No.	Total Area	No.		Total Area
Courtrooms												
2.01	Typical Courtroom	4	1,221	4,884	4	1,800	7,200	5	9,000	9	16,200	50 spectators, 15 p. jury box + bailiff
2.02	Hearing Courtroom	1	1,025	1,025	1	1,500	1,500	1	1,500	1	1,500	Commissioner's court, no jury box
2.03	Large Courtroom/Board Room	1	2,658	2,658	1	2,500	2,500	1	2,500	1	2,500	200 spectators, 16 p. jury box + bailiff, tables, bench for 3 county commissioners
2.04	Soundlock Entry Vestibule	0	0	0	6	80	480	7	560	11	880	
2.05	Attorney Client Conference Rooms	1	149	149	12	100	1,200	14	1,400	22	2,200	For public use
2.06	Court Waiting	0	0	0	6	240	1,440	7	1,680	11	2,640	(4) Four Public Service Windows
2.07	Evidence Storage	0	0	0	6	25	150	7	175	11	275	Storage during recesses; one per court clerk
2.08	AV / IT Technology	0	0	0	3	80	240	3.5	280	5.5	440	
2.09	Witness Waiting	0	0	0	2	120	240	2	240	2	240	
Jury Deliberation Suites												
2.10	Jury Deliberation Room	4	280	1,120	4	350	1,400	5	1,750	7	2,450	Also used for Mediation
2.11	Soundlock Vestibule	0	0	0	4	80	320	5	400	7	560	
2.12	Juror Toilets	5	18	90	8	50	400	10	500	14	700	
Court Holding												
2.15	Single Holding Cell	0	0	0	0	70	0	0	0	0	0	Use Sheriff's central holding
2.16	Large Holding Cell	1	96	96	0	100	0	0	0	0	0	Use Sheriff's central holding
2.17	Court Staging	1	0	0	3	80	240	4	320	6	480	
2.18	Soundlock Vestibule	2	0	0	6	0	0	7	0	11	0	
Judicial Offices												
2.20	Presiding Judge's Chambers	1	405	405	0	350	0	0	0	0	0	May use collegial floor
2.21	Judge's Chambers	5	360	1,800	6	300	1,800	7	2,100	11	3,300	Judges and Commissioners
2.22	Judge's Toilets	6	20	120	6	50	300	7	350	11	550	Shared with court staff; multi-fixture toilet rooms
2.23	Law Clerk/Intern Office	0	0	0	2	120	240	2	240	2	240	
2.24	Pro Tem Judge	0	0	0	1	120	0	1	120	1	120	
2.25	Judicial Assistants	3	128	384	3	80	240	4	320	6	480	
2.26	Court Administrator	1	322	322	1	180	180	1	180	1	180	
2.27	Administrative Services Manager	1	107	107	1	120	120	1	120	1	120	
2.28	Operations Manager	1	100	100	1	120	120	1	120	1	120	
2.29	Administrative Assistant	2	65	130	2	64	128	2	128	2	128	
2.30	Court Reporters	5	100	500	6	100	600	7	700	11	1,100	
2.31	Staff Attorney	1	182	182	1	120	120	1	120	1	120	
2.32	Copier/Printers/Supply Areas	1	59	59	2	100	200	2	200	2	200	
2.33	Receptionist/Office Assistant	0	100	0	1	80	80	2	160	2	160	
2.34	Reception Area	0	60	0	1	100	100	2	200	2	200	
2.35	Small Conference Room	0	0	0	0	120	0	2	240	3	360	
2.36	Bailiff Work Area	0	85	0	1	120	120	2	240	2	240	Shared work area by bailiffs on floor
2.37	Break Room	1	379	379	1	300	300	1	300	1	300	
2.38	Judicial Conference Room	0	0	0	0	500	0	1	500	1	500	
2.39	Storage Closet	4	35	140	2	30	60	4	120	4	120	
2.40	Staff Restroom	1	29	29	4	50	200	4	480	4	480	
2.41	Court Commissioner/Pro Tem	1	0	0	1	120	120	1	120	1	120	

## 2.0 Superior Courts

## Future Space Summary

**SUBTOTAL NET AREA**  
Circulation and Internal Walls  
**TOTAL DGGSF**



## 2.5 Pretrial Services

Interviewee: Marianne Clear, Director

[illegible]

### 3.0 County Clerk Future Space Summary

Interviewees: Linda Myhre Enlow, Thurston County Clerk  
Tawni I. Sharp, Chief Deputy Clerk

[illegible]

# Thurston County Courthouse and Civic Center Space Assessment

Olympia, Washington

## 4.0 District Courts

Future Space Summary

No.		Room Name/ Position	Existing		Existing to Standard		Current Need to Standard		2050 Space Projection		Additional Comments		
			No.	Area (SF)	Total Area	No.	Standard Area	Total Area	No.	Total Area			
Courtrooms													
4.01	Typical Courtroom	2	1,034	2,068	2	1,800	3,600	4	7,200	6	10,800	50 spectators; 15 p. jury box + bailiff	
4.02	Hearing Courtroom	2	632	1,264	2	1,500	3,000	2	3,000	2	3,000	Commissioner's court; no jury box	
4.03	Soundlock Entry Vestibule	0	0	0	0	0	80	240	5	400	7	560	
4.04	Attorney/Client Conference Rooms	2	99	198	6	100	600	10	1,000	14	1,400	For public use	
4.05	Court Waiting	3	200	600	3	240	720	5	1,200	7	1,680	Currently portion of lobby	
4.06	Evidence/Storage	0	0	0	0	0	50	150	5	250	7	350	
4.07	AV / IT Technology	3	42	126	2	80	160	3	240	4	320		
4.08	Witness Waiting	0	0	0	0	1	120	120	2	240	2	240	
4.09	Settlement Conference Rooms	0	0	0	0	0	150	0	0	0	2	300	Alternate Dispute Resolution
4.10	Settlement Judge	0	0	0	0	0	150	0	0	0	1	150	
Jury Deliberation Suites													
4.11	Jury Deliberation Room	1	211	211	1	350	350	2	700	4	1,400	Also used for conference room.	
4.12	Soundlock Vestibule	0	0	0	0	80	0	2	160	4	320		
4.13	Juror Toilets	0	0	0	0	0	50	0	4	200	8	400	
Court Holding													
4.14	Single Holding Cell	0	175	0	0	70	0	0	0	0	0		
4.15	Large Holding Cell	0	96	0	0	100	0	0	0	0	0		
4.16	Court Staging	0	124	0	2	80	160	3	240	4	320		
4.17	Soundlock Vestibule	0	232	0	0	42	0	0	0	0	0		
Judicial Offices													
4.18	Presiding Judge's Chambers	1	229	229	1	300	300	1	300	1	300		
4.19	Judge's Chambers	3	158	474	3	300	900	4	1,200	6	1,800		
4.20	Pro-Tem Judge's Office	1	0	0	1	120	120	1	120	1	120		
4.21	Law Clerk/Intern	0	0	0	0	0	120	0	1	120	1	120	
4.22	Judge's Toilet	1	124	124	0	50	0	0	0	0	0		Shared with court staff; multi-fixture toilet rooms
4.23	Judicial Assistant	1	128	128	1	80	80	2	160	4	320		
4.24	Court Administrator	1	153	153	1	180	180	1	180	1	180		
4.25	Judicial Conference Room	0	0	0	0	300	0	1	300	1	300		
4.26	Staff Restrooms	0	0	0	2	50	100	2	100	2	100		
4.27	Copy Room	0	0	0	1	40	40	1	40	1	40		
4.28	Break Room	0	0	0	0	180	0	1	180	1	180		



**Thurston County Courthouse and Civic Center Space Assessment**  
Olympia, Washington

**4.0 District Courts**  
Future Space Summary

No.	Room Name/ Position	Existing		Existing to Standard		Current Need to Standard		2050 Space Projection		Additional Comments								
		No.	Area (SF)	Total Area	No.	Standard Area	Total Area	No.	Total Area									
Clerk Functions																		
4.29	Administrative Service Manager	1	160	160		1	120	1	120									
4.30	Financial Supervisor	1	124	124	1	100	100	1	100									
4.31	Counter Queuing	1	300	300	1	300	300	1	300	Currently portion of lobby								
4.32	Account Clerks	2	232	464	2	100	200	2	200	4	400	Includes file storage shelving						
4.33	Court Assistant I	5	75	375	5	64	320	5	320	8	512							
4.34	Court Assistant II	5	68	340	5	64	320	5	320	9	576							
4.35	Civil Clerk	2	232	464	2	100	200	2	200	4	400							
4.36	Calendaring Coordinator	1	86	86	1	64	64	1	64	1	64							
4.37	Courtroom Coordinator	1	86	86	1	64	64	1	64	1	64							
4.38	Judicial Conference Room	0	0	0	0	300	0	1	300	1	300							
4.39	Supply/Workroom	1	98	98	2	80	160	2	160	2	160							
4.40	Copy Room	1	32	32	1	40	40	1	40	2	80							
4.41	File Room	1	256	256	1	200	200	1	200	1	200							
4.42	Staff Restroom	2	30	60	2	120	240	2	240	4	480							
4.43	Break Room	1	128	128	1	240	240	1	240	1	240							
	Probation																	
4.50	Probation Program Manager	0	0	0	1	140	140	1	140	1	140	Jail level 2						
4.51	Probation Officer	3	110	330	3	120	360	3	360	6	720	Jail level 2						
4.52	Probation Clerk	2	50	100	1	80	80	2	160	3	240	Jail level 2						
4.53	Copy/Print/Supply Area	0	32	0	1	60	60	1	60	1	60							
	Mental Health/Veterans Court																	
4.54	Program Manager	1	94	94	1	140	140	1	140	1	140							
4.55	Care Coordinator	2	72	144	2	120	240	2	240	2	240							
4.56	Administrator	1	50	50	0	120	0	0	0	0	0							
4.57	Copy/Print/Supply Area	0	32	0	1	60	60	1	60	1	60							
SUBTOTAL NET AREA											33	9,266	32	14,468	36	21,558	57	30,296
Circulation and Internal Walls												0.12		0.30		6.467		9.089
TOTAL DG/SF												10,341		18,808		28,025		39,385

## 5.0 Sheriff Court Support

**SUBTOTAL NET AREA**  
Circulation and Internal Walls  
**TOTAL DG/SF**





**Thurston County Courthouse and Civic Center Space Assessment**  
Olympia, Washington

**6.0 Prosecuting Attorney**  
Future Space Summary

No.	Room Name/ Position	Existing		Existing to Standard		Current Need to Standard		2050 Space Projection		Additional Comments
		No.	Area (SF)	No.	Standard Area	No.	Total Area	No.	Total Area	
	<i>Support</i>									
6.50	Break Room	2	290	1	350		350	1	350	Building 5 lunch room
6.51	Small Conference Rooms	0	0	0	120	0	120	2	240	4 person
6.52	Conference Rooms	3	200	2	240	2	480	2	480	Misdemeanor/Civil
6.53	Large Conference Room	1	209	209	360	1	360	1	360	18 person
6.54	Staff Toilets	0	0	0	120	0	120	2	240	
6.55	Supply Storage	1	119	1	120	1	120	1	120	
6.56	Copier/Printer	2	40	80	40	2	80	2	80	acoustical separation
6.57	File Storage	1	101	101	120	1	120	1	120	
6.58	AV/IT Equipment Room	0	100	0	120	1	120	1	120	
6.59	Technology Storage	1	93	93						
6.60	Storage	1	32	32	64	1	64	1	64	
6.61	Exhibit Storage	0	100	0	120	1	120	1	120	
	Subtotal	0		1,814	0		1,894	0	2,174	0 2,294
	<i>Family Services</i>									Separate secure area
6.55	Public Waiting	1	115	115	1	120	1	120	1	120
6.56	Reception	1	155	155	1	64	64	1	64	1 64
6.57	Closets	2	10	20	2	10	20	2	20	2 20
6.58	Attorneys	4	122	488	4	120	480	3	360	8 960
6.59	Paralegals	2	120	240	2	64	128	3	192	3 192
6.60	Legal Assistant	4	100	400	4	64	256	4	256	5 320
6.61	Large Conference Room	1	360	360	1	360	360	1	360	1 360 18 person
6.62	FS Conference/Breakroom	1	410	410	1	180	180	1	180	1 180
6.63	Family Restroom	0	0	0	70	0	70	1	70	1 70
6.64	FS File/Server Room	1	150	150	1	120	120	1	120	1 120
	Subtotal	11		2,338	11		1,728	11	1,742	17 2,406
	<i>Civil</i>									
6.66	Civil Attorneys	6	126	756	6	120	720	7	840	10 1,200
6.67	Chief Civil Attorney	1	215	215	1	180	180	1	180	1 180
6.68	Civil Legal Assistant	1	215	215	1	120	120	1	120	3 360
6.69	Civil Paralegal	2	122	244	2	64	128	2	128	5 320
6.70	Civil Intern	1	25	25	1	48	48	1	48	2 96
6.71	Civil File Room	1	105	105	1	200	200	1	200	1 200
6.72	Law Library/Conference Room	1	250	250	1	300	300	1	300	1 300
6.73	Law Library	2	50	100	1	100	100	1	100	1 100
	Subtotal	11		1,910	11		1,796	12	1,916	21 2,756
	<b>SUBTOTAL NET AREA</b>	<b>81</b>	<b>0.23</b>	<b>12,555</b>	<b>80</b>	<b>0.35</b>	<b>11,252</b>	<b>89</b>	<b>12,194</b>	<b>127 15,794</b>
	<b>Circulation and Internal Walls</b>			<b>2,915</b>			<b>3,938</b>		<b>4,268</b>	<b>5,528</b>
	<b>TOTAL DGsf</b>			<b>15,470</b>			<b>15,190</b>		<b>16,462</b>	<b>21,322</b>

## 7.0 Pubic Defense

## Future Space Summary

SUBTOTAL NET AREA	39		6,139	39	6,441	72	9,969	Includes space for interns
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## 8.0 Drug Court Program/Treatment Facility Separate Facility

**SUBTOTAL NET AREA**  
Circulation and Internal Walls  
**TOTAL DG/SF**



**Thurston County Courthouse and Civic Center Space Needs Assessment**  
**9.0 Sheriff's Office**  
 Olympia, Washington Administration

No.	Room Name/ Position	Existing		Existing to Standard		Current Need to Standard		2050 Space Projection		Additional Comments
		No.	Area (SF)	No.	Standard Area	No.	Total Area	No.	Total Area	
9.00	Vestibule	0	0	0	80	0	0	0	0	
9.01	Lobby/Waiting	1	76	1	150	1	150	1	150	
9.02	Civil Sergeant	1	86	86	1	80	1	80	1	80
9.03	Civil Deputy	1	60	60	1	80	80	1	80	
9.04	Civil Lieutenant	1	106	106	1	120	120	1	120	
9.05	Chief of Civil	1	118	118	1	150	150	1	150	
9.06	Legal Assistant - Weapons Transfer	1	104	104	1	64	64	1	64	
9.07	Legal Assistant - Supervisor	1	104	104	1	80	80	1	80	
9.08	Legal Assistant - Warrants	4	81	324	4	64	256	4	256	
9.09	Sheriff's Reception	1	139	139	1	300	300	1	300	
9.09	Administrative Aide	1	184	184	1	180	180	1	180	
9.10	Sheriff	1	198	198	1	300	300	1	300	
9.11	Undersheriff	1	227	227	1	240	240	1	240	
9.12	Chief's Office	1	208	208	1	180	180	1	180	
9.13	Conference Room	1	148	148	1	240	240	1	240	
9.14	Lieutenant - Support Services Bureau	1	108	108	1	120	120	1	120	
9.15	IT Supervisor	1	116	116	1	120	120	1	120	
9.16	Records Clerks	3	93	279	3	64	192	3	192	
9.17	Records Room	1	505	505	1	500	500	1	500	
9.18	Detectives	3	88	264	3	80	240	3	240	
9.19	Shared Detectives Office	1	137	137	1	240	240	1	240	
9.20	Detective Sergeant	0	124	0	120	0	120	2	240	
9.21	Detective Lieutenant	1	137	137	1	150	150	1	150	
9.22	Interview Room	1	87	87	1	120	120	2	240	
9.23	Court Deputy	1	147	147	1	120	120	1	120	
9.24	Computer Forensic Sergeant	0	124	0	120	0	120	1	120	
9.25	Computer Forensic Detective	1	161	161	1	160	160	1	160	
9.26	Copier/Supply Room	1	56	56	1	80	80	2	160	
9.27	Legal Assistant	1	81	81	1	80	80	1	80	
9.28	Financial Services Manager	1	81	81	1	80	80	1	80	
9.29	Financial Services Analyst	3	81	243	3	80	240	3	240	
9.30	Financial Services Supervisor	1	81	81	1	80	80	1	80	
9.31	Office Assistants	2	50	100	2	48	96	2	96	
9.32	Vacant Spaces	2	47	94	2	48	96	2	96	
9.33	Men's Locker Room	1	420	420	1	480	480	1	480	
9.34	Women's Locker Room	1	215	215	1	240	240	1	240	
9.35	Break Room	1	133	133	1	300	300	1	300	
9.36	Polygraph Examiner	1	94	94	1	120	120	1	120	
9.37	Polygraph/Float Office	1	131	131	1	120	120	1	120	
9.38	Cold Case Volunteers	2	134	268	2	48	96	2	96	
<b>Sexual Offender Reporting Allocated within Sheriff's Courthouse Space</b>										
SUBTOTAL NET AREA		37	6,020	37	6,490	39	6,930	45	7,658	
Circulation and Internal Walls			0.52		0.30		1,947		2,297	
TOTAL DGSP					8,437		9,009		9,955	

# Thurston County Courthouse and Civic Center Space Assessment

Olympia, Washington

## 10.0 Sheriff's Office Field Operations Bureau

No.	Room Name/ Position	Existing		Existing to Standard		Current Need to Standard		2050 Space Projection		Additional Comments
		No.	Area (SF)	No.	Standard Area	No.	Total Area	No.	Total Area	
10.00	Reception/Waiting	1	257	1	150	1	150	1	150	
10.01	Reception/Office Assistant	0	0	0	64	1	64	1	64	
10.02	Sergeant's Office	5	127	5	120	5	600	8	960	
10.03	Lieutenant's Office	3	140	3	150	3	450	5	750	
10.04	Chaplain's Office	1	104	1	120	1	120	1	120	
10.05	MSU Office	1	104	1	120	1	120	1	120	
10.06	CSU Office	1	97	1	120	1	120	1	120	
10.07	Captain's Office	1	133	1	150	1	150	1	150	
10.08	Chief's Office	0	208	0	180	0	0	0	0	
10.09	Conference Room	1	192	1	200	1	200	2	400	
10.09	Legal Assistant	1	240	1	80	1	80	2	160	
10.10	Evidence Processing	1	75	1	120	1	120	1	120	
10.11	Storage Room	1	245	1	240	1	240	1	240	
10.12	Explorer Office	1	107	1	120	1	120	1	120	
10.13	Patrol Room	1	365	1	300	1	300	1	300	now located at Field Operations Bureau
10.14	Patrol Storage	1	123	1	100	1	100	1	100	10 person capacity
10.15	Secure Storage	1	786	3	300	3	900	3	900	
10.16	Break Room	1	242	1	300	1	300	1	300	
10.17	Men's Restroom	1	124	1	120	1	120	1.5	180	
10.18	Women's Restroom	1	128	1	120	1	120	1.5	180	
10.19	Training/Briefing Room	1	800	1	1,000	1	1,000	1	1,000	
10.20	Physical Training Room	1	1,000	1	1,000	1	1,000	1	1,000	
10.21	Exercise Room	1	800	1	800	1	800	1	800	
10.22	Male Locker Room	0	0	1	660	1	660	1	660	
10.23	Female Locker Room	0	0	1	360	1	360	1	360	
<b>Drug Unit</b>										
10.24	Captain's Office	1	341	1	180	1	180	1	180	
10.25	Office	1	1,185	1	1,200	1	1,200	1	1,200	
10.26	Sergeant's Office	1	265	2	120	2	240	2	240	
10.27	Conference Room	1	675	1	350	1	350	1	350	
10.28	Office	1	663	1	600	1	600	1	600	
10.29	Prosecutor's Office	1	182	1	150	1	150	1	150	
10.30	Secure Storage	1	574	1	500	1	500	1	500	
10.31	Men's Restroom	1	149	1	150	1	150	1	150	
10.32	Women's Restroom	1	166	1	150	1	150	1	150	
10.33	Break Room	1	257	1	240	1	240	1	240	
10.34	Drug Unit Secure Vehicle Storage	1	2,400	0	0	0	0	0	0	
<b>Other</b>										
10.35	Dive Team Maintenance Room	1	116	1	150	1	150	1	150	
10.36	Covered Secure Vehicle Storage	1	10,600	0	0	0	0	0	0	
10.37	Enclosed Secure Vehicle Storage	1	2,555	1	1,920	1	1,920	1	1,920	
10.38	Vehicle Processing	0	0	0	500	0	500	1	500	
10.39	Secure Evidence Vault	1	491	1	500	1	500	1	500	
10.40	Secure Evidence Storage	1	491	1	500	1	500	1	500	
10.41	Cell Entry Training Room	1	69	1	70	1	70	1	70	
10.42	Evidence Tool Storage	1	87	1	100	1	100	1	100	
10.43	Telecom Vault	1	127	1	150	1	150	1	150	
<b>Exterior Space</b>										

## 10.0 Sheriff's Office

No.	Room Name/ Position	Existing			Existing to Standard			Current Need to Standard		2050 Space Projection		Additional Comments
		No.	Area (SF)	Total Area	No.	Standard Area	Total Area	No.	Total Area	No.	Total Area	
10.44	Drug Unit Secure Vehicle Storage	0	2,400	0	1	2,400		1	2,400	1	2,400	exterior space not included in total
10.45	Covered Secure Vehicle Storage	0	10,600	0	1	10,000		1	10,000	1	10,000	exterior space not included in total
SUBTOTAL NET AREA		36		28,370	36		15,280	41	15,844	60	16,904	
Circulation and Internal Walls			0.11	3,121		0.30	4,584		4,753		5,071	
TOTAL DGSP				31,491		19,864			20,597		21,975	



# Thurston County Courthouse and Civic Center Space Assessment

Olympia, Washington

## 11.0 Assessor Administration

No.	Room Name/ Position	Existing		Existing to Standard		Current Need to Standard		2050 Space Projection		Additional Comments
		No.	Area (SF)	No.	Standard Area	No.	Total Area	No.	Total Area	
11.01	Administrative Assistant	1	54	1	100	1	100	1	100	
11.02	Administration Manager BOE	1	110	1	100	1	100	2	200	
11.03	Appraisal Analyst BOE	1	60	60	80	1	80	1	80	
11.04	Appraisal Analyst - 01, 02, 03, 04	4	35	140	48	192	192	4	192	
11.05	Assessor	1	145	145	160	160	160	1	160	
11.06	Executive Assistant	1	60	60	64	64	64	1	64	
11.07	Chief Deputy Assessor	1	145	145	145	145	145	1	145	
11.08	Business Machines	1	90	90	100	100	100	1	100	
11.09	Business Application Administrator & 2 Analysts	3	35	105	3	144	144	3	144	
11.10	Commercial Appraiser	2	35	70	48	96	96	2	96	
11.11	Commercial Appraiser Analyst	1	35	35	48	48	48	1	48	
11.12	Large Shared Conference Room	1	295	295	400	132	132	0	132	20 seats; shared between Assessor and Treasurer
11.13	Small Conference Room	0	0	0	160	0	160	1	160	8 seats
11.14	Small Break-Out Meeting Rooms	0	0	0	100	0	200	2	200	now located at Field Operations Bureau
11.15	Customer Service	3	30	90	48	144	144	4	192	10 person capacity
11.16	Empty (current)/Mapping	1	35	35	48	48	48	1	48	
11.17	Break Room	1	100	100	240	240	240	1	240	
11.18	Mail Area	1	15	15	30	30	30	1	30	
11.19	Microfiche	1	45	45	45	45	45	1	45	
11.20	Personal Property Appraiser	1	60	60	64	64	64	1	64	
11.21	Senior Appraiser	8	35	280	48	336	336	7	432	
11.22	Senior Property Control Lead Analyst	1	65	65	64	64	64	1	64	
11.23	Senior Property Control Analyst	3	60	180	64	128	128	2	192	
11.24	Property Control Analyst	3	60	180	64	192	192	3	256	
11.25	Storage Files (1-12)	12	30	360	30	360	360	12	360	
11.26	Temp	1	35	35	48	48	48	1	48	
11.27	Test Workstation	1	35	35	48	48	48	1	48	
11.28	Lobby with Kiosk	1	200	200	300	300	300	1	300	share with Auditor and Assessor (300x3/3)
11.29	Staff Restrooms	0	0	0	0.67	0.67	140	0.67	140	share with Auditor and Assessor (210x2/3)
11.30	Interior Auditor	1	145	145	120	120	120	1	120	
11.31	Assessor GIS Tech III	1	110	110	120	120	120	1	120	
11.32	Shared Conference/Training Room	1	460	460	600	200	200	0.33	200	30 seats; shared with Assessor & Treasurer (subdividable)
SUBTOTAL NET AREA										
		33	3,704	33	3,988	35	4,396	38	4,720	
			0.20		0.30		1,319		1,416	
			753		1,196		5,715		6,136	
			4,457		5,184					
Circulation and Internal Walls										
TOTAL DGSE										

# Thurston County Courthouse and Civic Center Space Assessment

## Olympia, Washington

## 12.0 Auditor Administration

No.	Room Name/ Position	Existing		Existing to Standard		Current Need to Standard		2050 Space Projection		Additional Comments
		No.	Area (SF)	Total Area	No.	Standard Area	Total Area	No.	Total Area	
12.01	AVU Kiosk	1	30	30	1	30	30	1	30	accessible voting unit
12.02	Administrative Assistant	1	130	130	1	100	100	1	100	
12.03	Auditor	1	165	165	1	160	160	1	160	
12.04	Ballot Storage	1	55	55	1	55	55	1	55	
12.05	Small Conference Room	1	210	210	1	210	210	1	210	10 seats
12.06	Large Shared Conference Room	0	0	0	0.33	400	132	0.33	132	20 seats; shared between Assessor and Treasurer
12.07	Copy & Hot Trash	1	105	105	1	100	100	1	100	
12.08	Customer Service	5	75	375	5	64	320	5	320	7 448 15 year need plus 2
12.09	License and Recording Specialist	1	64	64	1	64	64	1	64	2 128
12.10	Deputy Auditor	1	210	210	1	120	120	1	120	
12.11	Elections Customer Service	1	115	115	1	100	100	1	100	
12.12	Election Supervisor	1	115	115	1	100	100	1	100	
12.13	Election Office	1	90	90	1	100	100	1	100	
12.14	Elections Manager	1	195	195	1	120	120	1	120	
12.15	Elections Specialist	2	55	110	2	64	128	2	128	
12.16	Elections Supply	1	40	40	1	40	40	1	40	
12.17	Elections Waiting	1	70	70	1	70	70	1	70	
12.18	Business Application Administration	1	110	110	1	100	100	1	100	
12.19	Break Room	1	80	80	1	240	240	1	240	kitchenette
12.20	Licensing and Records Manager	1	160	160	1	120	120	1	120	
12.21	Licensing and Supplies	1	215	215	1	200	200	1	200	
12.22	Licensing Records Supervisor	1	90	90	1	100	100	3	300	
12.23	Licensing Training & Title	1	90	90	1	100	100	1	100	
12.24	Microfiche	1	30	30	1	30	30	1	30	
12.25	Education and Outreach	1	95	95	1	100	100	1	100	
12.26	Project Office	1	60	60	1	64	64	1	64	
12.27	Recording	1	30	30	1	30	30	1	30	
12.28	Self-Help Area (5 stations)	5	24	120	5	24	120	5	120	located within lobby area
12.29	Speed Format	1	105	105	1	100	100	1	100	
12.30	Tills and Controlled Supplies	1	130	130	1	120	120	1	120	
12.31	Lobby/Waiting	1	416	416	1	300	300	1	300	share with Auditor and Assessor (300x3/3)
12.32	Staff Restrooms	0	0	0	0.67	210	140	0.67	140	share with Auditor and Assessor (210x2/3)
12.33	IT Security	0	0	0	0	0	0	0	0	combined open office
12.34	Internal Auditor	0	0	0	0	0	0	0	0	
12.35	Passport Services	1	200	200	0	0	0	0	0	
12.36	Grants Manager	1	120	120	0	0	0	0	0	in building 4
12.50	Accounts Payable Lead	1	45	45	1	64	64	1	64	
12.51	Account Analyst	1	75	75	1	64	64	1	64	
12.52	Accounts Payable Auditor	2	55	110	2	64	128	2	128	
12.53	Budget Analyst	1	75	75	1	64	64	1	64	
12.54	Shared Conference/Training Room	1	460	460	0.33	600	200	0.33	200	30 seats; shared with Assessor & Treasurer (subdividable)
12.55	Copy Area	1	70	70	1	70	70	1	70	
12.56	Department Accountant	1	75	75	1	64	64	1	64	
12.57	Financial Services Division Manager	1	115	115	1	120	120	1	120	
12.58	Financial Analyst	1	75	75	1	64	64	3	192	current need plus 2
12.59	Business Applications Administrator	1	115	115	1	100	100	1	100	

## 12.0 Auditor Administration

Election Shop									
12.75	Offices	2	242	484	2	240	480	2	480
12.76	Intake and Sorting Station	1	680	680	1	700	700	1	700
12.77	Break Room	1	286	286	1	240	240	1	240
12.78	Ballot Duplication Stations	1	200	200	1	200	200	1	200
12.79	Secure Storage Cage	1	140	140	1	150	150	1	150
12.80	Signature Checking Stations	2	210	420	2	210	420	2	420
12.81	Ballot Inspection Stations - Large	1	1,551	1,551	1	1,600	1,600	1	1,600
12.82	Ballot Inspection Stations - Small	1	183	183	1	200	200	1	200
12.83	Ballot Opening Stations	1	114	114	1	120	120	1	120
12.84	Counting Center	1	560	560	1	600	600	1	600
12.85	Secure Storage Cage	1	627	627	1	600	600	1	600
12.86	Office/Storage	1	822	822	1	800	800	1	800
12.87	Warehouse Storage	1	1,870	1,870	1	2,000	2,000	1	2,000
12.88	Staff Toilets	2	62	124	2	100	200	2	200
SUBTOTAL ELECTION SHOP NET AREA				8,061			8,110		8,110
Circulation and Internal Walls		0.31		2,478		0.25	2,028		2,028
TOTAL ELECTION SHOP DGSGF				10,539			10,138		10,138

SUBTOTAL AUDITOR NET AREA Circulation and Internal Walls TOTAL AUDITOR DGSF	23	15,097	23	13,672	25	14,000	27	14,292
		0.27	4,077	0.30	4,102	4,200		4,288
			19,174		17,368	17,794		18,174

## 13.0 Treasurer Administration

**SUBTOTAL NET AREA**  
**Circulation and Internal Walls**  
**TOTAL DGSEF**



## 14.0 Environmental Health Administration

SUBTOTAL NET AREA  
Circulation and Internal Walls  
TOTAL DGSPF



## 16.0 Development Review Administration

Notes:

1. Provide 3 vehicle parking spaces in close proximity to department; increase to 5 spaces in 15 years.
2. Need more front counter space.
3. Adjacencies: Primary - Resource Stewardship; Secondary - Commissioners

1. Provide 3 vehicle parking spaces in close proximity to department; increase to 5 spaces in 15 years.
2. Need more front counter space.
3. Adjacencies: Primary - Resource Stewardship; Secondary - Commissioners

# Thurston County Courthouse and Civic Center Space Assessment

## Olympia, Washington

# 17.0 CPED

## Community Planning and Economic Development

Room Name/ Position		Existing			Existing to Standard			Current Need to Standard		2050 Space Projection		Additional Comments
No.		No.	Area (SF)	Total Area	No.	Standard Area	Total Area	No.	Total Area	No.	Total Area	
Water Resources												
17.01	Education & Outreach Specialist III	1	24	24		48	48	1	48	1	48	
17.02	Education & Outreach Specialist II	2	40	80	2	48	96	2	96	4	192	
17.03	Comp. & Planning Coord. (Supervisor)	1	146	146	1	120	120	1	120	3	360	
17.04	Hyd Geologist Stg/Work	1	78	78	0	80	0	0	0	0	0	
17.05	Water Resource Specialist III	1	128	128	1	80	80	1	80	1	80	
17.06	Water Resource Specialist II	2	78	156	10	80	800	11	880	3	240	
17.07	Hydro Geologist	1	82	82	1	80	80	1	80	1	80	
17.08	Conference Room	1	431	431	1	400	400	1	400	1	400	
17.09	Storage Shelves	2	65	130	2	80	160	2	160	2	160	
17.10	Drying Room	1	108	108	1	180	180	1	180	1	180	
17.11	File Storage	0	160	0	0	0	0	0	0	1	0	
17.12	Small Conference Room	0	180	0	0	0	0	0	0	1	0	
Water Resources at Public Works Building												
17.15	Admin. Asst. I	1	85	85	1	80	80	1	80	1	80	
17.16	Intern	1	56	56	1	48	48	1	48	1	48	
17.17	Bus. Appl. Tech. II	1	56	56	1	64	64	1	64	1	64	
17.18	Utility Planner	1	56	56	1	64	64	1	64	1	64	
17.19	Manager	1	143	143	1	140	140	1	140	1	140	
17.20	Sr. Civil Eng.	1	140	140	1	120	120	1	120	1	120	
17.21	Utility Planner	1	82	82	1	80	80	1	80	2	160	
17.22	Water Resource Specialist I	1	87	87	1	80	80	1	80	1	80	
17.23	Reception	1	34	34	1	80	80	1	80	1	80	
17.24	Copier/Storage/Work Space	1	117	117	1	100	100	1	100	1	100	
17.25	Copier/Small Meeting Space	1	78	78	1	120	120	1	120	1	120	
17.26	Coffee Area/Storage/Coat Hanging Area	1	35	35	1	100	100	1	100	1	100	
17.27	Break Room	1	122	122	1	200	200	1	200	1	200	
17.28	Staff Toilet Rooms	0	0	0	2	160	320	2	320	2	320	
17.29	Equipment Storage Room	0	0	0	1	160	160	1	160	1	160	
17.30	Small Shop	0	0	0	1	96	96	1	96	1	96	
17.31	Resource Materials Storage	0	0	0	1	140	140	1	140	1	140	
17.32	Print & Copy Room	0	0	0	1	108	108	1	108	1	108	
17.33	Library	0	0	0	1	120	120	1	120	1	120	
17.34	Vacant	7	0	0	0	0	0	0	0	0	0	
Resource Stewardship												
17.40	Admin I	2	105	210	2	100	200	2	200	2	200	Bldg. 1, Level 2
17.41	Fire Inspector / Admin I	1	105	105		0	0		0			
17.42	Break Room	1	106	106	1	100	100	1	100	1	100	
17.43	Work Room	1	121	121	1	120	120	1	120	1	120	
17.44	Building Inspector II	4	56	224	3	64	192	4	256	4	256	
17.45	Permit Tech II / Building Inspector II	1		0		0	0		0		0	
17.46	Permit Tech. I	1	58	58	3	48	144	3	144	3	144	
17.47	Permit Tech. II	1	1190	1190	1	1,400	1400	1	1400	1	1,400	
17.48	Planning Technician	2	63	126	1	64	64	1	64	1	64	
17.49	Conference Room	1	180	180	1	180	180	1	180	1	180	
17.50	Senior Planner	3	106	318	2	100	200	2	200	2	200	
17.51	Associate Planners	8	125	1000	2	80	160	2	160	2	160	



# Thurston County Courthouse and Civic Center Space Assessment

Olympia, Washington

17.0 CPED  
Community Planning and Economic Development

No.	Room Name/ Position	Existing		Existing to Standard		Current Need to Standard		2050 Space Projection		Additional Comments
		No.	Area (SF)	No.	Standard Area	No.	Total Area	No.	Total Area	
17.52	Planning Manager	1	110	1	120	1	120	1	120	
17.53	GeoData Planner	1		0		0	0	0	0	
17.54	Storage	1	193	1	200	1	200	1	200	
17.55	Storage	1	39	39	40	1	40	1	40	
17.56	Storage (office products)	1	55	55	100	1	100	1	100	
17.57	Commercial Planning Supervisor	1	113	113	120	2	240	2	240	
17.58	Commercial Plans Coordinator	2		0		0	0	0	0	
17.59	Intern	1		0		0	0	0	0	
17.60	Copy / Storage Room + Plot/Scan	1	142	142	140	1	140	1	140	
17.61	Sleep Room	1	64	64	64	1	64	1	64	
17.62	Break Room	1	155	155	300	1	300	1	300	
17.63	Assistant Planner	2	94	188	2	80	160	2	160	
17.64	Associate Planner	4	96	384	4	80	320	4	320	
17.65	Resource Stewardship Directors Office	1	186	186	1	180	180	1	180	
17.66	Planning Manager	1	89	89	1	100	100	1	100	
17.67	Senior Planner	2	89	178	1	100	100	1	100	
17.68	Operations manager	1	90	90	1	100	100	1	100	
17.69	Compliance Supervisor	1	89	89	1	100	100	1	100	
17.70	Permit Tech. Supervisor	1	90	90	1	100	100	1	100	
17.71	Resource Stewardship I.T.	1	103	103	1	80	80	1	80	
17.72	Compliance Coordinator	2	103	206	1	80	80	1	80	
17.73	Budget Commissioners	1	86	86	1	80	80	1	80	
17.74	Conference Room	1	278	278	1	400	400	1	400	sized for 24
17.75	Fiscal Manager	1	95	95	1	100	100	1	100	
17.76	Fiscal Analyst	1	87	87	1	100	100	1	100	
17.77	Public Information Officer (PIO)	1	64	64	1	100	100	1	100	
17.78	Staff Restrooms	0	0	0	2	160	320	2	320	
17.79	Drying Room (for field staff)	0	0	0	1	120	120	1	120	
17.80	Shower Rooms (for field staff)	0	0	0	2	100	200	2	200	
17.81	Wellness Room	0	0	0	1	240	240	1	240	
17.82	Small Conference Room	0	0	0	0	0	0	0	0	
17.83	Resources Library Room	0	0	0	0	0	0	0	0	
17.84	Flex Room/Space	0	0	0	30	0	0	0	0	
17.85	Admin II	2	64	128	2	0	0	0	0	
17.86	Permit Tech. III	1	0	0	0	0	0	0	0	
17.87	Plan Examiner II	2	0	0	0	0	0	0	0	
SUBTOTAL NET AREA		71		9,304	61		11,008	62	11,272	11,138
Circulation and Internal Walls			0.13	1,210			3,302		3,382	3,341
TOTAL DGSE				10,514			14,310		14,654	14,479

- Notes:
1. Provided space for (12) vehicles (pickups, vans) and (1) small utility trailer directly adjacent to office space.
  2. In 15 years, add 5 vehicles to above total. In 30 years, add 4 more vehicles to above total.
  3. Additional boats & trailers, beyond those referenced above, are stored at other County facilities.
  4. Field staff should be close to building exit and vehicles.
  5. Adjacencies: Primary - near long range planning and development review groups.
  6. In addition, ideal location would be close to Environmental Hearth (@ Lilly Rd) and/or Public Works (@ Tilly Rd).

**Thurston County Courthouse and Civic Center Space Assessment**  
Olympia, Washington

**18.0 Information Technology (IT) Administration**

No.		Room Name/ Position		Existing			Existing to Standard			Current Need to Standard			2050 Space Projection			Additional Comments	
		Information Technology		No.	Area (SF)	Total Area	No.	Standard Area	Total Area	No.	Total Area	No.	Total Area				
20.19		Network Supervisor		1	102	102	1	100	100	1	100	1	100	1	100	Private Office (confidentiality/heavy phones) - Desktop Supv	
20.20		Telecom Phone Support/Operations		2	150	300	2	80	160	2	160	3	240	3	240	80 sf workstations; 2 (current); add 1 FTE at 30 years	
20.21		Telecom Storage Area		0	0	0	0	1	40	40	1	40	1	40	1	40	40 sf storage area
20.22		Application		4	70	280	4	64	256	4	256	6	384	6	384	Currently 4. Add 1 (64 sf ea) at 15 years; add 1 at 30 years	
20.23		IT Security		1	89	89	1	100	100	1	100	4	400	4	400	Standard workstation	
20.24		Project Manager		1	87	87	1	64	64	1	64	3	192	3	192	Private Office (confidentiality/heavy phones) - Applications Supv	
20.25		Applications Supervisor		1	104	104	1	100	100	1	100	1	100	1	100	Private Office (confidentiality/heavy phones) - Applications Supv	
20.26		Application		2	70	140	2	64	128	2	128	4	256	4	256	New 2 FTEs. Add 1 FTE (64 sf ea) at 15 years; add 1 FTE at 30 years. Co-locate with rest of Apps.	
20.27		Data Room/ Storage+Bench		1	980	980	1	1,000	1,000	1	1,000	1	1,000	1	1,000	With virtualization/cloud technology, do not foresee major increase in data center space	
20.28		IT Desktop		7	67	469	7	64	448	7	448	11	704	11	704	Currently 7. Add 2 (64 sf ea); Add 2 (64 sf ea) at 30 yrs.	
20.30		Card Key Photo Station		0	73	0	0	64	0	1	64	1	64	1	64	Currently in CPS-Reception	
20.31		IT Manager		1	134	134	1	120	120	1	120	1	120	1	120	Private Office 120 sf	
20.32		Office Assistant to IT Manager		0	0	0	0	64	0	0	0	1	64	1	64	Add 64 sf for Office Asst workstation	
20.33		Network Technician		4	329	1,316	3	64	192	3	192	6	384	6	384	Currently 4 FTEs (64 sf ea); Add 2 FTE (64 sf) at 30 years; Should be co-located with rest of Network team	
20.34		Conference Room		0	0	0	0	300	0	1	260	1	300	1	300	Shared work	
20.35		Conference Room		0	0	0	0	150	0	0	1	150	1	150	1	150	Online in 1-year
20.36		Business Application		0	0	0	0	64	0	6	64	6	64	6	64	Online in 1-year	
20.37		Lunch/Break counter		0	0	0	0	120	0	0	0	1	120	1	120		
20.38		Printer Area-Central		0	0	0	0	64	0	0	0	1	64	1	64		
20.39		File Area		0	0	0	0	24	0	0	0	1	24	1	24	1-2 Lateral files	
20.40		IT Department Director		1	90	90	1	100	100	1	100	1	100	1	100		
20.41		Work Room		1	240	240	1	300	300	1	300	1	300	1	300		
Subtotal				23		4331	24		2708	24	3032	35	4,348				

**Thurston County Courthouse and Civic Center Space Assessment** **18.0 Information Technology (IT)**  
 Olympia, Washington Administration

Room Name/ Position		Existing		Existing to Standard		Current Need to Standard		2050 Space Projection		Additional Comments	
No.		No.	Area (SF)	Total Area	No.	Standard Area	Total Area	No.	Total Area		
GIS											
18.01	Reception	1	56	56	1	60	60	1	60	1	60
18.02	Plotter Area	1	56	56	1	60	60	1	60	1	60
18.03	Copy Area	1	35	35	1	40	40	1	40	1	40
18.04	GIS Supervisor	1	130	130	1	140	140	1	140	1	140
18.05	Analyst I	0	0	0	0	64	0	3	192	2	128
18.06	Analyst II	4	82	328	4	64	256	4	256	5	320
18.07	Analyst III	3	100	300	3	84	252	3	252	4	336
18.08	Intern	1	42	42	1	48	48	1	48	1	48
18.09	Flex Station	0	0	0	0	60	0	1	60	2	120
18.10	Break Room	1	95	95	1	120	120	1	120	1	120
18.11	Conference Room	0	0	0	0	120	0	1	120	1	120
	Subtotal	9		1,042	9		976	13	1,348	15	1,492
SUBTOTAL NET AREA											
32		0.15	5,373	33		3,684	37	4,380	50	5,840	
Circulation and Internal Walls											
			819		0.30	1,105		1,314		1,752	
Information Technology (IT) TOTAL DGSF											
			4,553			4,291		4,774		5,291	

- Notes:
1. Projected FTE increases at 15 years: 5 in Facilities; 3 in Admin; 5 in IT
  2. Projected FTE increases at 30 years: 3 in Facilities; 2 in Admin; 4 in IT; and 1 in CPM
  3. Co-locate GeoData (Bldg. 4) with IT (1 Supervisor @ 100 sf, 7 FTEs @ 80 sf ea; workshop @ 100 sf; public reception counter @100 sf) for 860 sf + Circulation 300 sf = total 1,160 sf
  4. Fleet satellite ops at Courthouse (350 sf workshop/reception space+10 parking spaces); Sustainability office 100 sf
  5. Assume more open plan for groups of workstations to achieve more efficient circulation and use of space

## 19.0 Human Resources Administration

1. Some autonomy to this department is preferred. Provide screened entry.
2. Adjacencies: Primary - near financial services. Should be located near the rest of the campus.
3. Not too close to Commissioner's office, it can be a bit intimidating for staff.
4. Labor negotiation meetings 2-3 weeks-2x per year. Two separate caucus
5. Civil Service testing for sheriff 1-2 days per quarter, 4-5 rooms at 4-6 people per room.

# Thurston County Courthouse and Civic Center Space Assessment

Olympia, Washington

## 20.0 Central Services

Future Space Summary

No.	Room Name/ Position	Existing		Existing to Standard		Current Need to Standard		2050 Space Projection		Additional Comments
		No.	Area (SF)	No.	Standard Area	Total Area	No.	Total Area	No.	Total Area
	<b>Administration</b>									
20.01	Large Conference Room	1	257	1	220	220	1	220	1	220
20.02	Small Conference Room	0	0	0	100	0	1	100	3	300
20.03	Mail Room	1	272	1	300	300	1	300	1	300
20.04	Work Room	1	286	1	300	300	1	300	1	300
20.05	Work Room Support Space	0	0	0	50	0	0	0	2	100
20.06	Administrative Manager	1	147	1	120	120	1	120	1	120
20.07	Office Assistant to Admin. Mgr.	0	0	0	64	0	0	0	1	64
20.08	Directors Office	1	150	1	120	120	1	120	1	120
20.09	Executive Assistant to Director/Mgt. Analyst	0	0	0	80	0	0	0	1	80
20.10	Account Assistant	3	87	3	80	240	3	240	5	400
20.11	Account Supervisor	1	133	1	100	100	1	100	1	100
20.12	Reception Analyst & Reception Asst.	2	88	2	80	160	2	160	2	160
20.13	Capital Project Managers (CPM)	2	93	2	120	240	2	240	3	360
20.14	Admin. Assistants to Capital Project Mgrs.	2	108	2	64	128	2	128	3	192
	Subtotal	12	2,084			1,928	12	2,028	18	2,816
	<b>Facilities</b>									
20.15	Shop	1	462	1	600	600	1	600	1	600
20.16	Storage Room	1	152	1	300	300	1	300	1	300
20.17	Facilities Workroom	1	240	1	300	300	1	300	1	300
20.18	Facilities Manager	1	89	1	120	120	1	120	1	120
	Subtotal	1	943	1		1,320	1	1,320	1	1,320



## 20.0 Central Services

Notes:

**City of Olympia Justice Center**  
Olympia, Washington

**21.0 Municipal Court**  
Future Space Summary

No.	Room Name/ Position	Existing		Existing to Standard		Current Need to Standard		2050 Space Projection		Additional Comments
		No.	Area (SF)	Total Area	No.	Standard Area	Total Area	No.	Total Area	
	<i>Municipal Court</i>									
21.01	Court Public Waiting	0	0	0	2	160	320	2	320	Judge Scott Ahlf 6-8 seats for waiting; in public lobby (see Building Support)
21.02	Courtroom	2	1,400	2,800	2	1,700	3,400	2	3,400	50 spectators, 7 p. jury box, judge, clerk, witness stand, 2 counsel tables
21.03	Soundlock Entry Vestibule	0	0	0	2	80	160	4	320	
21.04	Attorney Client Conference Rooms	1	149	149	4	100	400	2	200	For public use
21.05	Court Waiting	0	0	0	2	240	480	2	480	
21.06	AV / IT Technology	0	0	0	1	80	80	1	80	
21.07	Jury Deliberation Room	0	0	0	1	200	200	1	200	7-person
21.08	Juror Toilets	0	0	0	1	50	50	1	50	
21.09	Soundlock Vestibule	0	0	0	1	50	50	1	50	
21.10	Staging - In-Custody Defendants	0	0	0	1	80	80	1	80	1 per pair of courtrooms
21.11	Judge	1	136	136	1	180	180	1	180	Office now rm. 216 in Bldg. 3; should be located with staff; small conf. table with 4 chairs; secure files
21.12	Toilet	0	0	0	0	80	0	1	80	Open work area with table & chairs in open office area
21.13	Pro-Tem Judge/Commissioner	1	0	0	1	120	120	1	120	
21.14	Conference Room	0	0	0	0	400	0	1	400	12-15 people
21.15	Clerk Windows	2	0	0	2	70	140	2	140	windows to lobby
21.16	Clerk Workstations	0	0	0	0	50	0	2	100	
21.17	Courtroom Clerk Office	1	0	0	1	120	120	1	120	
21.18	Court Administrator	1	0	0	1	120	120	1	120	
21.19	Court Operations Manager	1	0	0	1	120	120	1	120	
21.20	Community Court Case Manager	1	0	0	1	120	120	1	120	use visiting office at community court when req'd.
21.21	File Storage	0	0	0	1	180	180	1	180	printer/copier/scanner, work counter, supply storage
21.22	Copy/Supply/Work Room	0	0	0	1	120	120	1	120	
21.23	Staff Restrooms	0	0	0	2	120	240	1	120	access to staff toilets on floor or within office
21.24	Break Room	0	0	0	1	180	180	1	180	required if booking/jail located elsewhere
21.25	Officer Workstation	0	0	0	0	50	0	1	50	
21.26	Holding Cells	0	0	0	0	70	0	2	140	1-3 capacity
21.27	Large Group Holding Cell	0	0	0	0	120	0	2	240	8-10 capacity
21.28	Staff Work Area	0	0	0	0	120	0	1	120	with kitchenette
21.29	Storage Room	0	0	0	0	60	0	1	60	
	Subtotal	8		3,085	8		6,860	10	7,280	
								13	7,500	
	<i>City Prosecutor</i>									Rocio Ferguson
21.30	Reception Area	0	0	0	1	120	120	1	120	
21.31	Paralegal	1	0	0	1	48	48	1	48	handles reception duties
21.32	Intern Workstations	1	0	0	1	36	36	3	108	victim assistance; paralegal, Rule 9 intern/extern
21.33	Victim Assistance	1	0	0	1	100	100	1	100	
21.34	Interview Rooms	0	0	0	2	100	200	2	200	adjacent to reception area
21.35	Chief Prosecutor	1	0	0	1	120	120	1	120	
21.36	Assistant Prosecutor	1	0	0	1	120	120	1	120	
21.37	Assistant Prosecutor II	0	0	0	0	120	0	1	120	

**City of Olympia Justice Center**  
Olympia, Washington

**21.0 Municipal Court**  
Future Space Summary

No.		Room Name/ Position	Existing			Existing to Standard			Current Need to Standard		2050 Space Projection		Additional Comments
No.	Area (SF)	Total Area	No.	Standard Area	Total Area	No.	Total Area	No.	Total Area	No.	Total Area		
21.38		File Storage	0	0	0	1	100	100	1	100	1	100	
21.39		Break Room	0	0	0	1	120	120	1	120	1	120	
21.40		Workroom	0	0	0	1	100	100	1	100	1	100	
21.41		Short Term Evidence Storage	0	0	0	1	80	80	1	80	1	80	
21.42		Staff Toilets	0	0	0	2	50	100	2	100	2	100	
		Subtotal	5			5		1,244	8	1,436	9	1,536	
		Community Court											
21.50		Security Queuing	0	0	0	0	0	0	0	0	0	0	See Building Support
21.51		Security Screening	0	0	0	0	0	0	0	0	0	0	See Building Support
21.52		Multi-Purpose/Provider Booths	0	0	0	1	1,600	1,600	1	1,600	1	1,600	12 booths
21.53		Meeting Rooms	0	0	0	4	100	400	7	700	8	800	confidential interviews with providers
21.54		Waiting Area	0	0	0	1	300	300	1	300	1	300	20 seats
21.55		Prosecutor Office / Meeting Room	0	0	0	1	120	120	1	120	1	120	
21.56		Public Defender Office / Meeting Room	0	0	0	1	120	120	1	120	1	120	
21.57		Children's Play Area	0	0	0	1	80	80	1	80	1	80	
21.58		Staff Restrooms	0	0	0	2	50	100	2	100	2	100	
21.59		Public Restrooms	0	0	0	0	120	0	0	0	0	0	in public lobby
21.60		Kitchen	0	0	0	1	120	120	1	120	1	120	
21.61		Therapeutic Garden	0	0	0	1	0	0	1	0	0	0	outdoor garden
21.62		Gardening Shed	0	0	0	1	80	80	1	80	1	80	
21.63		Visiting Office	0	0	0	1	120	120	1	120	2	240	visiting offices for court staff
		Subtotal	0			0		3,040	0	3,340	0	3,560	
		Probation Services											Monica Schneider, supervisor
21.65		Waiting Area	0	0	0	1	120	120	1	120	1	120	
21.66		Front Counter / Check-In	0	0	0	1	50	50	1	50	1	50	window to waiting area
21.67		Program Manager	1	0	0	1	120	120	1	120	1	120	windows for visual supervision
21.68		Probation Officers	1	0	0	1	120	120	1	120	3	360	
21.69		Jail Alternatives Coordinator	0	0	0	1	120	120	1	120	1	120	
21.70		Program Specialist	0	0	0	1	120	120	1	120	1	120	
21.71		Work Crew Supervisors	0	0	0	2	48	96	2	96	2	96	
21.72		File/Supply/Copy Room											
21.73		Break Room											
21.74		Classroom/Meeting Room	0	0	0	1	400	400	1	400	1	400	20 people; DUI impact panel
21.75		Urinalysis Room	0	0	0	1	60	60	1	60	1	60	
21.76		Storage for Electronic Monitoring Equipment	0	0	0	1	100	100	1	100	1	100	
21.77		Electronic Monitoring Equipment Fitting	0	0	0	1	80	80	1	80	1	80	
21.78		Staff Toilets	0	0	0	2	50	100	2	100	2	100	
21.79		Secure Van Parking	0	0	0	0	0	0	0	0	0	0	Work crew
21.80		Work Crew Storage	0	0	0	1	150	150	1	150	1	150	
21.81		Work Crew Toilets/Showers	0	0	0	2	100	200	2	200	2	200	
		Subtotal	2			2		1,836	2	1,836	4	2,076	

**City of Olympia Justice Center**  
Olympia, Washington

**21.0 Municipal Court**  
Future Space Summary

No.	Room Name/ Position	Existing		Existing to Standard		Current Need to Standard		2050 Space Projection		Additional Comments
		No.	Area (SF)	Total Area	No.	Standard Area	Total Area	No.	Total Area	
	<i>Public Defense Coordinator</i>									
21.82	Reception Area	0	0	0	1	120	120	1	120	<i>Diane Wahley, coordinator</i>
21.83	Coordinator	1	0	0	1	120	120	1	120	
21.84	Social Worker	1	0	0	1	48	48	1	48	
21.85	Intern Workstation	0	0	0	1	36	36	1	36	
21.86	Interim Work Area	0	0	0	0	150	0	1	150	carrels for use of 5 contract attorneys
21.87	Meeting Room	0	0	0	1	100	100	1	100	2 to 3 people
21.88	Witness Interview Room	0	0	0	0	0	0	1	0	use attorney/client conference room at courtroom
21.89	Open File Storage	0	0	0	1	50	50	1	50	
21.90	Evidence Storage	0	0	0	0	50	0	1	50	
21.91	Supply Storage	0	0	0	0	50	0	1	50	
21.92	Copier/Work Area	0	0	0	0	50	0	1	50	
21.93	Staff Toilets	0	0	0	1	50	50	1	50	
	Subtotal	2			2		404	2	824	
	<b>Subtotal Courts / Offices</b>				17		13,384	22	14,716	
	DGSF Grossing Factor		0.35				4,684		5,424	
	<b>Subtotal DGSF</b>						18,068		20,920	
	<i>Olympia City Police / Holding</i>									
22.00	Vehicle Sallyport	0	0	0	1	800	800	1	800	<i>Chandra Brady, support administrator</i>
22.01	Secure Vestibule	0	0	0	1	80	80	1	80	drive-thru, enclosed, two-lane, decontamination shower; gun lockers
22.02	Breathalyzer Testing (BAT)	0	0	0	1	120	120	1	120	between vehicle sallyport and booking
	<i>Booking / Reception</i>									access off vehicle sallyport
22.03	Search Room	0	0	0	1	70	70	1	70	
22.04	Police Work Area	0	0	0	2	50	100	2	100	completion of arrest reports
22.05	Open Waiting	0	0	0	12	20	240	12	240	
22.06	Arrestee Toilets	0	0	0	2	50	100	2	100	for open seating area
22.07	Booking Counter	2	0	0	2	80	160	2	160	
22.08	ID Station	0	0	0	1	100	100	1	100	AFIS, digital photo, ink print
22.09	Copier/Work Area	0	0	0	1	65	65	1	65	
22.10	Single Isolation Cells	0	0	0	2	72	144	2	144	ss. combi units; 2 ADA; per Chapter 289-12 WAC
22.11	Single Holding Cells	0	0	0	2	72	144	2	144	
22.12	Small Group Holding Cells	0	0	0	2	110	220	2	220	ss. combi units; 6 capacity
22.13	Large Group Holding Cells	0	0	0	1	180	180	1	180	ss. combi units; 12 capacity
22.14	Detox Holding Cells	0	0	0	2	180	360	2	360	ss. combi units; 8 capacity
22.15	Sergeant's Office	1	0	0	1	120	120	1	120	
22.16	Warrants Clerk	1	0	0	1	80	80	1	80	
22.17	Warrants Files	0	0	0	1	120	120	1	120	
22.18	Warrants Work Area	0	0	0	1	80	80	1	80	
	<i>Property</i>									
22.19	Property Exchange	0	0	0	1	150	150	1	150	change booths, showers; schedule by gender
22.20	Property Room	0	0	0	1	350	350	1	350	
22.21	Issue Storage	0	0	0	1	80	80	1	80	





## 21.0 Municipal Court

[illegible]

Thurston County Courthouse and Civic Center Space Needs Assessment  
Olympia, Washington

Parking Summary

Staff Parking				
No.	Department	Existing Need	Current Need	2050
Courthouse				
1.00	Courthouse Lobby + Public Space	9	12	12
2.00	Superior Court	23	31	41
3.00	Clerk	30	35	44
4.00	District Court	30	32	51
6.00	Sheriff	6	7	8
	Subtotal Courthouse	98	117	156
Court-Related Offices/Agencies				
6.00	Prosecutor	69	76	108
7.00	Office of Assigned Counsel	33	39	61
8.00	Drug Court (outside courthouse)	8	7	7
	Subtotal Court Related	110	122	176
City of Olympia				
21.00	Municipal Court	8	10	13
21.30	City Prosecutor	5	8	9
21.50	Community Court	-	-	-
21.65	Probation Services	2	2	4
21.82	Public Defense Coordinator	2	2	2
22.00	Olympia City Police / Holding	-	12	12
22.75	Building Support	1	2	2
	Subtotal City of Olympia	18	36	42
	Reduction Factor	0.85	0.85	0.85
Total Staff Parking Downtown				
		208	239	333
Total Factored Staff Parking Downtown				
		187	215	299

Includes 5 County pool car spaces

Includes 5 County pool car spaces

# Thurston County Courthouse and Civic Center Space Needs Assessment Olympia, Washington

## Parking Summary

Visitor Parking				
No.	Department	Existing Need	Current Need	2050
<i>Courthouse</i>				
1.00	Courthouse Lobby + Public Space			
	Attorney Work Area	4	4	4
2.00	Law Enforcement Work Area	4	4	4
	Superior Court	0	0	0
	Jury Call	80	96	160
	Spectators - typical courtroom	75	90	150
	Spectators -large courtroom	40	40	40
	Attorneys	12	14	44
	Drug Court	2	2	2
	Law Library/Pro Se Center	15	18	18
	Pretrial	3	4	6
3.00	Clerk	8	10	14
4.00	District Court	0	0	0
	Spectators - typical courtroom	60	90	120
	Attorneys	8	12	16
	District Court Office	12	18	30
	Settlement Conference	0	0	9
	Probation	0	0	0
	Mental Health/Veterans Court	4	4	4
5.00	Sheriff	0	0	0
	Subtotal Courthouse	327	406	621
<i>Court-Related Offices/Agencies</i>				
6.00	Prosecutor	14	15	26
7.00	Office of Assigned Counsel	16	19	30
8.00	Drug Court (outside courthouse)	26	26	26
	Subtotal Court Related	56	60	82
<i>City of Olympia</i>				
21.00	Municipal Court			
	Lobby/Waiting	8	13	13
	Jury Call	40	45	45
	Spectators	33	33	33
	Clerk Visitors	10	10	10
21.30	City Prosecutor	4	4	4
21.50	Community Court			
	Agencies	7	7	10
	Clients	13	13	16
21.65	Probation Services	14	14	14
21.82	Public Defense Coordinator	4	4	5
22.00	Olympia City Police / Holding	6	6	10
22.75	Building Support	1	1	1
	Subtotal City of Olympia	140	150	161
	Total Visitor Parking Downtown	523	616	864
	Total Visitor Parking Downtown	444	524	734

78%

# Thurston County Courthouse and Civic Center Space Needs Assessment Olympia, Washington

## Parking Summary

Courthouse Service Areas				
No.	Department	Existing Need	Current Need	2045
<i>Thurston County Service Areas</i>				
	Vehicle Sallyport (2 lane)			
	Bus	1	1	1
	6 Transport Car and Van Spaces	6	6	6
	Sheriff's Vehicles	6	6	6
	Loading Dock			
	Loading Bay	1	1	1
	Trash Bay	1	1	1
	Recycling	1	1	1
	Service Vehicles	2	2	2
	<i>City of Olympia Service Areas</i>			
	Vehicle Sallyport (2 lane)			
	2 Patrol Car or Van Spaces	2	2	2
	Probation Work Crew Vans	2	2	2
	Police Vehicles	6	6	6
	Loading Dock			
	Loading Bay	1	1	1
	Trash Bay/Recycling Bay	1	1	1
	Service Vehicles	2	2	2
Court Service Vehicles		32	32	32

Drive Thru  
1 bus or 2 vans

### PARKING FOR SITE OUTSIDE DOWNTOWN\*

Total Parking at Courthouse	425	523	777
Total Parking for County Offices	166	182	259
<b>SUBTOTAL PARKING</b>	<b>590</b>	<b>705</b>	<b>1,036</b>
City of Olympia Parking	158	186	203
<b>TOTAL PARKING</b>	<b>749</b>	<b>891</b>	<b>1,239</b>

### PARKING FOR DOWNTOWN SITE NEAR TRANSIT CENTER\*

Total Parking at Courthouse	382	471	699
Total Parking for County Offices	141	155	220
<b>TOTAL PARKING</b>	<b>523</b>	<b>625</b>	<b>919</b>
City of Olympia Parking	143	168	182
<b>TOTAL PARKING</b>	<b>665</b>	<b>793</b>	<b>1,102</b>

### PARKING ON A GSF BASIS PER CITY ORDINANCE\* (Does not reflect court assembly use)

Courthouse GSF	57,773	119,816	158,390
Courthouse Parking	202	419	554
County Offices GSF	111,521	114,758	137,738
Court Related Agencies Parking	390	402	482
<b>SUBTOTAL COUNTY PARKING</b>	<b>593</b>	<b>821</b>	<b>1,036</b>
City of Olympia GSF	-	43,741	45,057
City of Olympia Parking	0	153	158
<b>TOTAL PARKING</b>	<b>593</b>	<b>974</b>	<b>1,194</b>

\* Does not include Courthouse Service Vehicles

# Thurston County Courthouse and Civic Center Space Needs Assessment Olympia, Washington

## Parking Requirements

Existing	Existing Spaces
<b>Lot</b>	
Public	
Lot A	50
Lot B	84
Lot C	23
<b>Subtotal Public</b>	<b>157</b>
Staff & Jurors	
Lot D	54
Lot E	29
Lot F	16
Lot G	122
Berry Patch Lot	48
Lot H	6
Lot I	25
Lot J	23
Lot K	19
Building 4 Lot	29
Building 5 Lot	48
Building 6 Lot	26
<b>Subtotal Staff &amp; Jurors</b>	<b>445</b>
<b>TOTAL OFF STREET PARKING</b>	<b>602</b>
On-Street Parking	46
<b>TOTAL PARKING AVAILABLE</b>	<b>648</b>

Proposed	Current Need	2030	2045
<b>Lot</b>			
Public			
Courthouse	194	230	258
Jurors	107	127	142
Court-Related Agencies	54	60	67
Administrative Offices	65	72	78
Sheriff	18	21	24
<b>Subtotal Public</b>	<b>438</b>	<b>510</b>	<b>568</b>
Staff			
Courthouse	108	128	143
Court-Related Agencies	111	137	151
Administrative Offices	195	202	233
Sheriff	65	69	71
<b>Subtotal Staff</b>	<b>479</b>	<b>535</b>	<b>598</b>
<b>TOTAL OFF STREET PARKING</b>	<b>917</b>	<b>1,045</b>	<b>1,166</b>
On-Street Parking	36	36	36
<b>TOTAL PARKING AVAILABLE</b>	<b>953</b>	<b>1,081</b>	<b>1,202</b>

### Service Parking

Secure Parking	22
Courthouse	2
Court-Related Agencies	10
Administrative Offices	3
Sheriff	37
Subtotal Secure Parking	12,500 SF
Sheriff's Covered Parking	6 outdoor spaces
Courthouse Vehicle Sallyport	as needed
Service Parking	as needed
Loading Docks	as needed



EXISTING CONDITIONS FLOOR PLANS

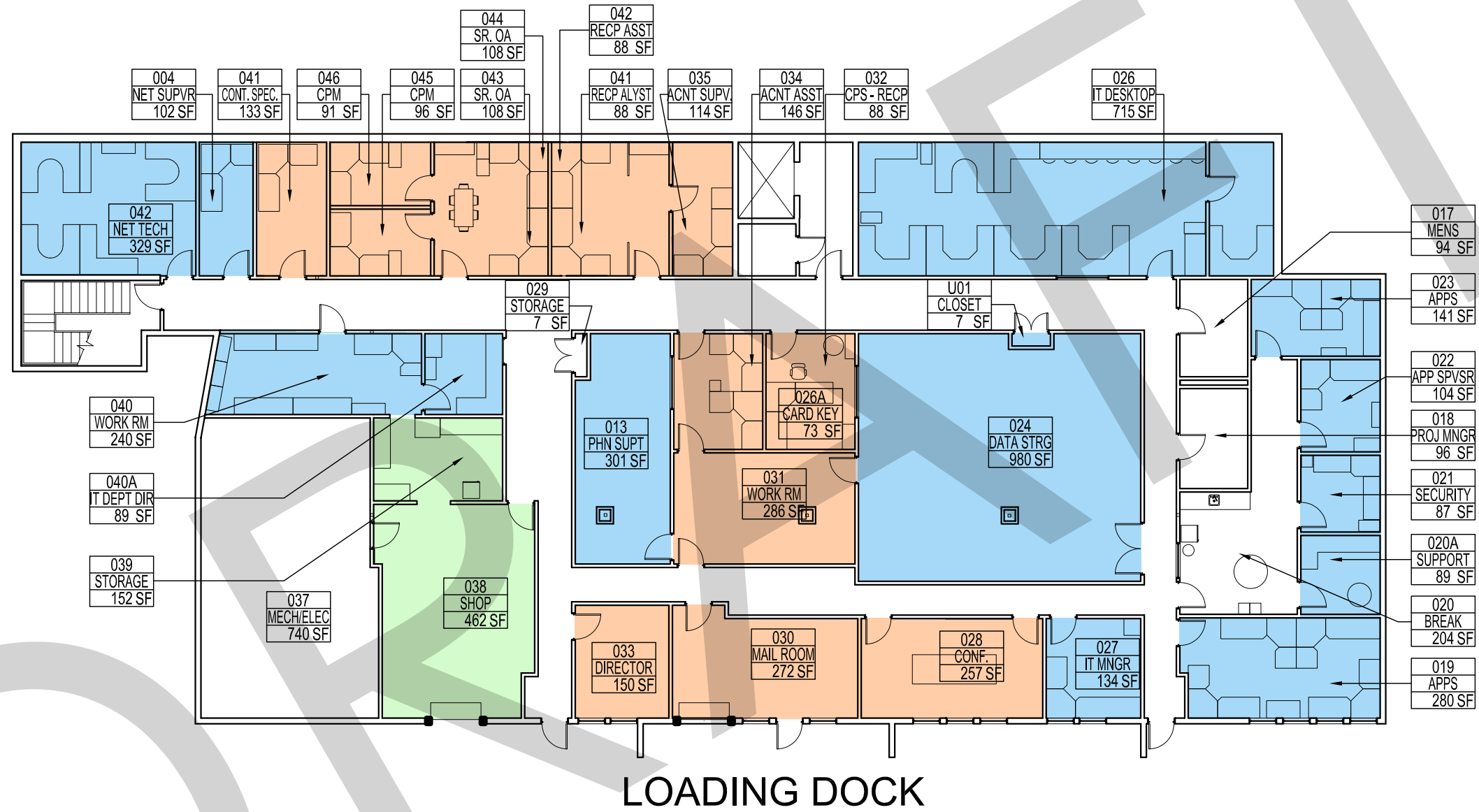
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2000 LAKE RIDGE DR SW, OLYMPIA, WASHINGTON 98502

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SEPTEMBER 28, 2018

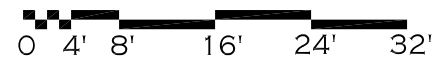
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**A1.01**

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**BUILDING 1 - BASEMENT**  
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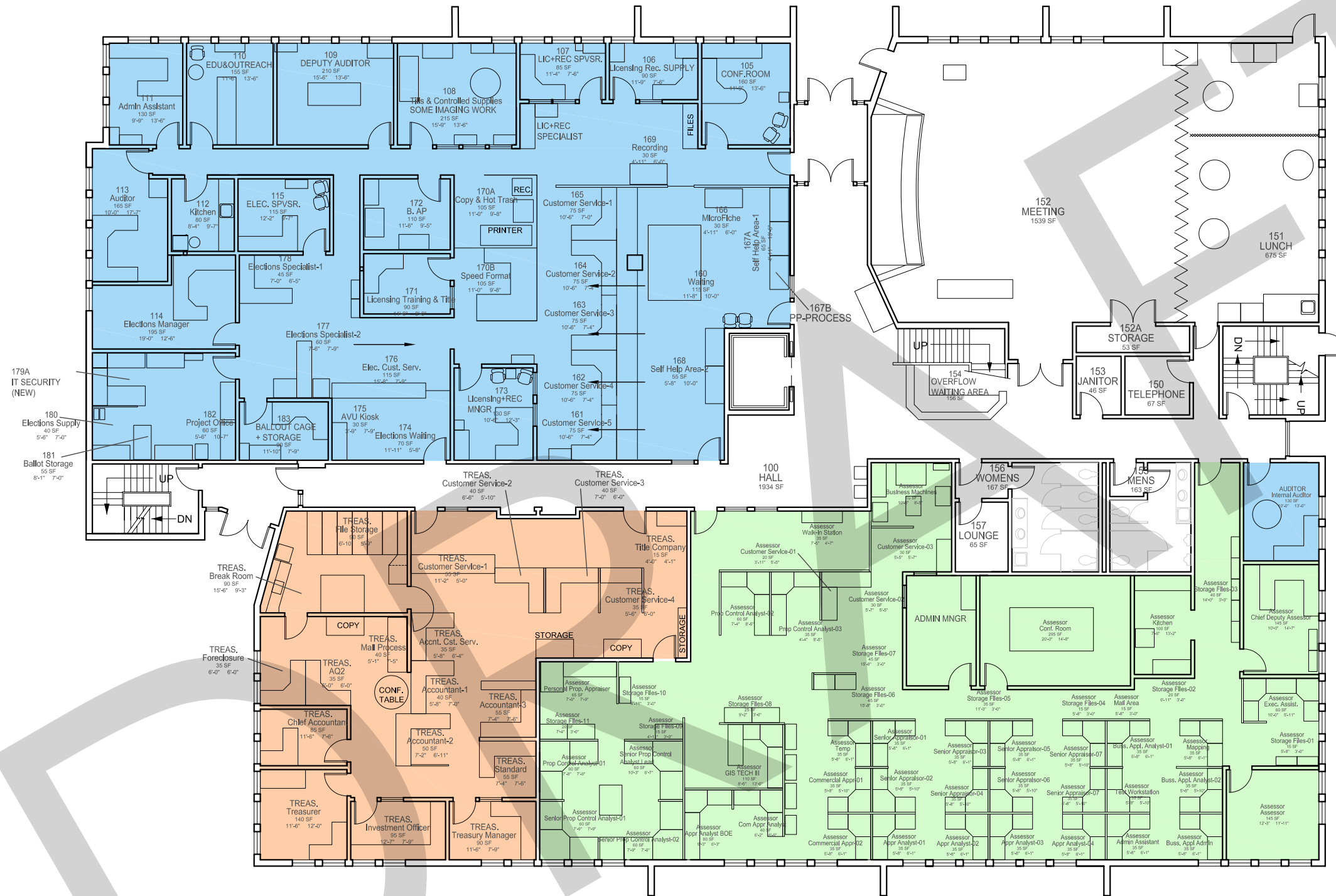


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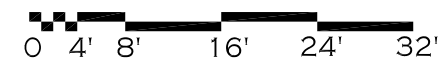
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1  
A1.10  
BUILDING 1 - LEVEL 1 - FLOOR PLAN  
1/16"=1'-0"

- = AUDITOR
- = ASSESSOR
- =TREASURER



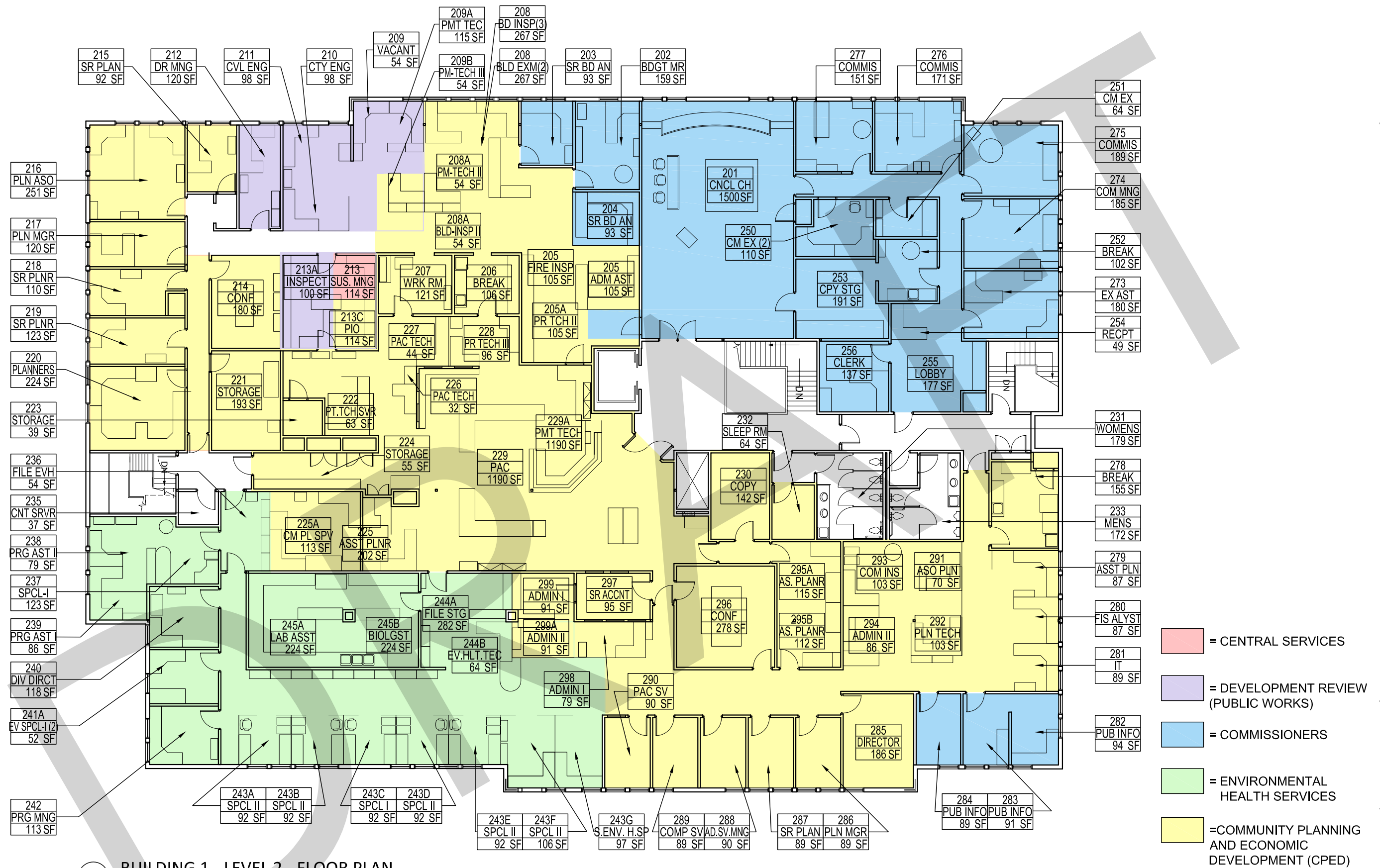
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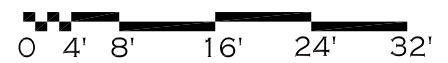
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FLOOR PLAN**

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**BUILDING 1 - LEVEL 2 - FLOOR PLAN**  
1  
A1.20  
1/16"=1'-0"



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**EXISTING  
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1  
A2.10  
**BUILDING 2 - LEVEL 1 - FLOOR PLAN**  
1/16"=1'-0"

0 4' 8' 16' 24' 32'

- = PRE-TRIAL
- = SUPERIOR COURT
- = CLERKS OFFICE



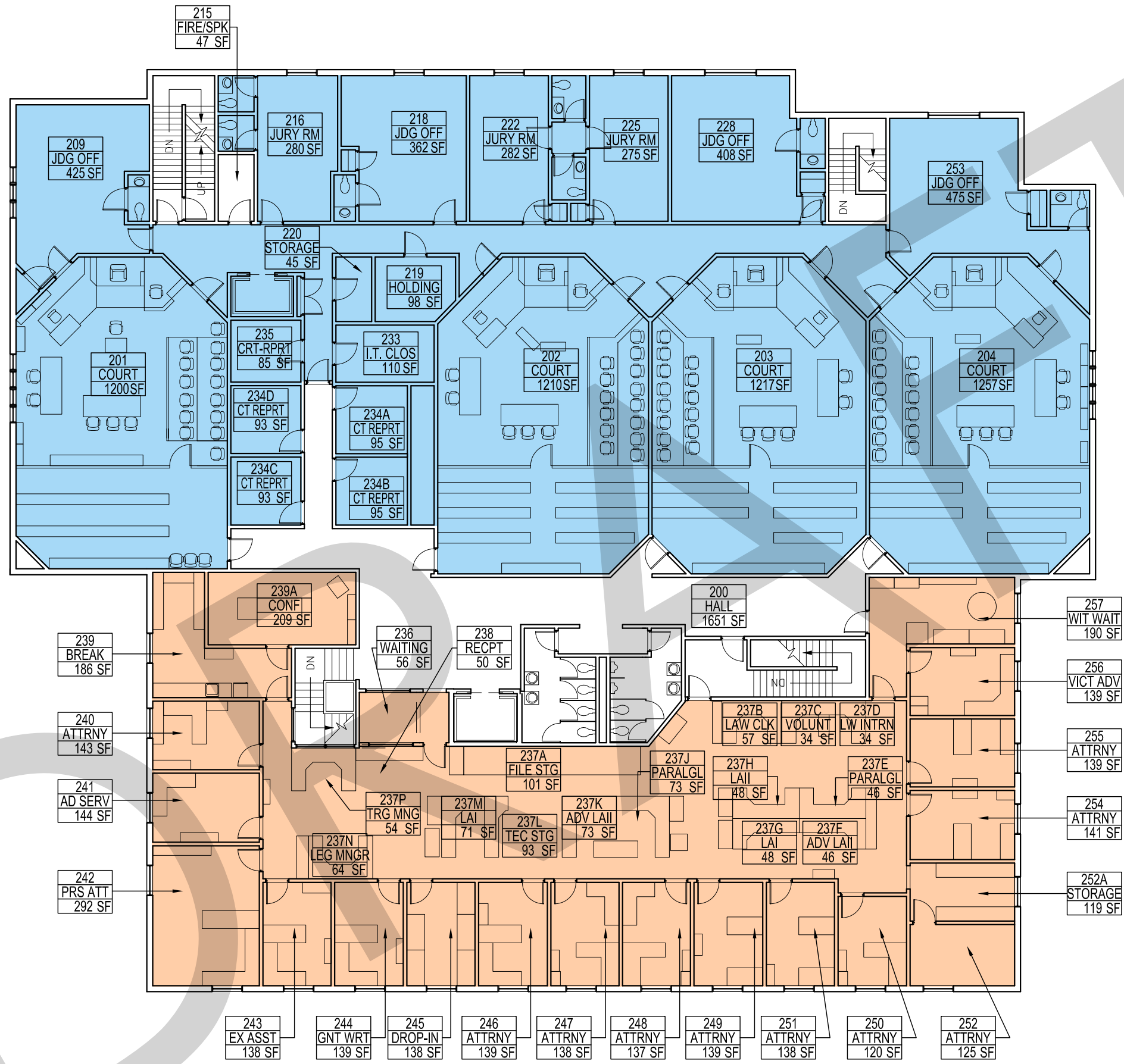
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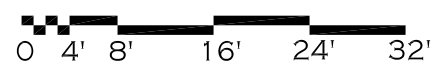
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FLOOR PLAN

A2.20

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1  
A2.20  
BUILDING 2 - LEVEL 2 - FLOOR PLAN  
1/16"=1'-0"



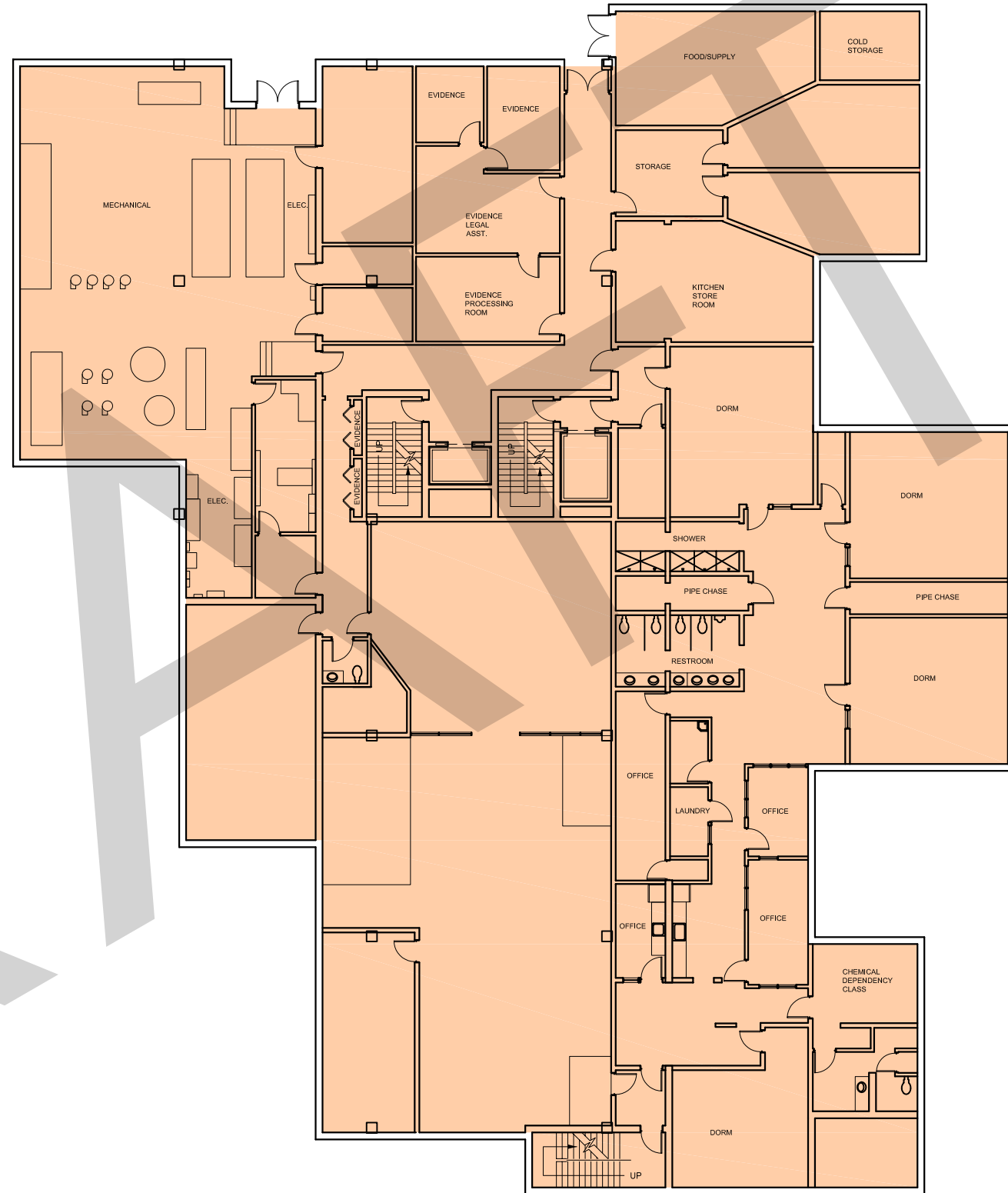
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EXISTING  
CONDITIONS  
FLOOR PLAN

A3.00


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1  
A3.00

BUILDING 3 - BASEMENT - FLOOR PLAN

3/64"=1'-0"

 = JAIL

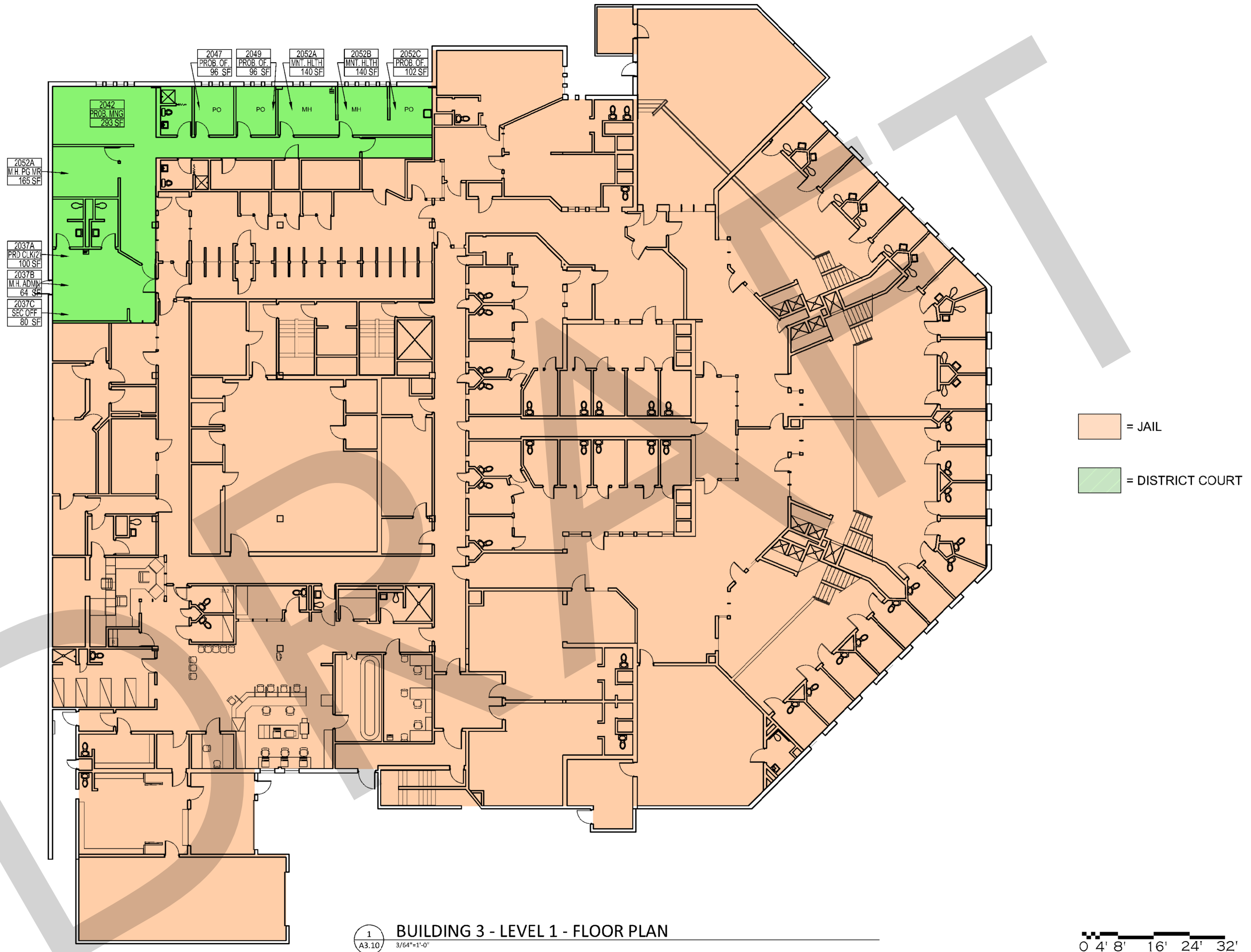
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 SEPTEMBER 28, 2018

EXISTING  
 CONDITIONS  
 FLOOR PLAN

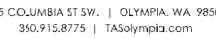
**A3.10**

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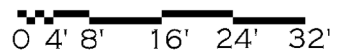
**BUILDING 3 - LEVEL 1 - FLOOR PLAN**

0 4' 8' 16' 24' 32'



**EXISTING  
CONDITIONS  
FLOOR PLAN**

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**BUILDING 3 - LEVEL 2 - FLOOR PLAN**



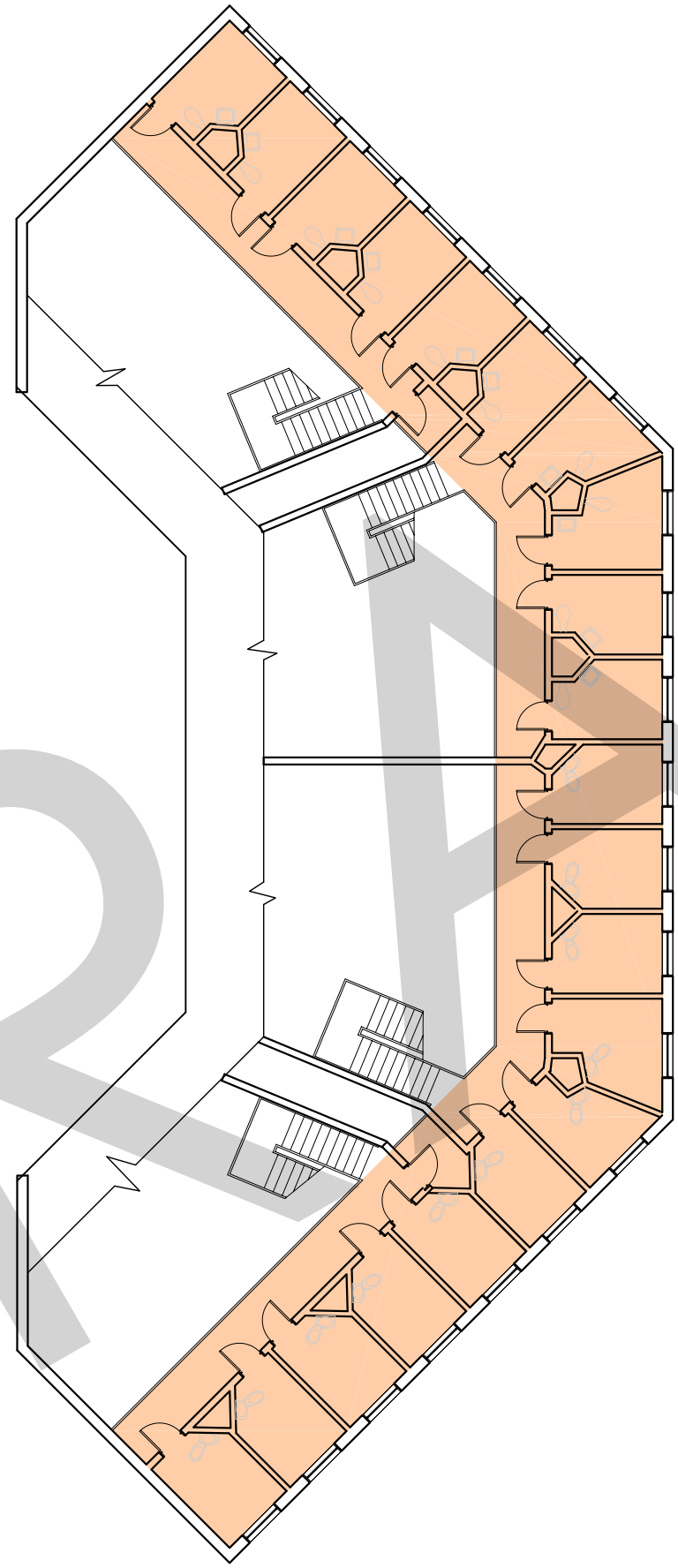
THURSTON COUNTY  
**COURTHOUSE & CIVIC CENTER**  
2000 LAKERIDGE DR SW, OLYMPIA, WASHINGTON 98502


PROJECT NUMBER: 1821  
**THURSTON COUNTY  
COURTHOUSE & CIVIC  
CENTER**  
SEPTEMBER 28, 2018

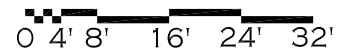
**EXISTING  
CONDITIONS  
FLOOR PLAN**

**A3.30**

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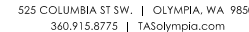


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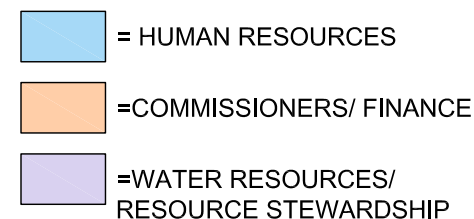


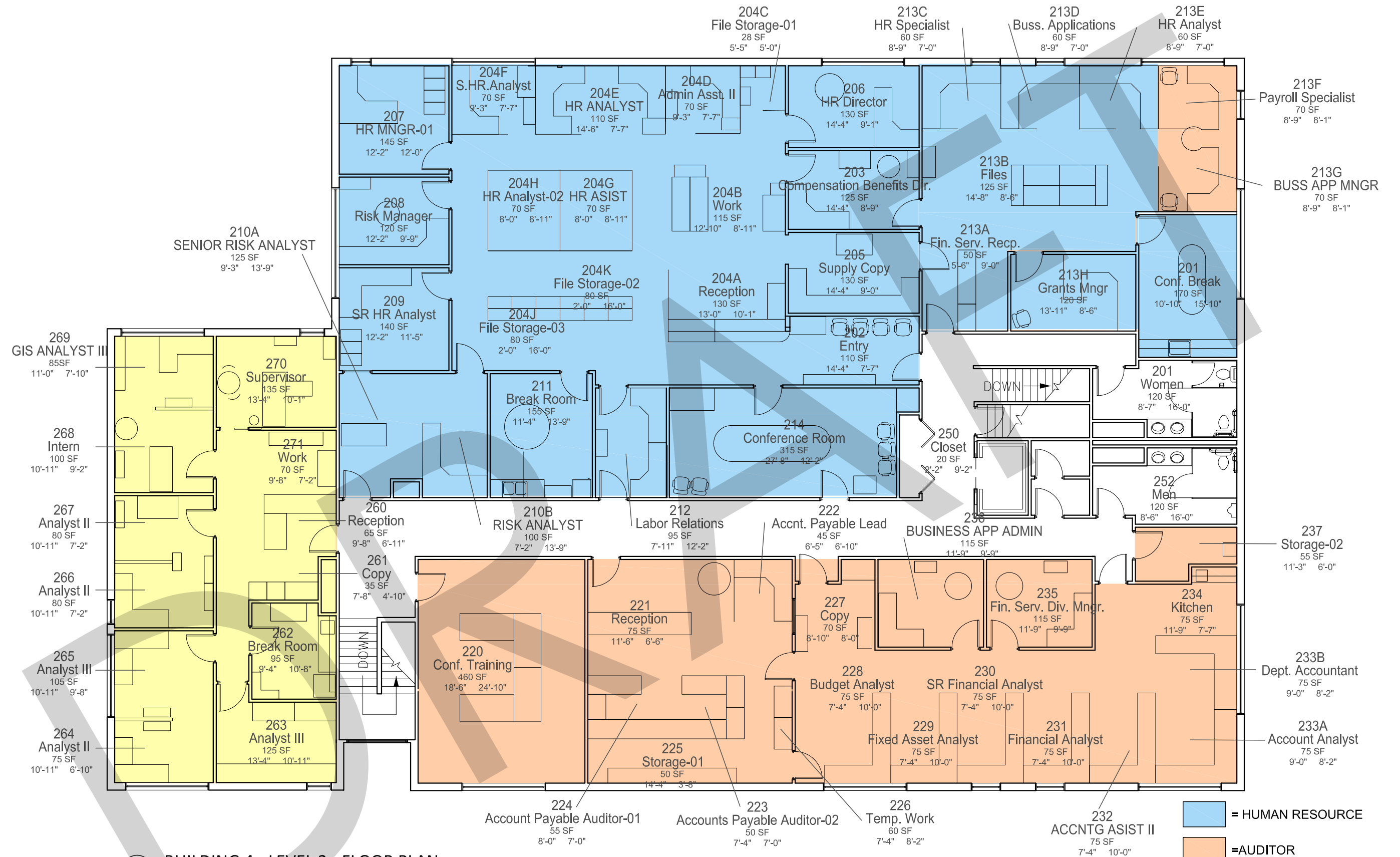
**BUILDING 3 - LEVEL 3 - FLOOR PLAN**  
1  
A3.30  
3/64"=1'-0"





## 2000 LAKERIDGE DR SW. OLYMPIA, WASHINGTON 98502





## BUILDING 4 - LEVEL 2 - FLOOR PLAN

THURSTON COUNTY  
COURTHOUSE & CIVIC CENTER  
2000 LAKE RIDGE DR SW. OLYMPIA, WASHINGTON

PROJECT NUMBER: 1821  
THURSTON COUNTY  
COURTHOUSE & CIVIC  
CENTER  
SEPTEMBER 28, 2018

EXISTING  
CONDITIONS  
FLOOR PLAN

A5.01

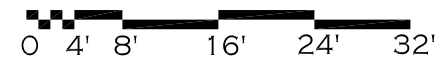
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1  
A5.01

BUILDING 5 - FLOOR 1 - FLOOR PLAN - PROSECUTORS OFFICE

1/16"=1'-0"



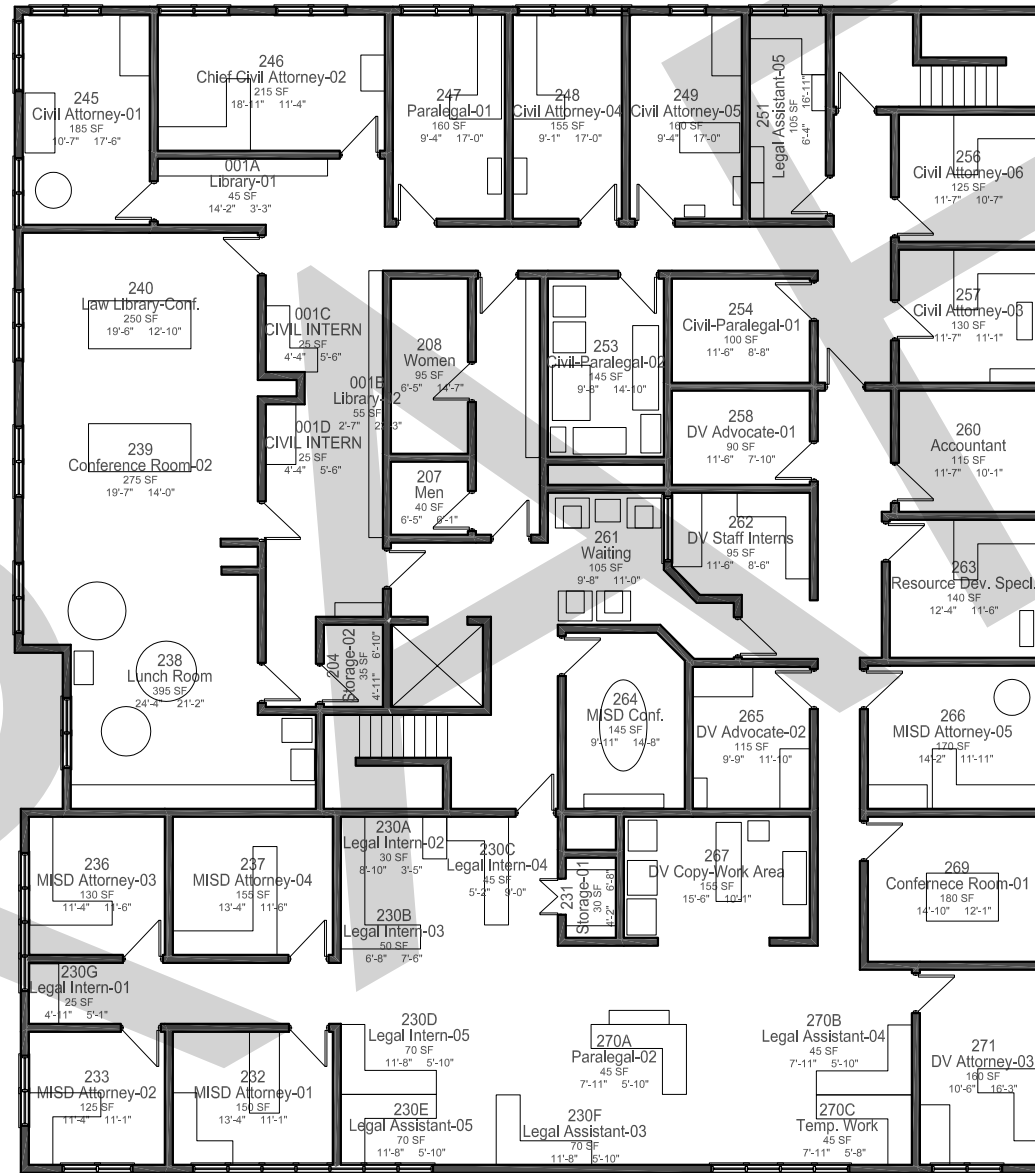
THURSTON COUNTY  
COURTHOUSE & CIVIC CENTER  
2000 LAKERIDGE DR SW. OLYMPIA, WASHINGTON

PROJECT NUMBER: 1821  
THURSTON COUNTY  
COURTHOUSE & CIVIC  
CENTER  
SEPTEMBER 28, 2018

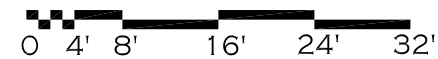
EXISTING  
CONDITIONS  
FLOOR PLAN

A5.02

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1  
A5.02  
BUILDING 5 - FLOOR 2 - FLOOR PLAN - PROSECUTORS OFFICE  
1/16"=1'-0"



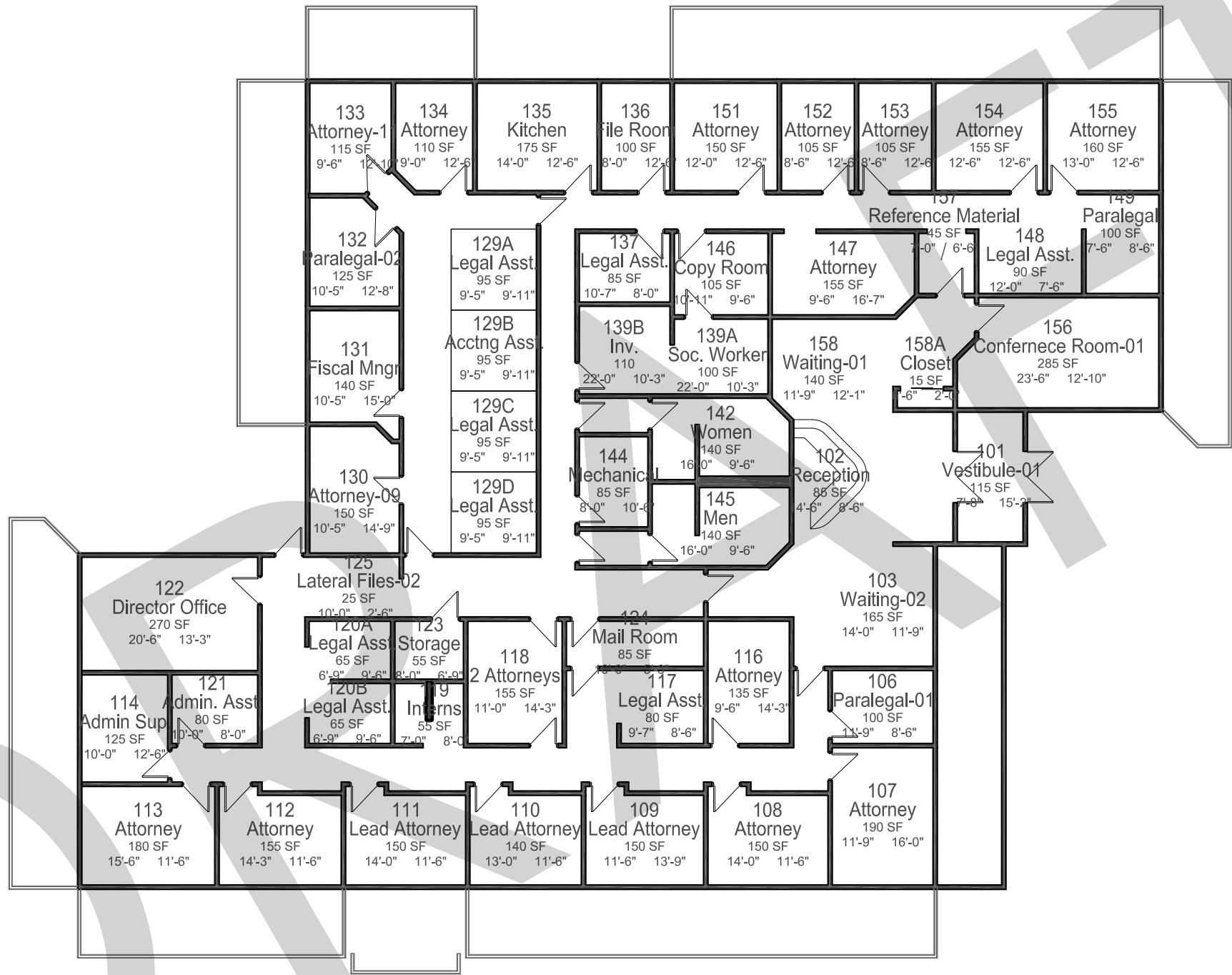
THURSTON COUNTY  
COURTHOUSE & CIVIC CENTER  
2000 LAKE RIDGE DR SW. OLYMPIA, WASHINGTON

PROJECT NUMBER: 1821  
THURSTON COUNTY  
COURTHOUSE & CIVIC  
CENTER  
SEPTEMBER 28, 2018

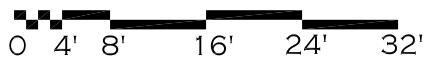
EXISTING  
CONDITIONS  
FLOOR PLAN

A6.01

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1  
A6.01  
BUILDING 6 - FLOOR 1 - FLOOR PLAN - OFFICE OF ASSIGNED COUNSEL  
1/16"=1'-0"





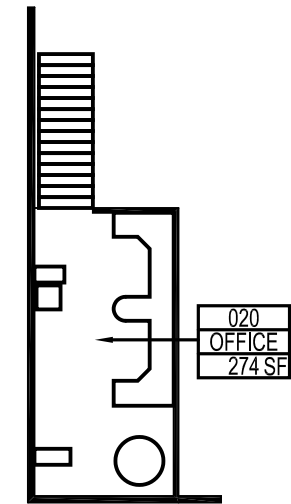
THURSTON COUNTY  
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PROJECT NUMBER: 1821  
THURSTON COUNTY  
COURTHOUSE & CIVIC  
CENTER  
SEPTEMBER 28, 2018

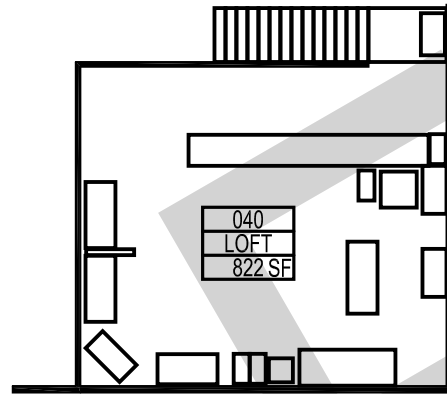
EXISTING  
CONDITIONS  
FLOOR PLAN

**A7.01**

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1  
A7.01  
ELECTIONS SHOP - MEZZANINE PLAN  
1/16"=1'-0"



2  
A7.01  
ELECTIONS SHOP - LOFT PLAN  
1/16"=1'-0"



1  
A7.01  
ELECTIONS SHOP - FLOOR PLAN  
1/16"=1'-0"

0 4' 8' 16' 24' 32'

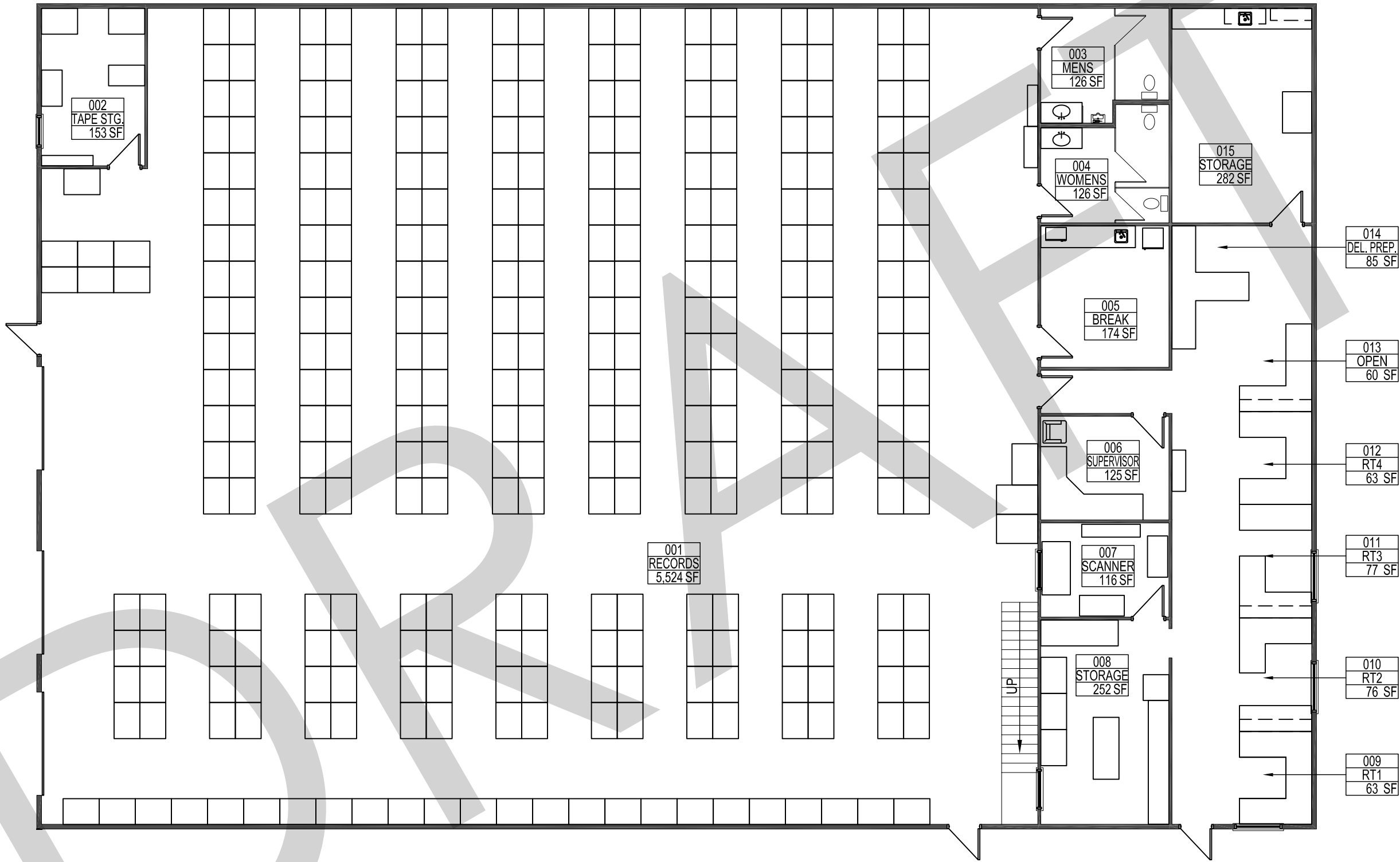
THURSTON COUNTY  
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2000 LAKE RIDGE DR SW, OLYMPIA, WASHINGTON 98502

PROJECT NUMBER: 1821  
**THURSTON COUNTY  
COURTHOUSE & CIVIC  
CENTER**  
SEPTEMBER 28, 2018

**EXISTING  
CONDITIONS  
FLOOR PLAN**

**A8.01**

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**1**  
A8.01  
CENTRAL SERVICES RECORDS STORAGE - FLOOR PLAN  
3/32"=1'-0"

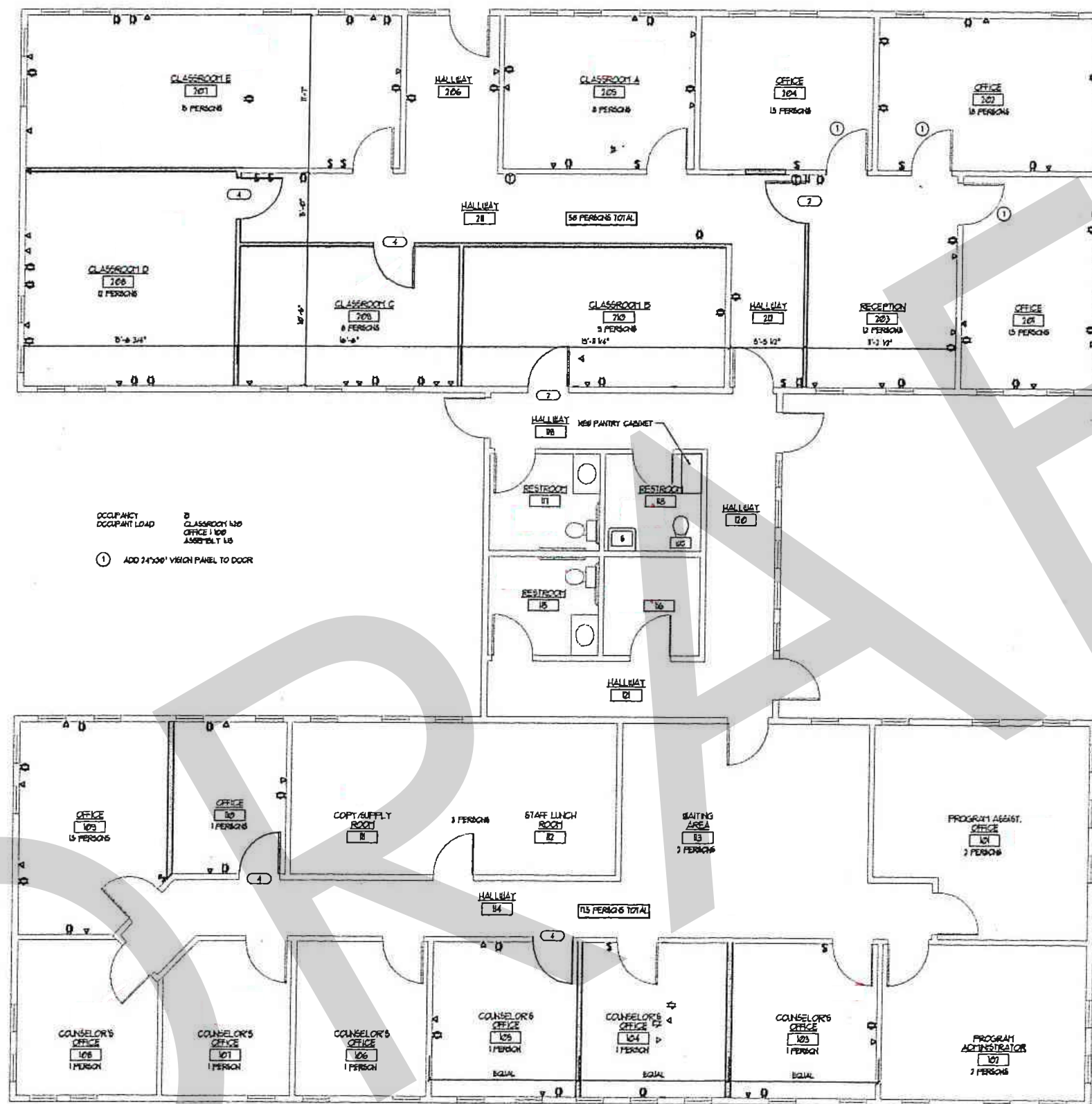
THURSTON COUNTY  
**COURTHOUSE & CIVIC CENTER**  
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PROJECT NUMBER: 1821  
THURSTON COUNTY  
COURTHOUSE & CIVIC  
CENTER  
SEPTEMBER 28, 2018

**EXISTING  
CONDITIONS  
FLOOR PLAN**

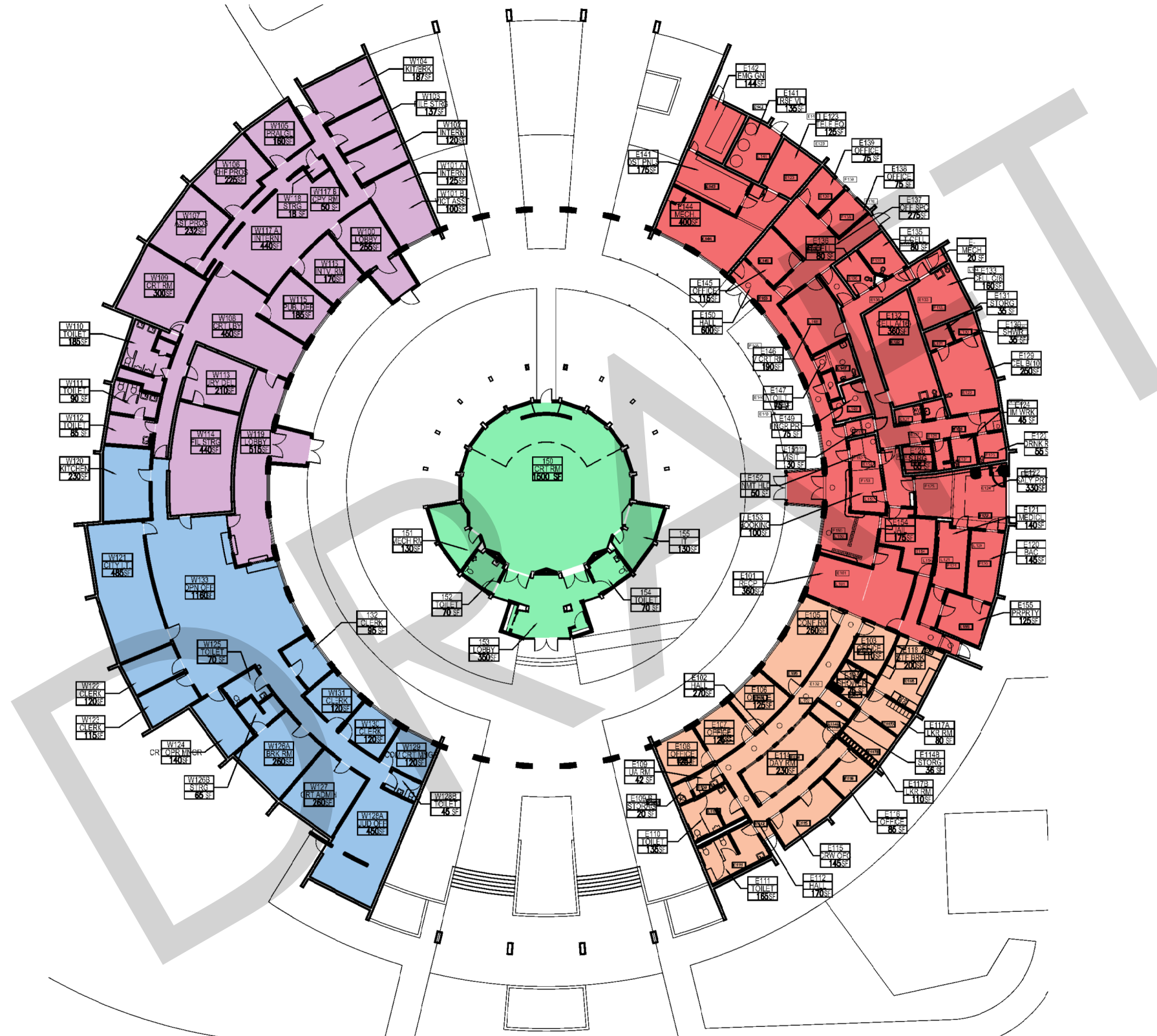
**A9.01**

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1  
A9.01 9.0 DRUG COURT  
1/16"=1'-0"

0 4' 8' 16' 24' 32'



- = MUNICIPAL COURT  
2,690 SF
- = PROBATION  
2,910 SF
- = MUNICIPAL JAIL  
6,610 SF
- = CITY PROSECUTOR  
5,400 SF
- = COURT & PARKING  
JUDICATION  
4,560 SF

1 JUSTICE CENTER - LEVEL 1 - FLOOR PLAN  
 ##### 1/16"=1'-0"

THURSTON COUNTY  
**COURTHOUSE & CIVIC CENTER**  
 2000 LAKERIDGE DR SW. OLYMPIA, WASHINGTON 98502

PROJECT NUMBER: 1821  
**THURSTON COUNTY  
 COURTHOUSE & CIVIC  
 CENTER**  
 SEPTEMBER 28, 2018

**EXISTING  
 CONDITIONS  
 FLOOR PLAN**

**A1.10**



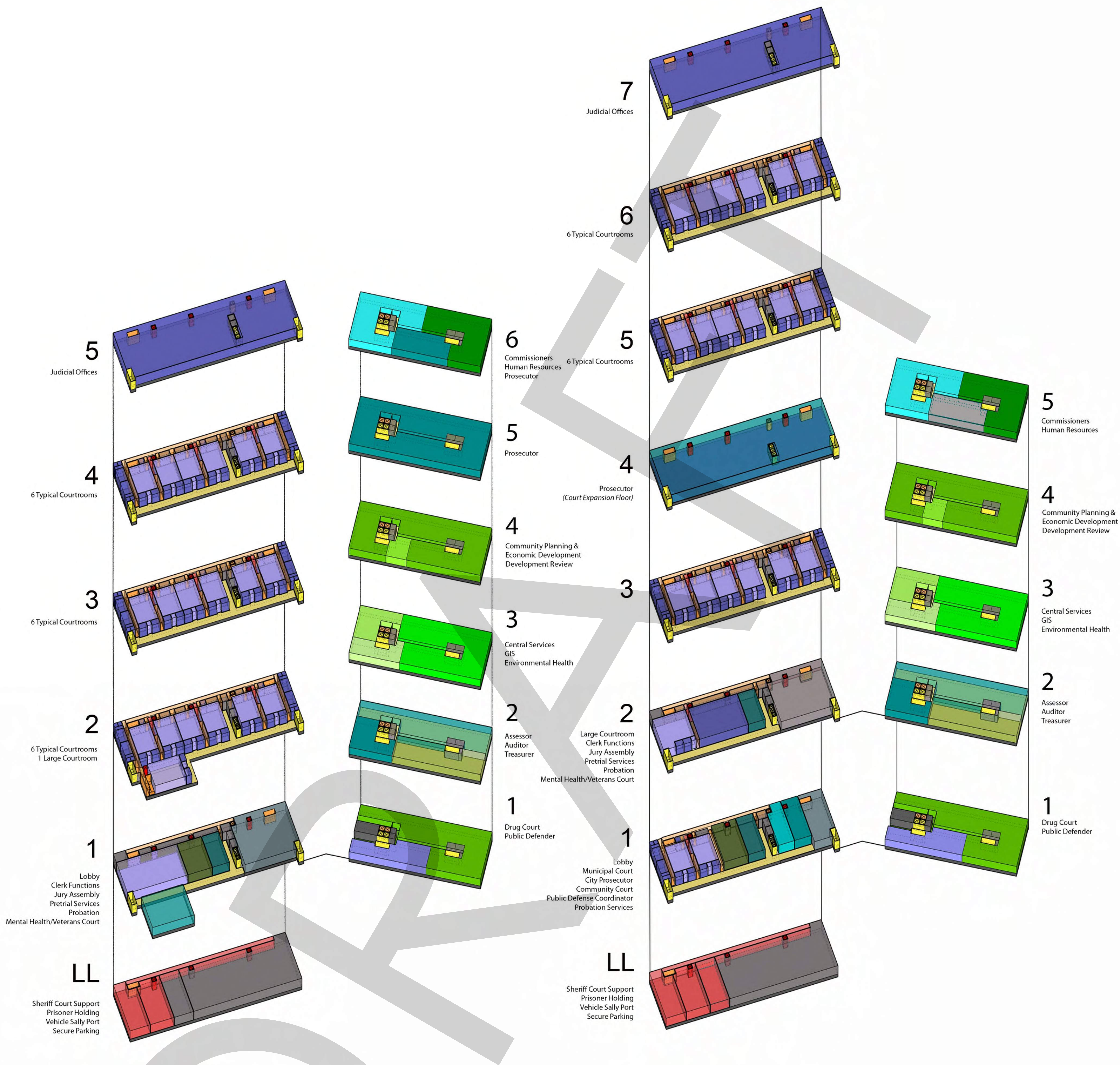
# VI.

THURSTON-COUNTY COURTHOUSE FINAL REPORT

## STACKING DIAGRAMS

DRAFT









vii.

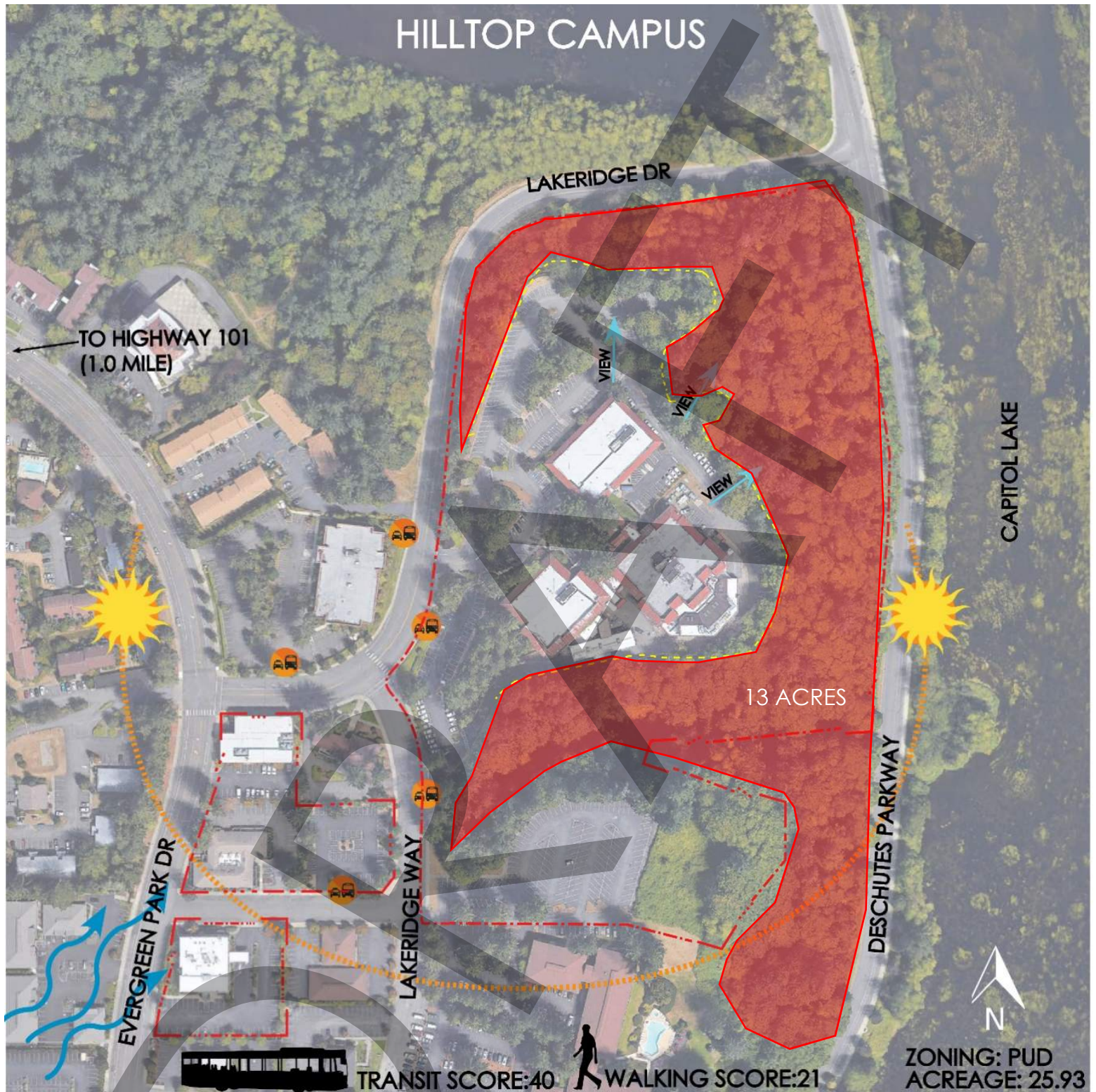
THURSTON COUNTY COURTHOUSE FINAL REPORT

SITE CONDITIONS

DRAFT

# SITE CONDITIONS: HILLTOP CAMPUS

# SITE DATA: HILLTOP

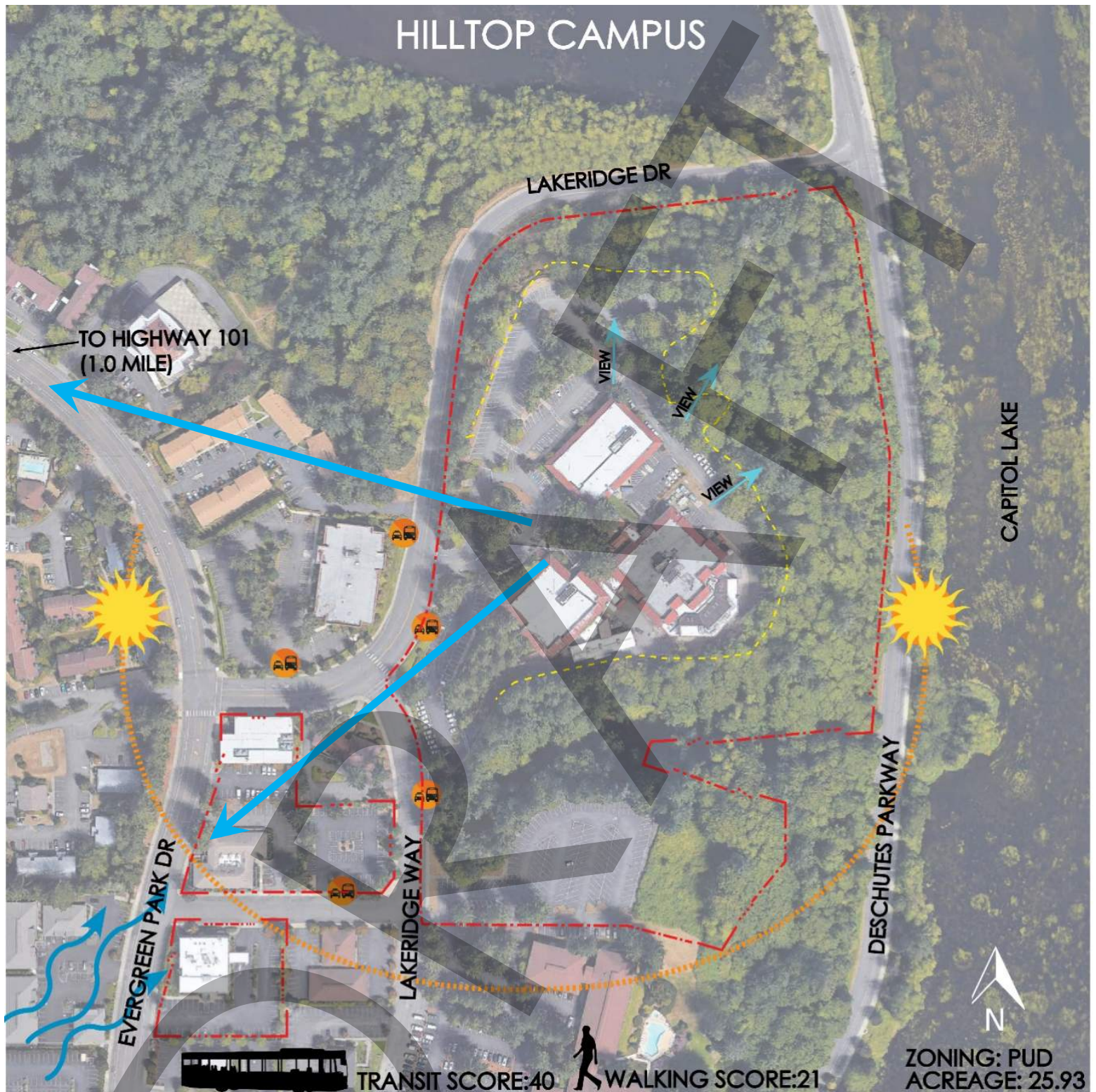


## STEEP SLOPES / LANDSLIDE HAZARD

The steep hill along Deschutes Parkway, approximately 13 acres of challenging terrain for construction, renders nearly half of this site undevelopable. Some erosion control and slope stabilization techniques may need to be used for new development.



# SITE DATA: HILLTOP

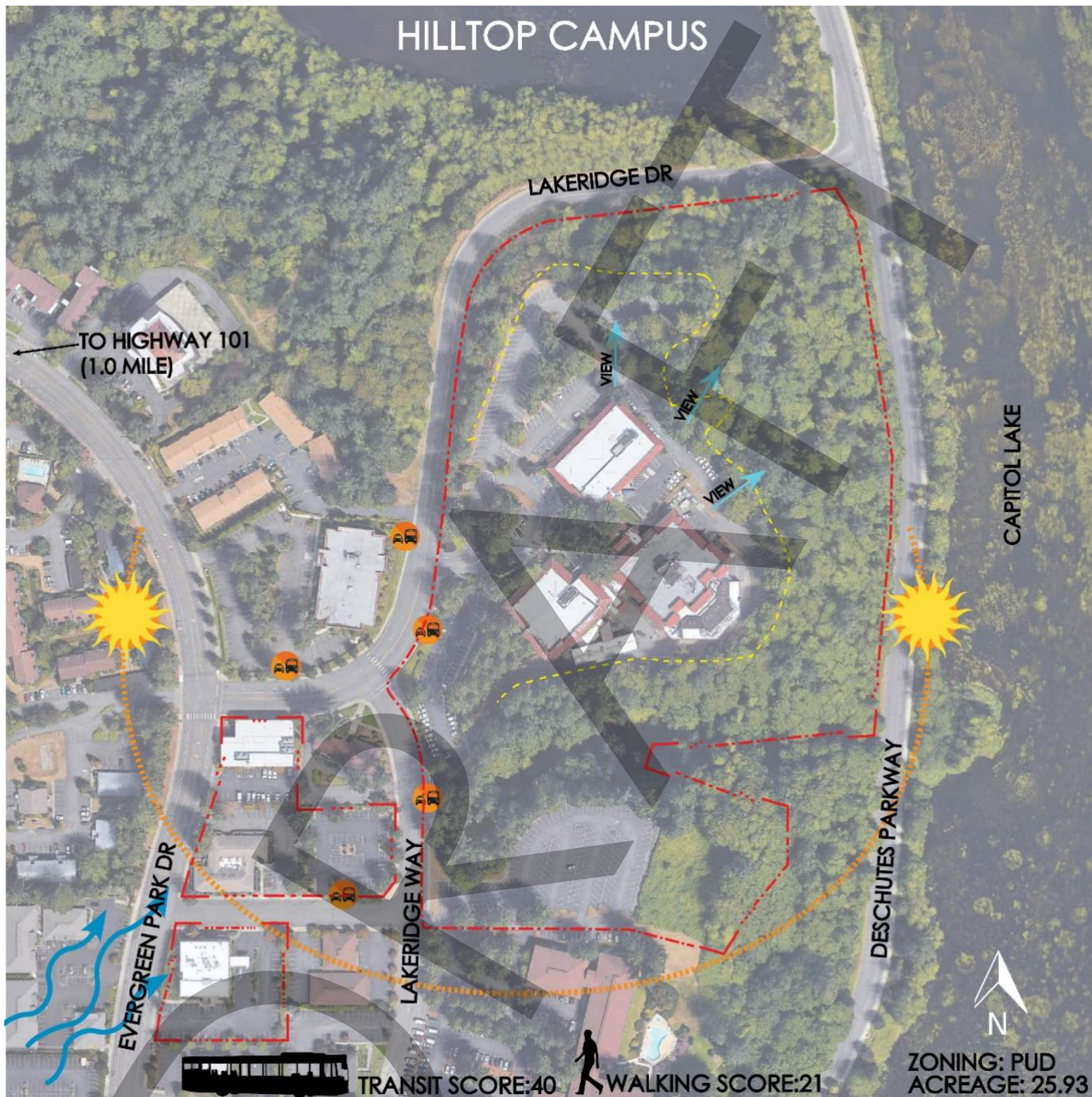


## STORMWATER FLOW EXEMPT

The stormwater pipes serving this site are adequate in size to all for point source treatment and then immediately discharge into the City's stormwater system. (As opposed to utilizing detention ponds.)



# SITE DATA: HILLTOP



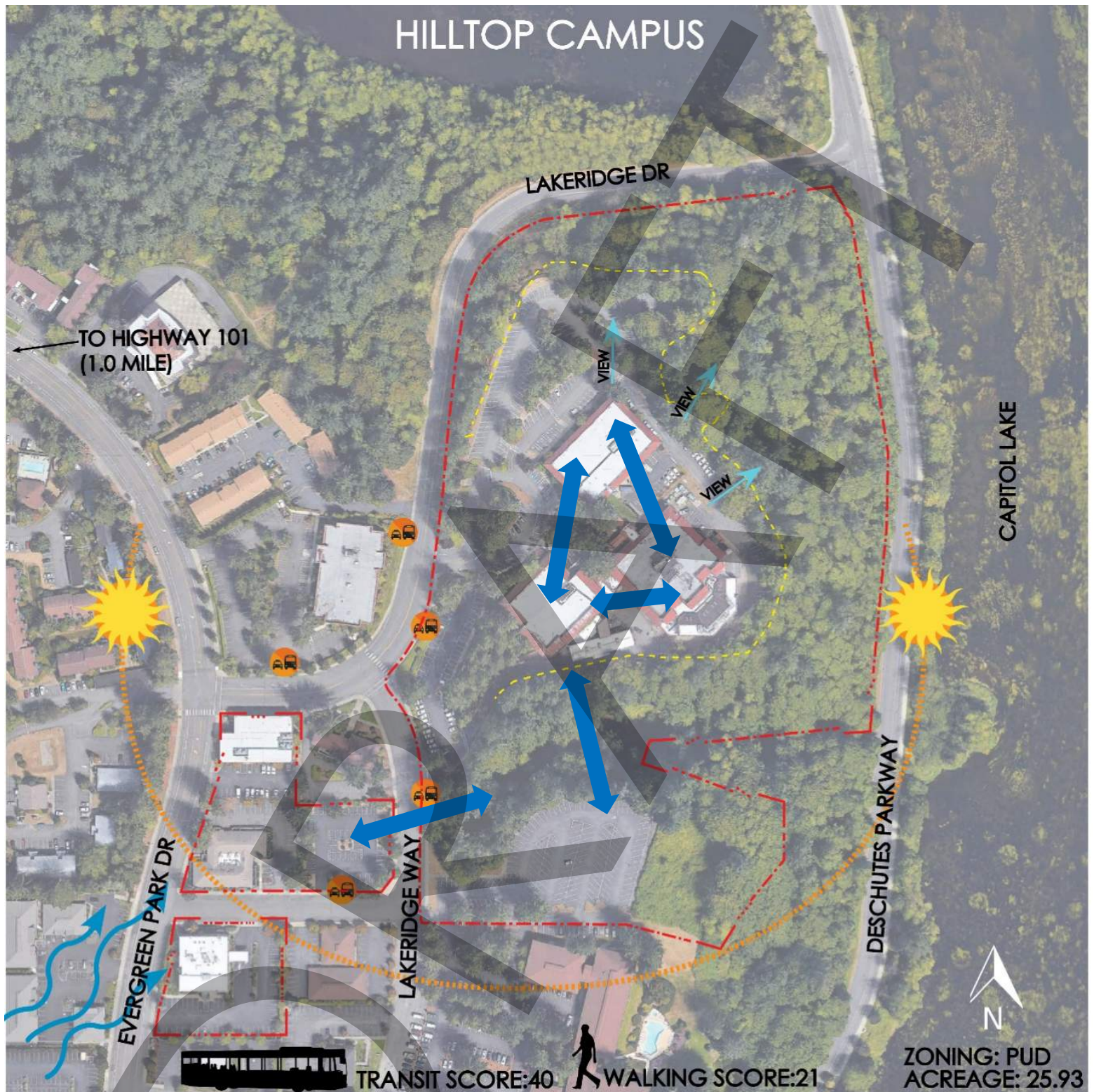
## UTILITIES CURRENTLY ON SITE

The site is already supplied with the main utilities needed such as; Water, Sewer, Gas, and Electricity.





# SITE DATA: HILLTOP

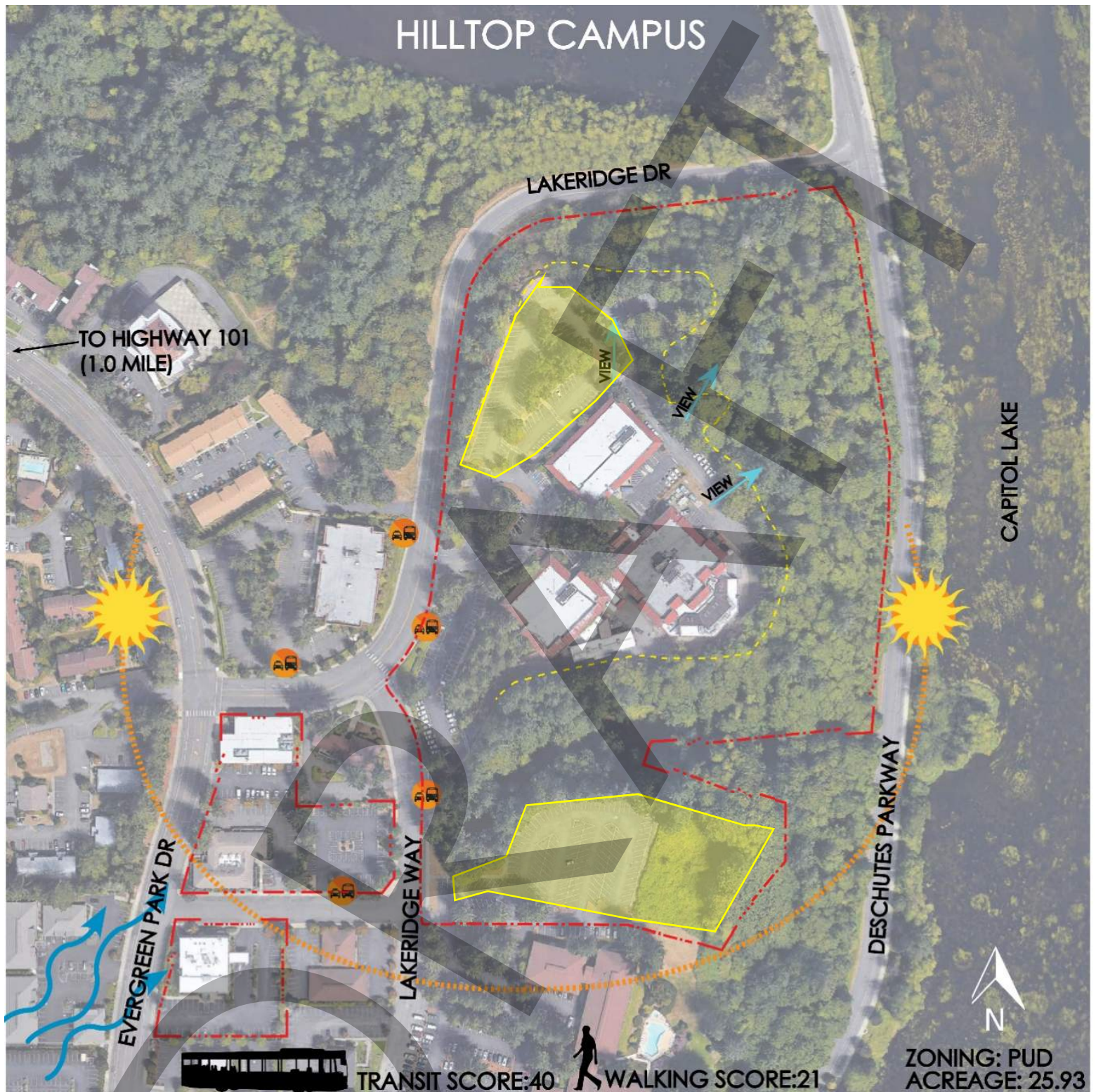


## CIRCULATION BETWEEN EXISTING BUILDINGS

Existing grades are such that currently allow for accessible circulation from building to building. There is some grade change on site that will need to be address in future designs.



# SITE DATA: HILLTOP

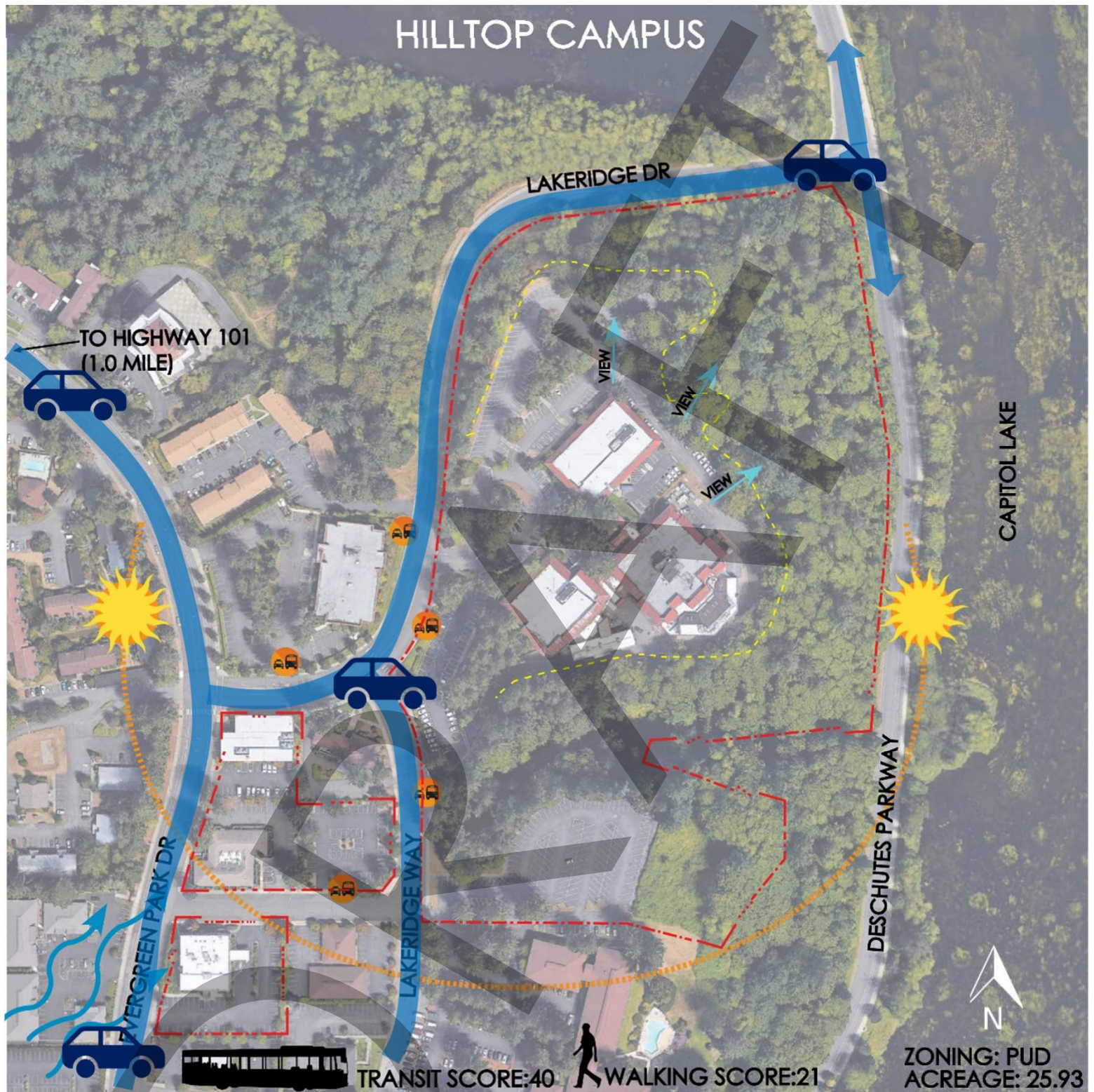


## INSUFFICIENT PARKING WITH LIMITED SURFACE LOT EXPANSION SPACE

Due to the site terrain, surface parking is limited, and a parking structure(s) would be necessary.



# SITE DATA: HILLTOP

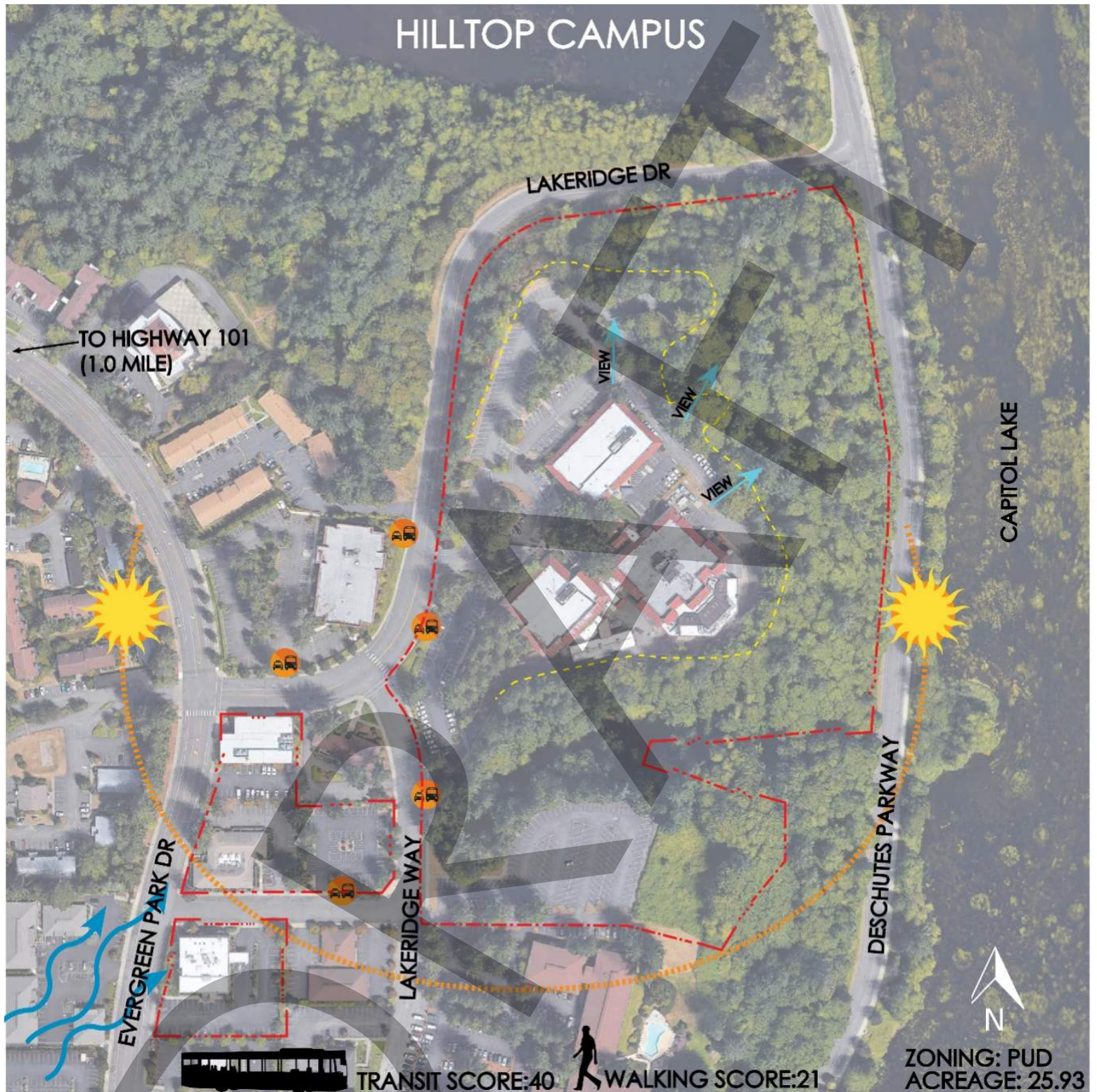


## SITE ACCESS

Several streets provide good access to this area however there is only one entry point into the existing campus, and the steep slopes to the North and East do not allow for additional access points from Lakeridge Dr. or Deschutes Parkway.



# SITE DATA: HILLTOP



## TRANSIT SCORE 40 OUT OF 100

Transit score measures how well the location is served by public transit. This site is at the outer edge of transit service

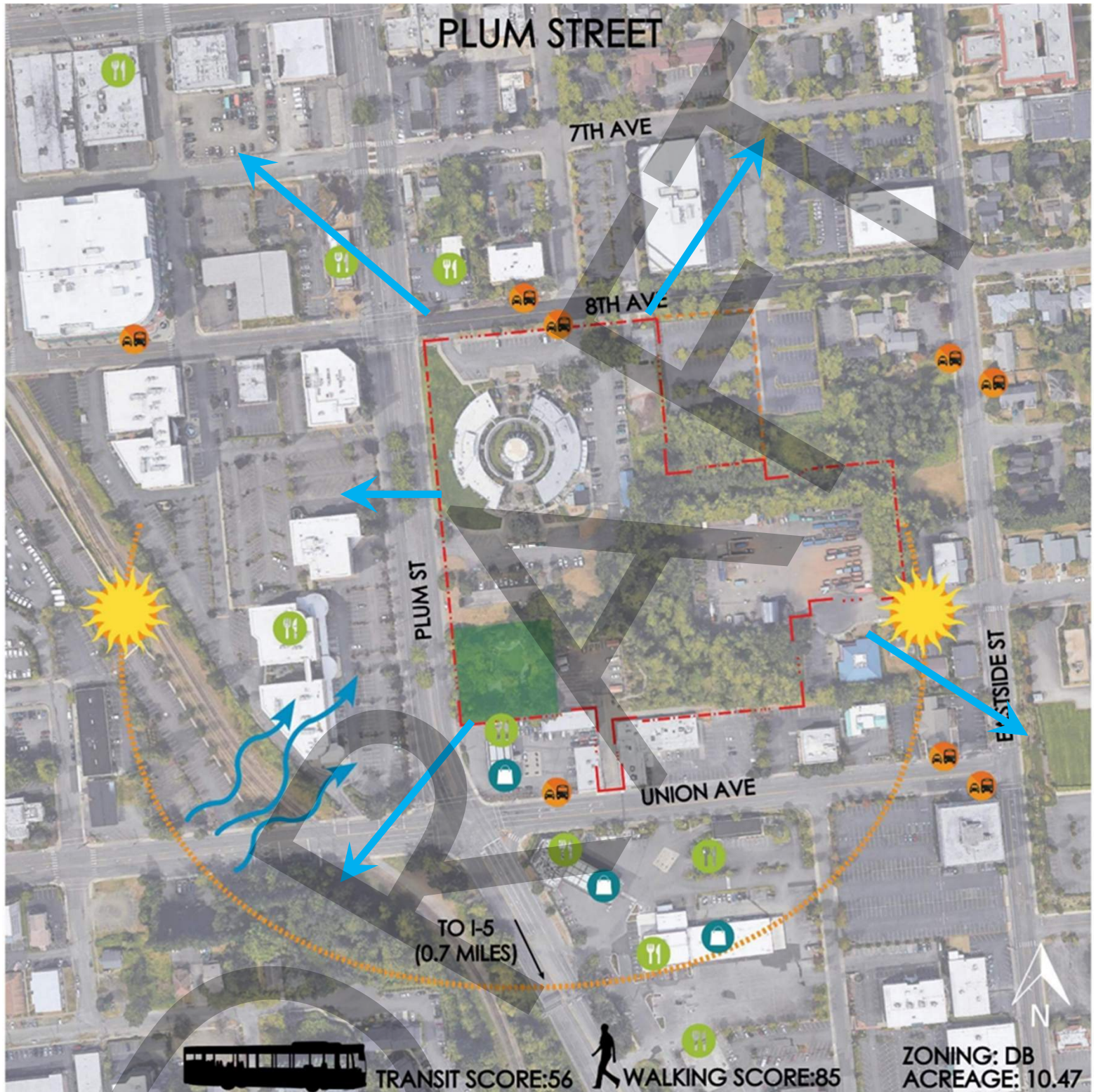
## WALK SCORE 21 OUT OF 100

Walking score measures the ability for pedestrians to access the site as well as access to nearby amenities. This site has a couple of nearby amenities.

# SITE CONDITIONS: PLUM STREET



# SITE DATA: PLUM STREET



## STORMWATER FLOW EXEMPT

The stormwater pipes serving this site are adequate in size to all for point source treatment and then immediately discharge into the City's stormwater system. (As opposed to utilizing detention ponds.)



# SITE DATA: PLUM STREET

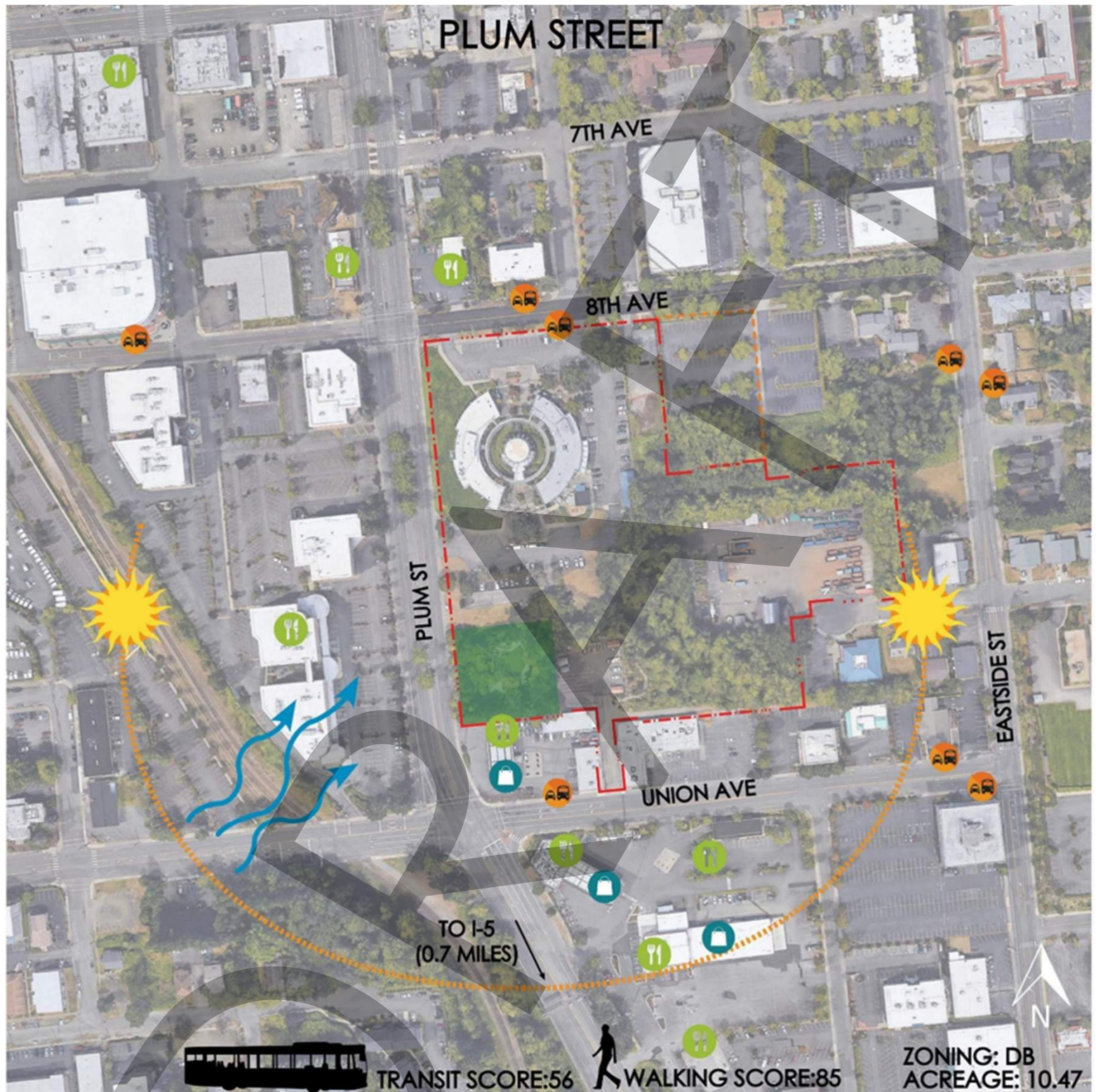


## STORMWATER REQUIREMENTS MAY BE INCREASED DUE TO WETLAND

Measures must be taken to ensure any on site stormwater flows away from nearby wetlands and into the City's stormwater collection system.



# SITE DATA: PLUM STREET



## UTILITIES CURRENTLY ON SITE

The site is already supplied with the main utilities needed such as; Water, Sewer, Gas, and Electricity

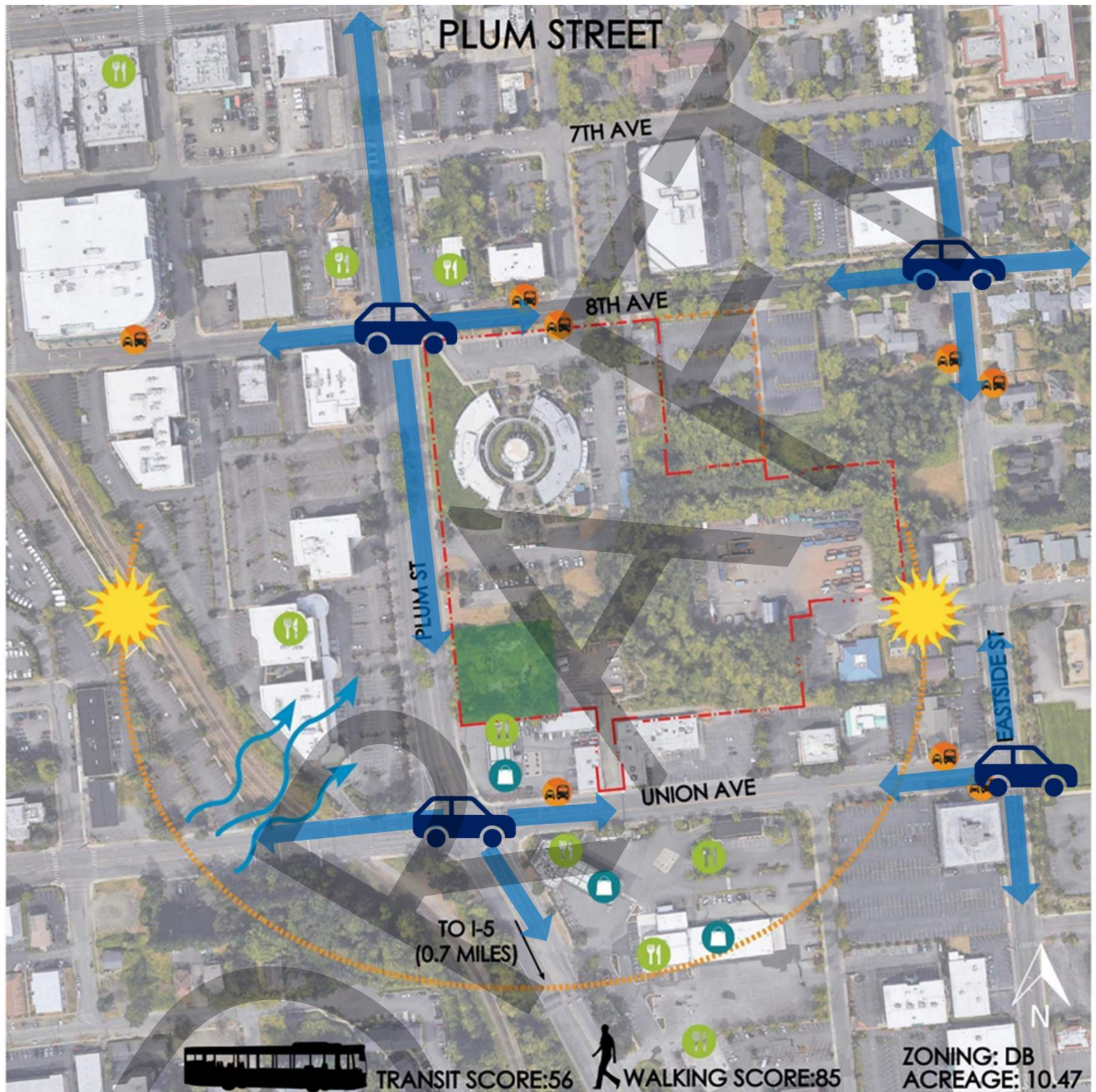




There are some non-developable areas due to wetland on this site. Wetland compensatory mitigation may be a more effective solution to increase developable area by removing these two small wetland areas and enhancing a much larger wetland at a location yet to be determined.



# SITE DATA: PLUM STREET

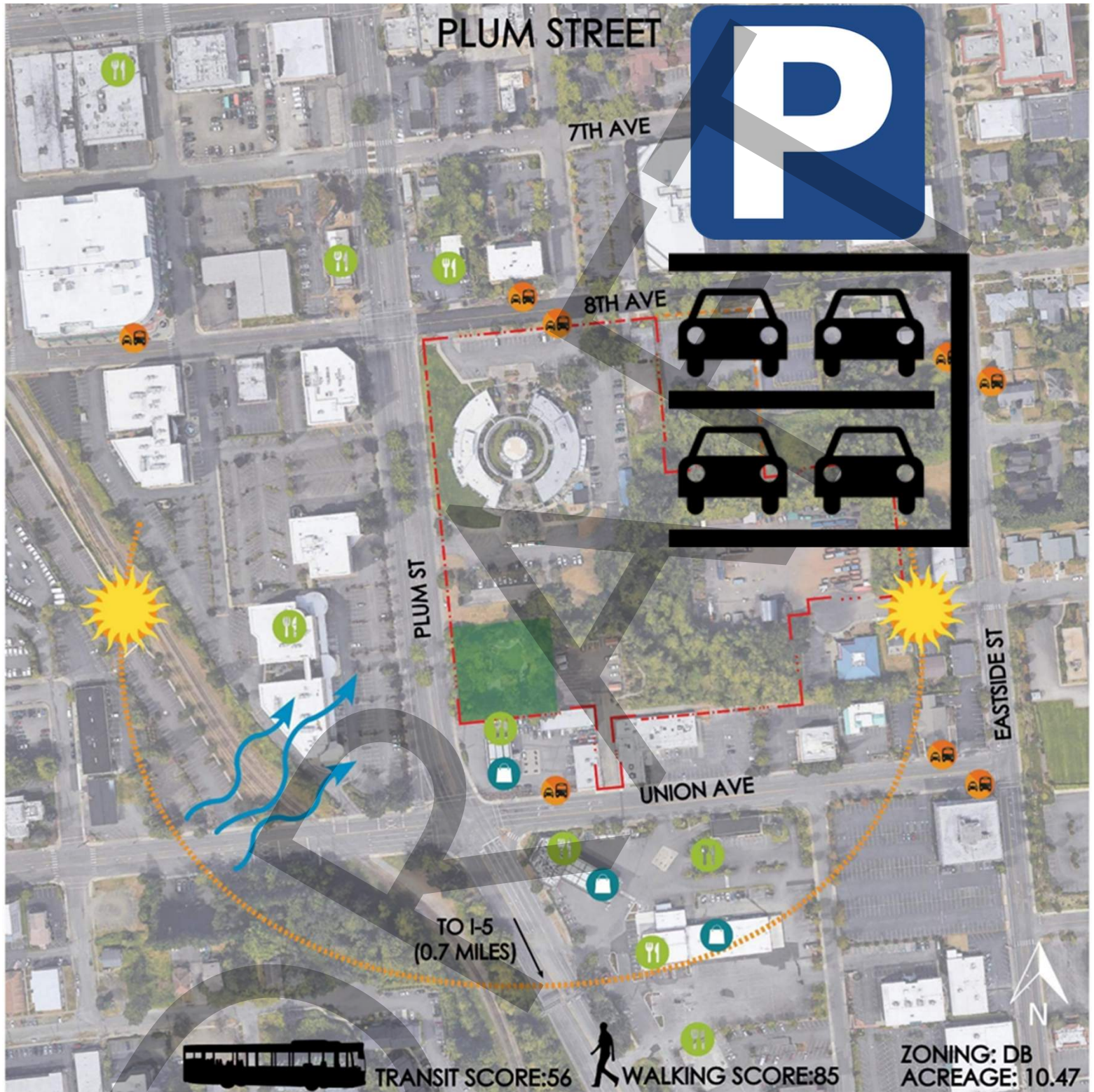


## SITE ACCESS

This site is surrounded by two to four lane streets. Three of the Four intersections have traffic control lights, and there is good direct access to I-5 from the South. Access to the site from the public right of way can be achieved from; Plum St, 8<sup>th</sup> Ave, and Union. Site Access off Eastside St. is also achievable.



# SITE DATA: PLUM STREET

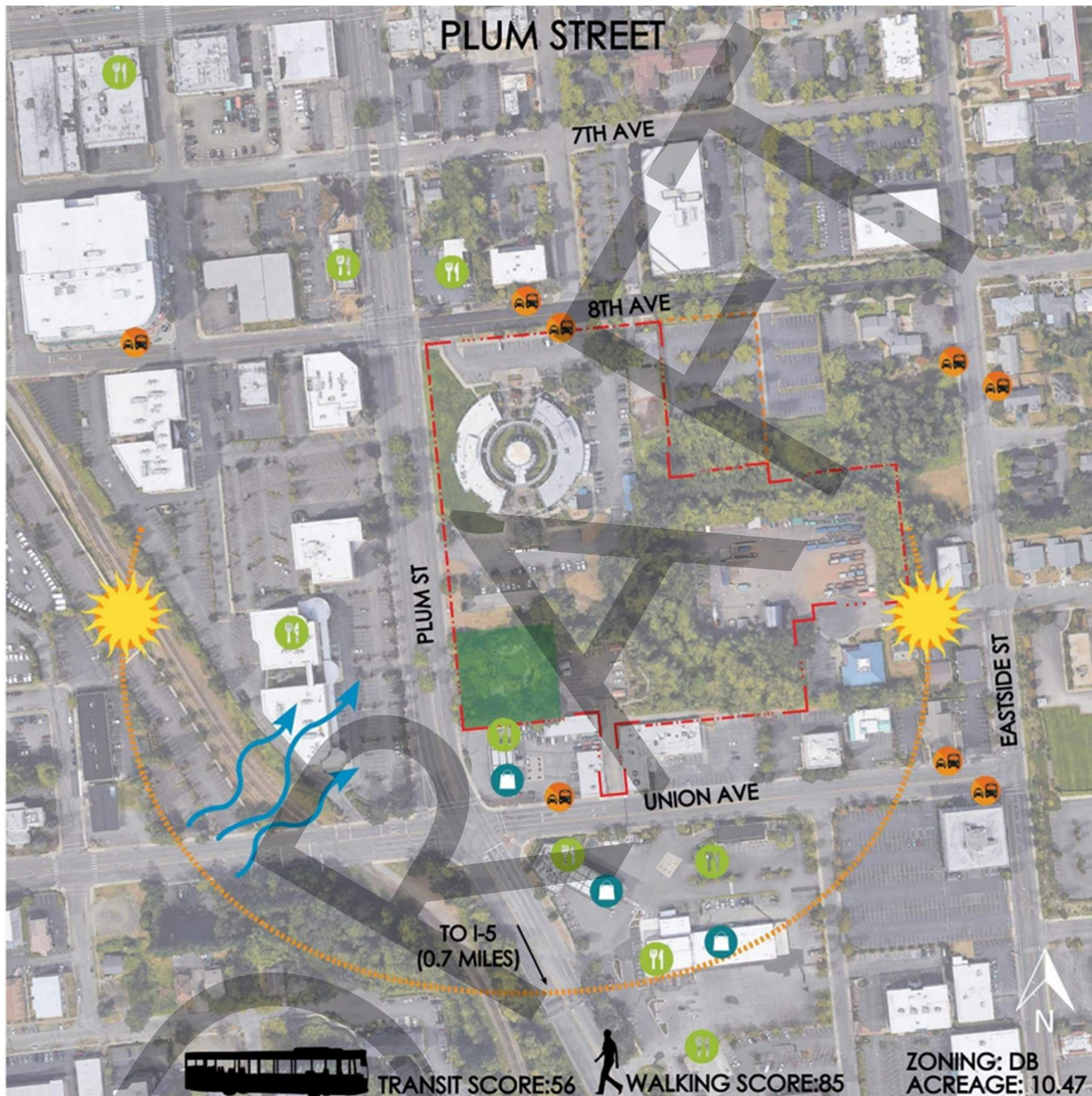


## STRUCTURED PARKING NEEDED

Due to this site's smaller surface area, there may be some surface parking, but a parking structure would be needed to cover number of parking stalls required.



# SITE DATA: PLUM STREET



## TRANSIT SCORE 56 OUT OF 100

Transit score measures how well the location is served by public transit. This site is closest to a transit hub and is served by multiple bus lines.

## WALK SCORE 85 OUT OF 100

Walking score measures the ability for pedestrians to access the site as well as access to nearby amenities. This site has access to many nearby amenities.



# SITE CONDITIONS: HARRISON WEST

# SITE DATA: HARRISON

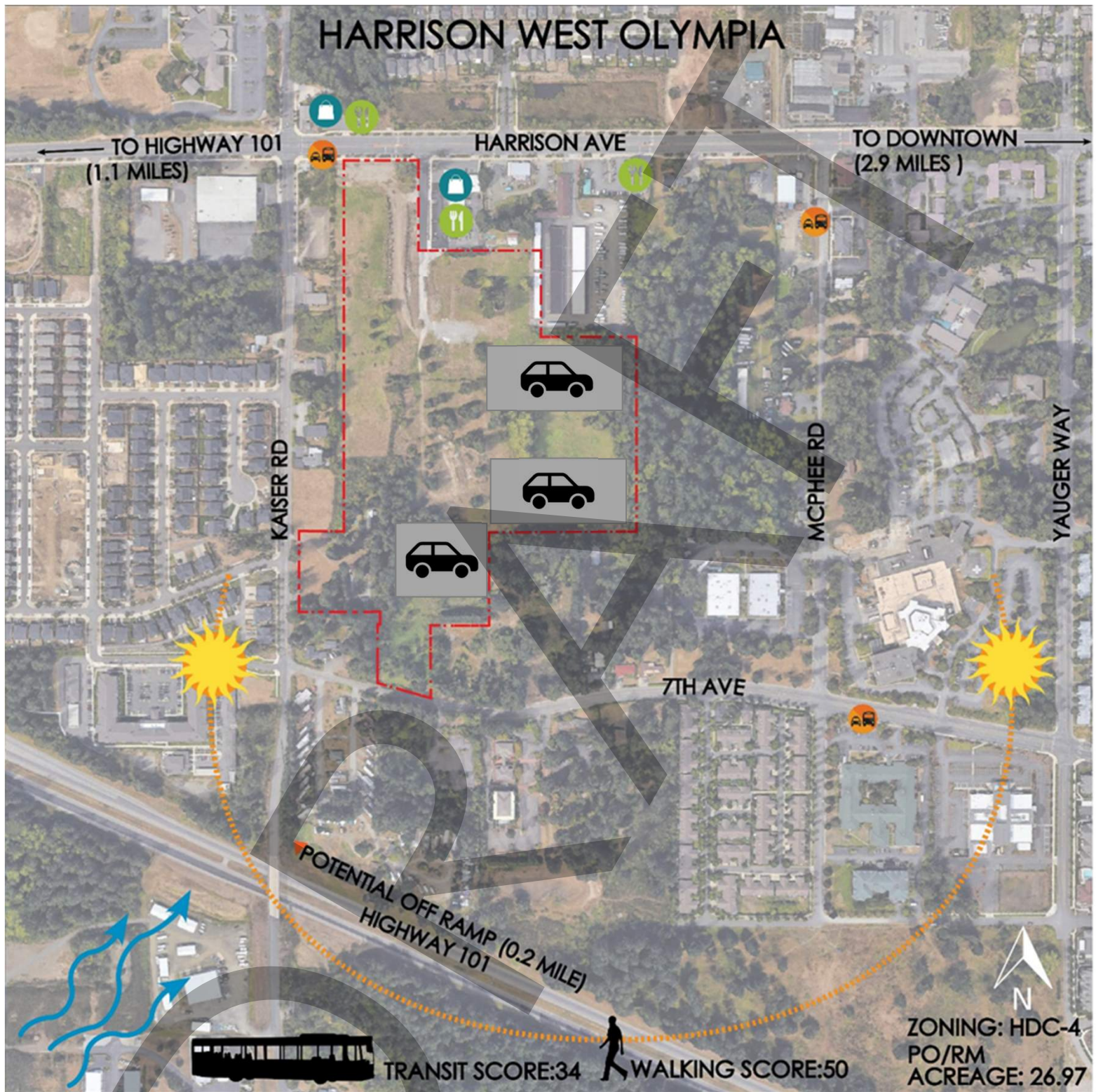


## LARGE DEVELOPABLE AREA

Of the nearly 27 acres on this site all of it is developable.



# SITE DATA: HARRISON

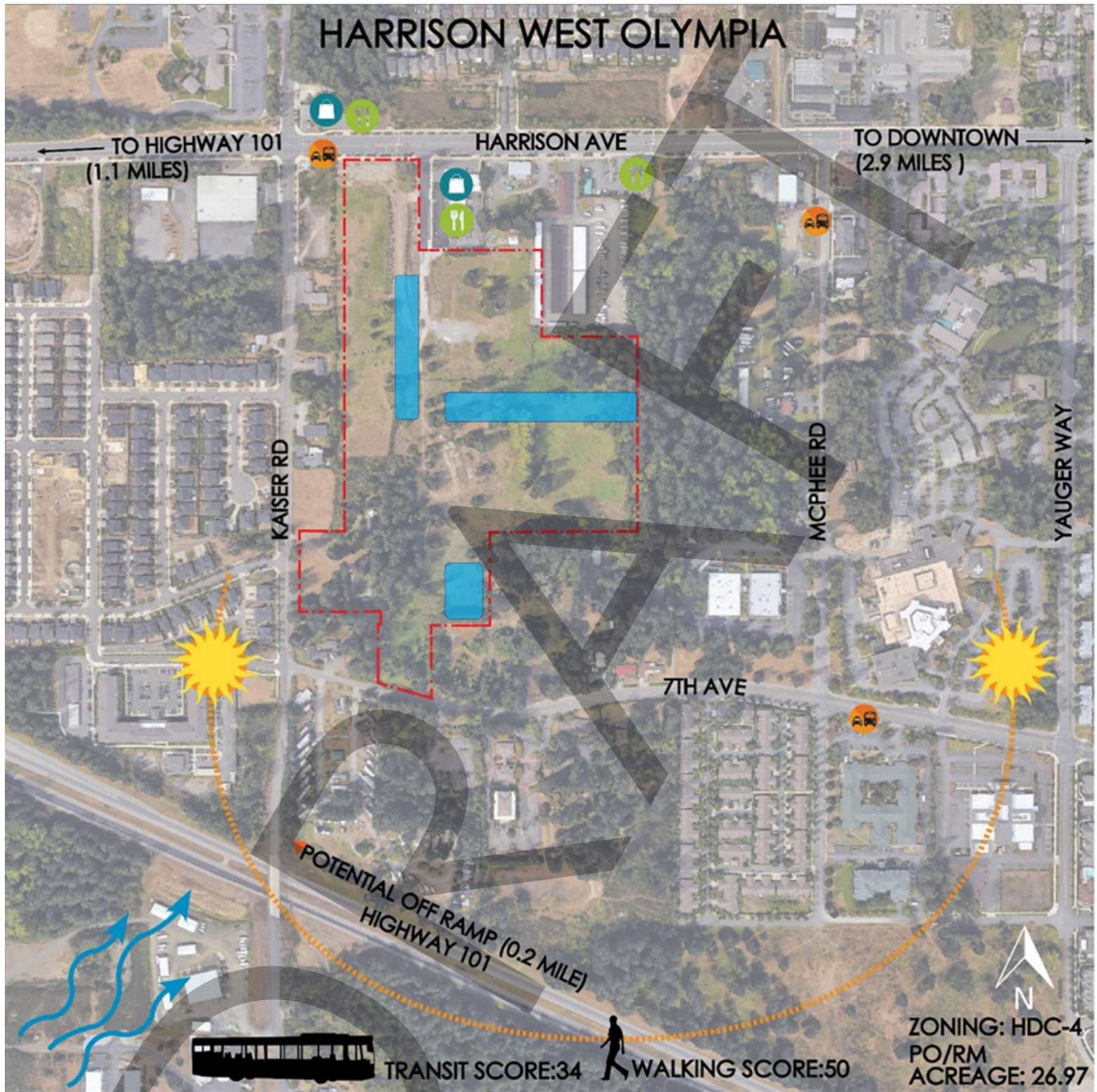


## SURFACE PARKING ONLY (POTENTIAL)

This site has the ability to provide all the parking required on the surface, without needing to build a parking structure. It would take approximately 9 acres to park 1,200 cars.



# SITE DATA: HARRISON



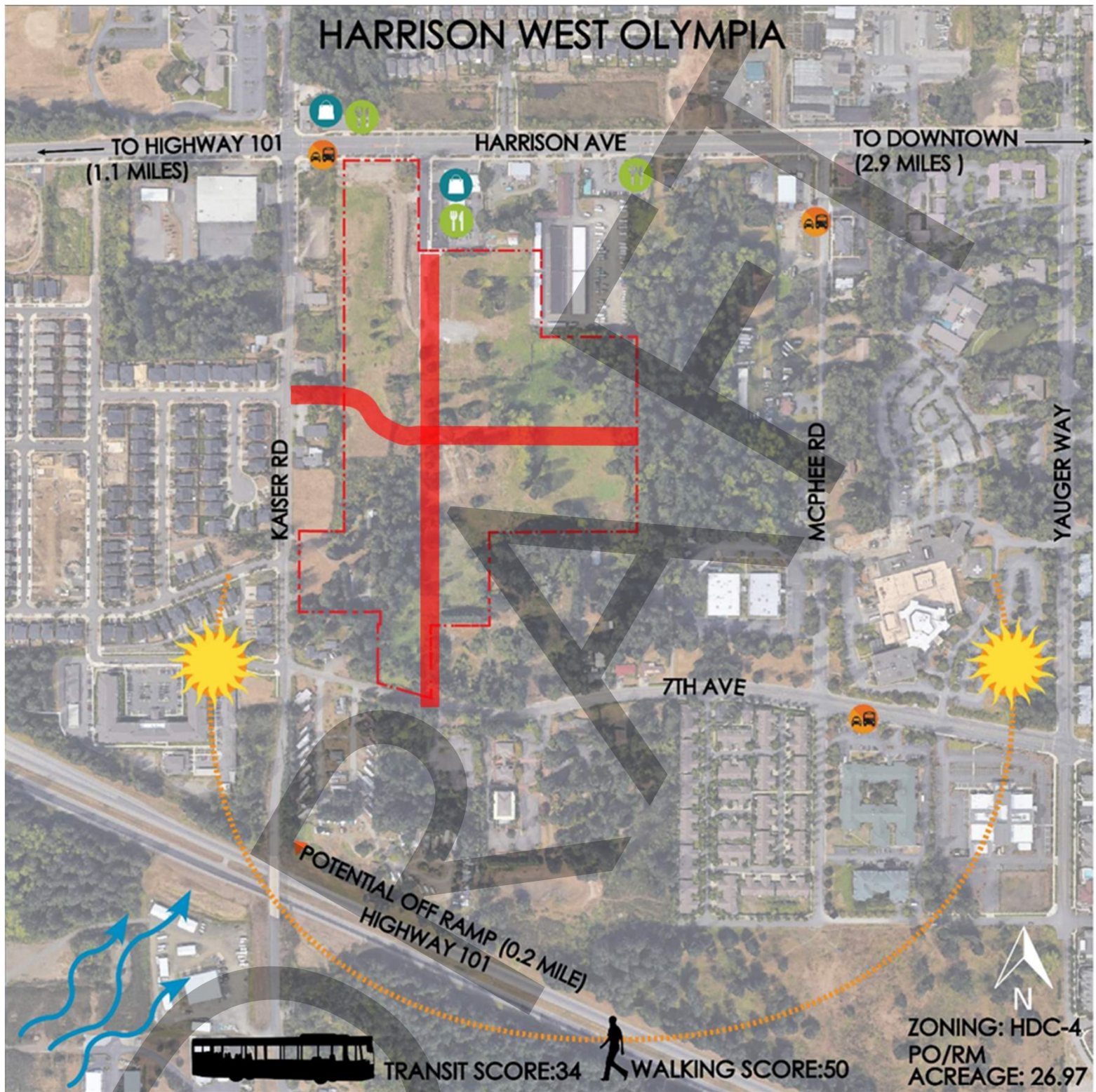
## MORE EXTENSIVE STORMWATER FACILITIES NEEDED

Soils on this site do not percolate well. Stormwater detention ponds or below grade vaults will be needed.





# SITE DATA: HARRISON

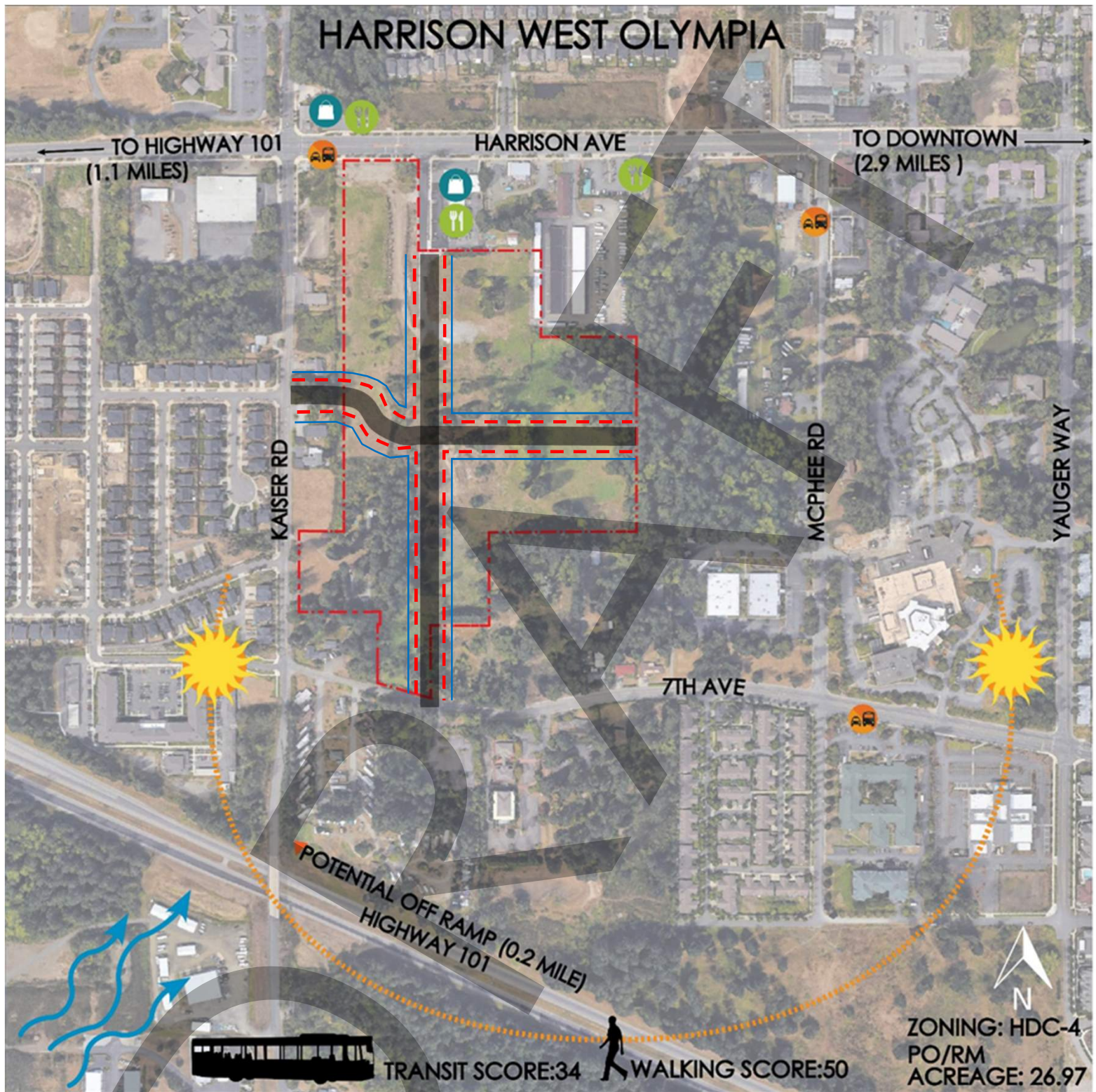


## REQUIRED ROAD EXTENSIONS

City of Olympia is currently requiring the addition of 1 connector road between Harrison and 7<sup>th</sup> Ave. as well as an East/West connector road off Kaiser



# SITE DATA: HARRISON

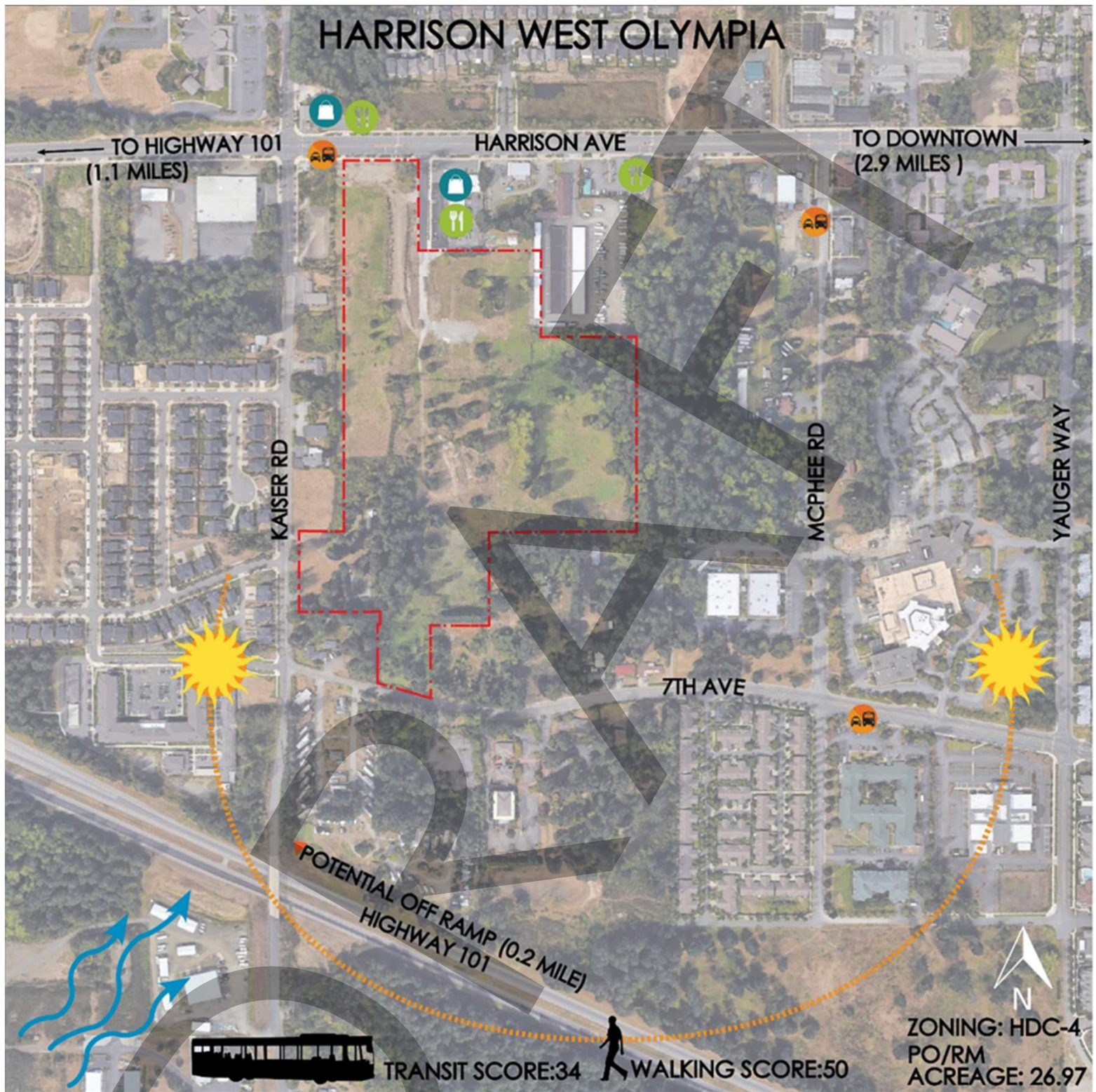


## UTILITY EXTENSIONS TO MATCH ROAD EXTENSIONS

Street improvements (such as: utilities, sidewalks, vegetation, street lighting) will need be required along new access roads.



# SITE DATA: HARRISON



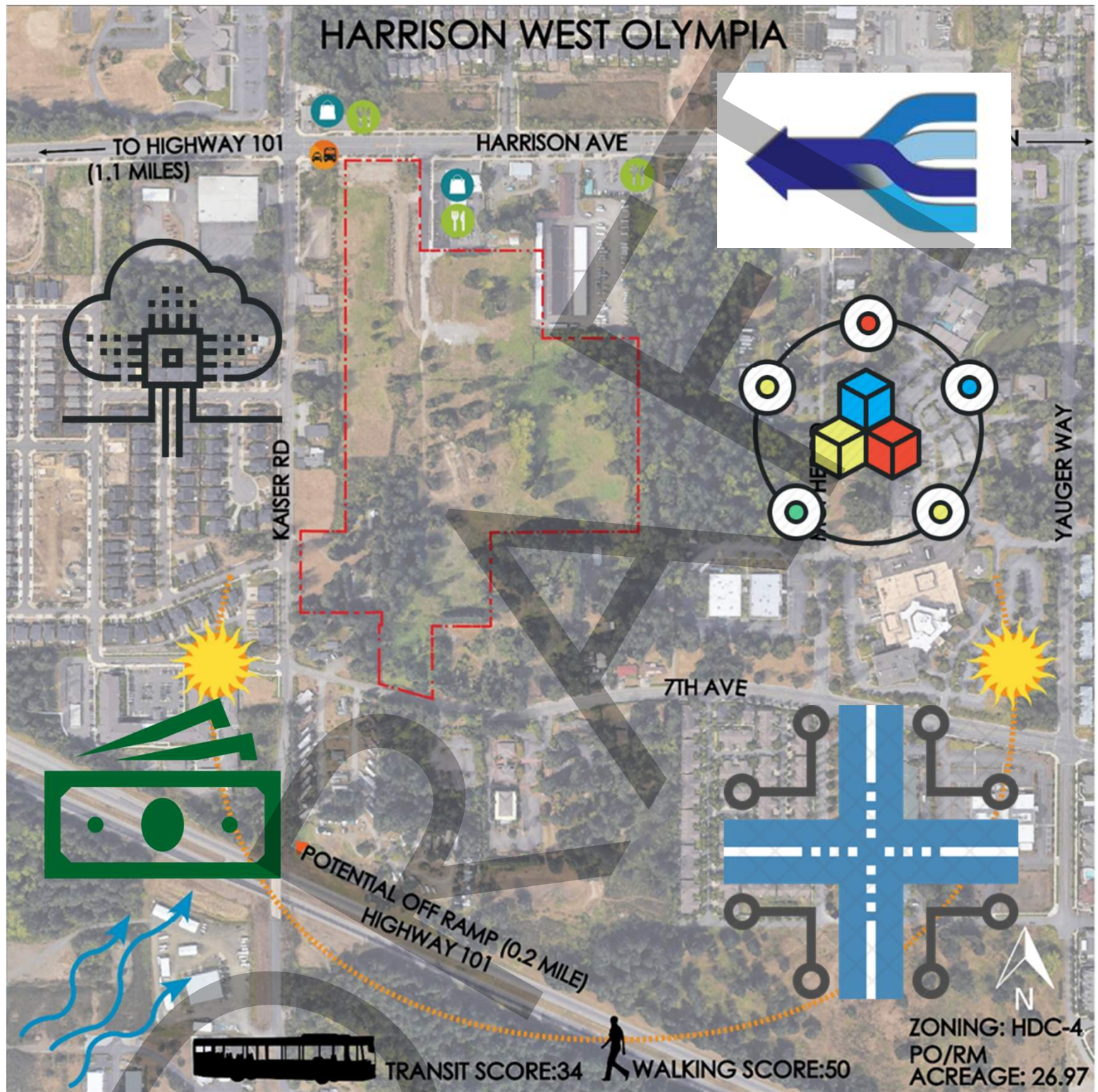
## UTILITIES CURRENTLY ON SITE

The perimeter of the site is currently supplied with the main utilities needed such as; Water, Sewer, Gas, and Electricity. These utilities will however need to be extended through the site to serve the new buildings.





# SITE DATA: HARRISON

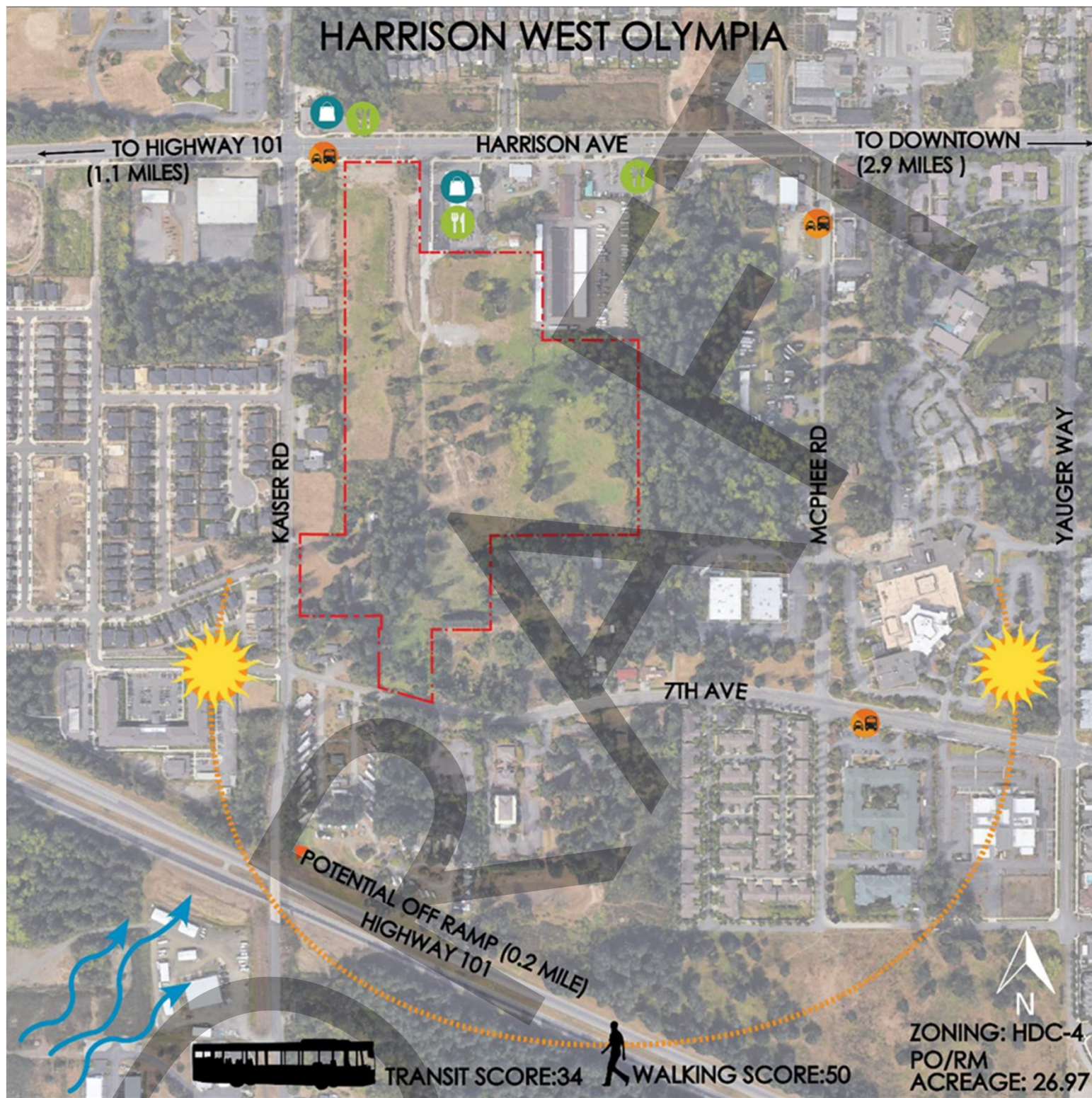


## POTENTIALLY HIGHER INFRASTRUCTURE COSTS

Due to the more remote location of the site there more than likely will be additional infrastructure costs to extend services to this site.



# SITE DATA: HARRISON



## TRANSIT SCORE 34 OUT OF 100

Transit score measures how well the location is served by public transit. This site is at the outer edge of transit service

## WALK SCORE 50 OUT OF 100

Walking score measures the ability for pedestrians to access the site as well as access to nearby amenities. This site has several nearby amenities.





PUBLIC OUTREACH PLAN

PUBLIC  
OUTREACH PLAN

IV.

THURSTON COUNTY COURTHOUSE FINAL REPORT

PUBLIC OUTREACH PLAN

DRAFT

## IV. PUBLIC OUTREACH PLAN

Thomas Architecture Studios and Thurston County planned multiple opportunities and methods to reach out across the County as part of the site evaluation process. The goal was to gather input and feedback on site criteria and the layout concepts for each of the three potential sites, from a broad range of stakeholders.

### Outreach Activities

#### *In-person participation opportunities*

- **15** Open House Sessions with a total of **202** attendees held in downtown Olympia, Lacey, Yelm, Rainier, Tenino, Rochester, and West Olympia
- **6** Staff Brown Bag Sessions with a total of **120** attendees

#### *Online participation opportunities*

- **20** Facebook posts with a total known reach of **19,040**, including **85** likes, **37** shares, and **25** comments
- **19** Twitter and Instagram posts with **37** likes, **6** comments/replies, and **8** retweets (Twitter and Instagram do not provide reach analytics)
- Project website with two online surveys posted, generating **57** responses

#### *Email outreach*

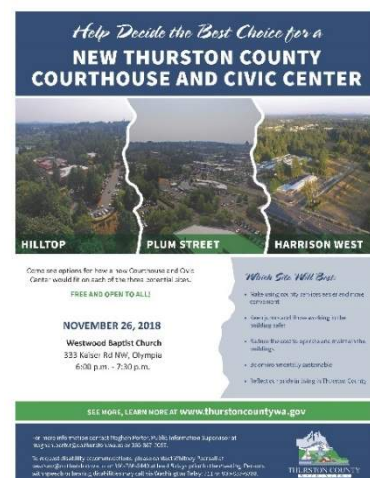
- Three emails to **769** individual recipients and distribution lists with an unknown total reach
- Three all-staff emails announcing the Staff Brown Bag Sessions

#### *Posters*

- Posting flyers for the October and November sessions at local spots around Yelm, Rainier, Tenino, Grand Mound, and Rochester
- Posting flyers on local neighborhood posting boards for the final open house in West Olympia

#### *Traditional mail outreach*

- Direct mailing of flyers on October 31 and November 19 reaching approximately **100** neighbors and businesses within a half mile of each site



## **Citizens Advisory Committee**

The Citizen Advisory Committee (CAC) provided important input to the public outreach plan and helped in encouraging participation by stakeholders. The CAC met six times over four months with TAS receiving briefings on the project's process, progress, and public input.

CAC members also participated in monthly meetings with the executive team including County and City of Olympia elected officials; answered questions and comments by phone, email, and social media; and attended public outreach sessions.

CAC members include:

Gerry Alexander  
Whitney Bowerman  
Michael Cade  
Molly Carmody  
Virgil Clarkson  
Monica Crawford  
Kyle Cronk  
Matt DeBord  
Sue Dubuisson  
Dean Foster  
Ann Freeman-Manzaneres

Aslan Meade  
Rick Nelson  
Dave Platt  
Christy Reynolds  
David Schaffert  
Scott Spence  
Jonathan Sprouffske  
Shauna Stewart  
Juanita Taurman  
Larry Whitaker  
Angela White

CAC members helped the County and TAS build email lists and a process to invite input from members of stakeholder groups with interests including real estate, design, workplace, judiciary, legal profession, environmental sustainability, advocacy for underrepresented populations, neighborhood impacts, and cultural heritage.

The CAC discussed that once a site is selected, the public outreach effort should be enhanced and extended to assure members of the public are aware and have additional opportunities to engage in the process.

## **Project Website**

The County created and maintained an up-to-date website about project activities including open house notices, meeting dates, CAC information, images and information about each site, recordings of the sessions, and surveys. The County homepage also houses a direct link to the project website and is listed at the top of the page for easy access.



## EXPERIENCE WITH EXISTING COURTHOUSE

# IX.

THURSTON COUNTY COURTHOUSE FINAL REPORT

## EXPERIENCE WITH EXISTING COURTHOUSE

DRAFT

## **IX. EXPERIENCES WITH THE EXISTING COURTHOUSE**

Stakeholders provided important input about their experience with the current courthouse during outreach meetings and briefings. These comments came from people who experience the facility in a range of circumstances from jury service to buying a license to residing in a courtroom to represent a defendant.

Highlights of their comments follow.

### *Atmosphere and basic amenities*

- Buildings do not feel welcoming
- Wayfinding is difficult—there is no kiosk; a docent or greeter is needed
- Challenging wayfinding
- Artless
- Little sense of dignity in the space
- There is no family friendly space with children's furniture
- There are very little food or snack options
- Men's and women's restrooms need to be sized appropriately
- Poor cell phone reception within the buildings
- Poor HVAC

### *Security*

- Limits on site access – safety/ security concern
- Does not feel secure for both public or staff
- Little queuing space for security makes it feel crowded

### *Parking and transportation*

- Parking is difficult
- Parking issues
- Limited car/transit options

### *Court-related functional needs*

- Currently there is no space to talk with clients
- Jury staging occurs in the public hall
- No meeting space available. Need rooms with windows or open feeling
- Needs to serve: attorney/client, advocate/client, victims, witness etc.



# EVALUATION CRITERIA

X.

THURSTON COUNTY COURTHOUSE FINAL REPORT

**EVALUATION CRITERIA**

**DRAFT**



## X. EVALUATION CRITERIA

TAS asked stakeholders what criteria are most important in evaluating the three alternative courthouse sites. Criteria covered five broad categories: community values, community development, functionality, transportation/access and implementation.

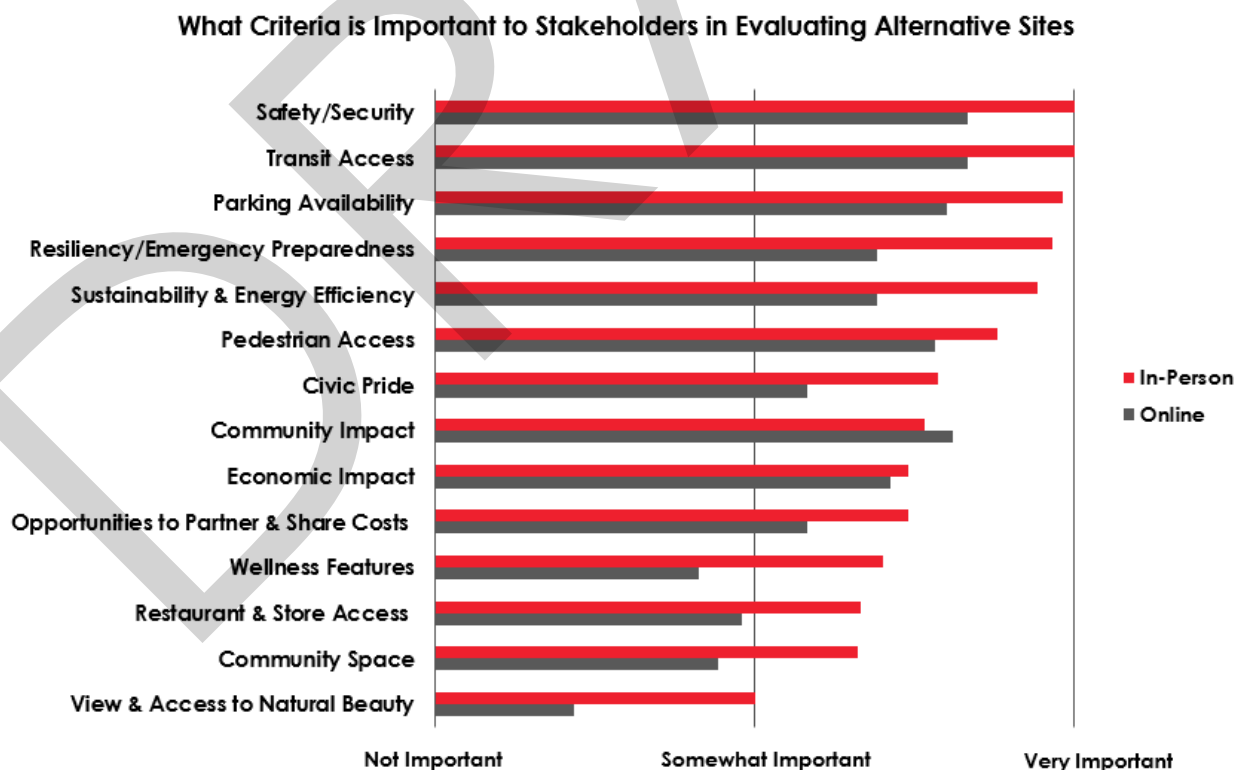
Input was gathered in-person during sessions in Yelm, Tenino and Olympia. Input was also gathered using an online survey hosted on the County's website and promoted on social media.

### COMBINED STAKEHOLDER INPUT

The findings are combined below to show the general sentiment of those who participated. The three most important criteria for both those who responded in-person as well as those who answered online were safety/security, transit access and parking availability.

The two groups were similar in their ranking of least important criteria, though the online respondents rated views and access to natural beauty far lower than the in-person attendees.

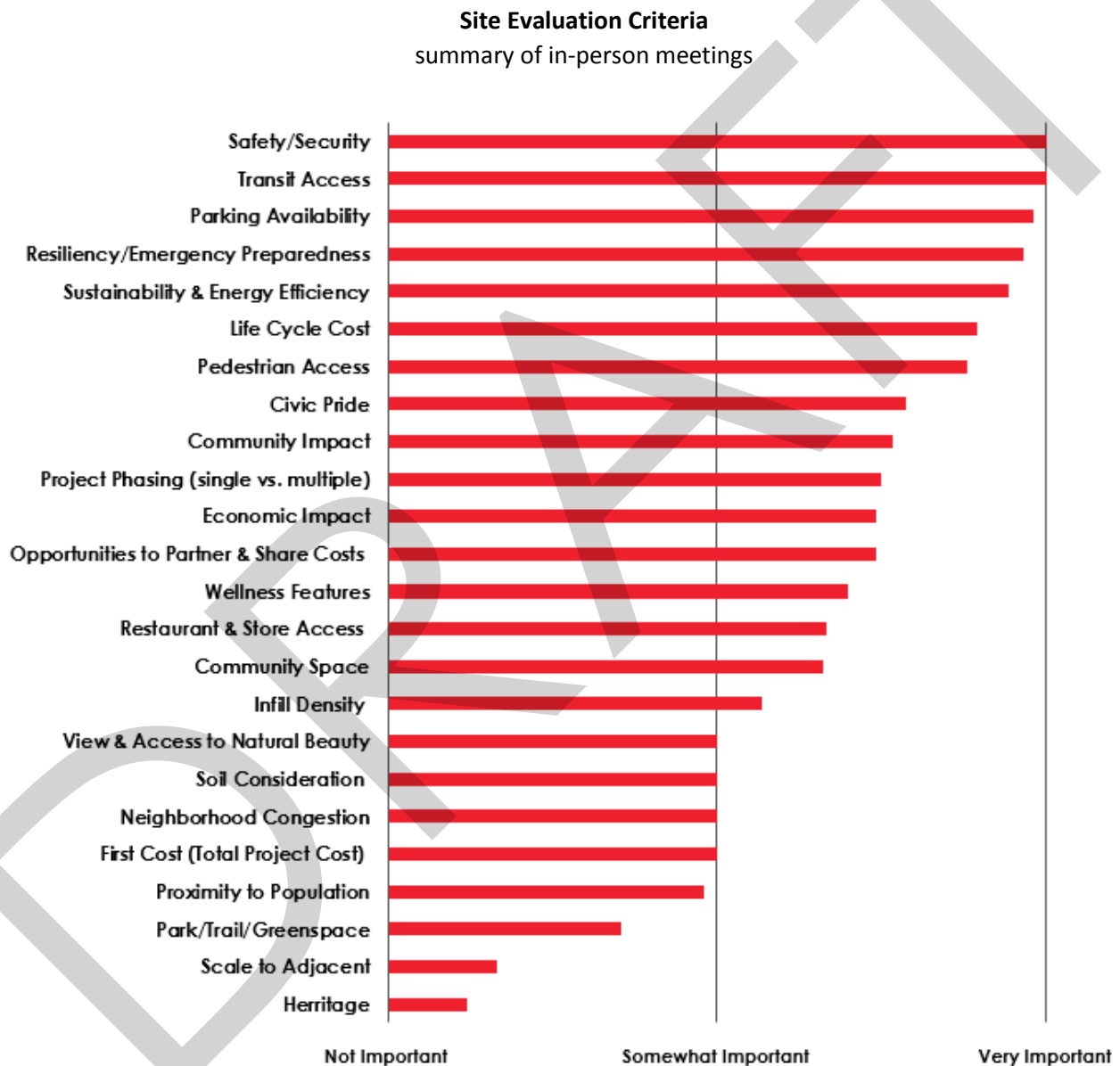
The online respondents rated all criteria lower than the meeting participants, with the exception of community impact; the two groups gave community impact a nearly identical rating between somewhat and very important.



Data may not represent the sentiment of Thurston County residents overall. Data was gathered informally, and the list of criteria considered varied somewhat in-person from the online survey.

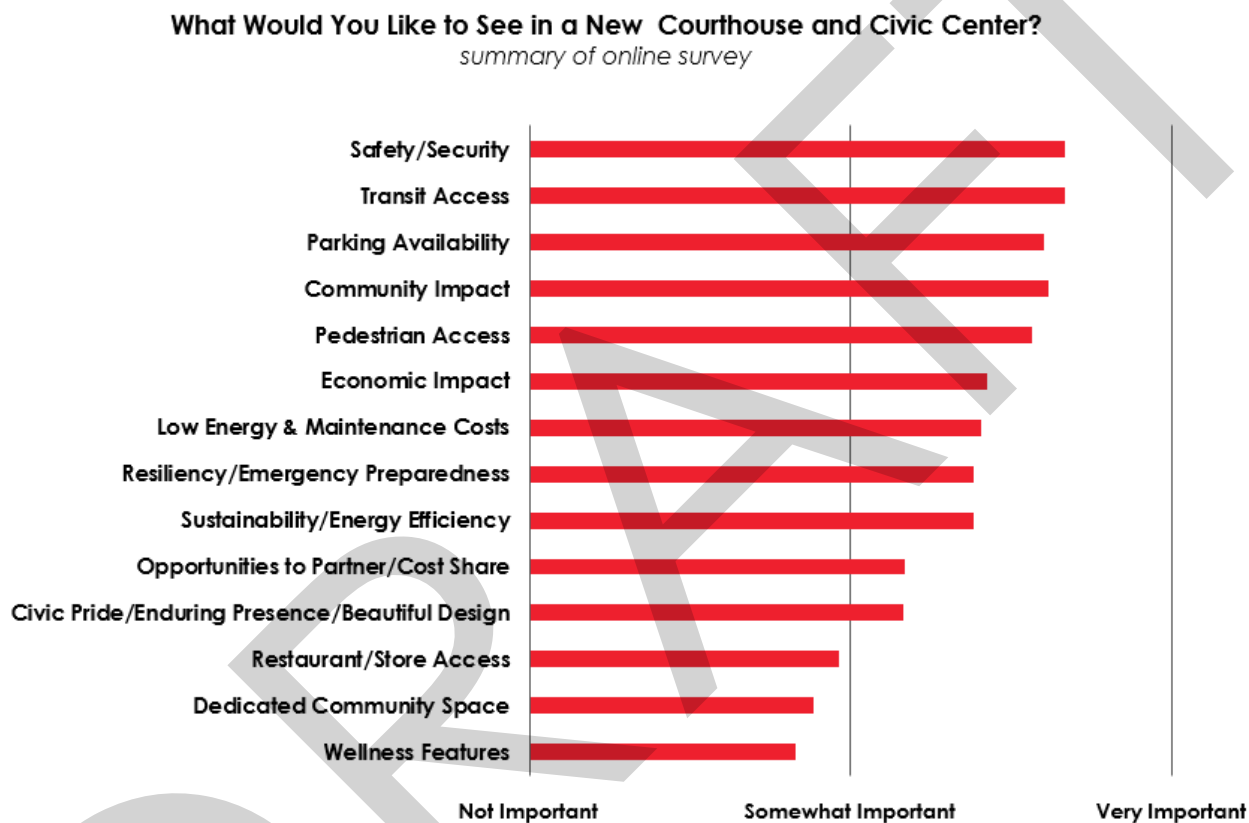
## IN-PERSON STAKEHOLDER INPUT

Input about the importance of various evaluation criteria was gathered and summarized informally during the meetings. As shown on the graph below, attendees felt the most important criteria are safety/security, transit access and parking availability.



## ONLINE STAKEHOLDER INPUT

The online survey was completed by 56 people as of December 3, 2018. (The survey will remain open for one more week.) As shown on the graph below, online respondents ranked safety/security, transit access, parking availability and community impact as most important.



Online respondents also were asked four open-ended questions. Highlights of their responses follow.

### 1. Are there any additional [site selection] criteria you think should be considered?

#### Multiple mentions:

- Green buildings
- Distance and ease from all parts of the county
- Cost to acquire and build
- Lifecycle cost
- Traffic impact
- Adaptability for future expansion

#### Single mentions:

- Natural surroundings
- Access for bicycle users



- The need to revitalize downtown Olympia
- Effect on surrounding neighborhood
- Opportunity for parking at Plum Street to be used off-hours by citizens
- Safety from sea-level rise
- Redevelopment of an existing site

**Also mentioned were these design considerations:**

- Separate entrance for staff and attorneys
- Inclusive features—unisex restrooms, step stools for shorter people, etc.
- Restroom safety
- Shelter from weather between buildings
- Communications connectivity

## **2. What would make a new Courthouse and Civic Center distinctly Thurston County?**

Following are the verbatim survey responses. Environmental sustainability was the most frequent theme of the responses.

**Environmental sustainability**

- Sustainable, low impact, energy efficient design and construction materials and practices.
- Sustainable with low energy inputs needed to operate. Built with recycled or renewable building materials, timber and wood come to mind.
- Energy efficient building w/ environmental impact considerations.
- Sustainability. Forward-thinking design embracing natural/local elements.
- Accessibility.
- Sustainable and energy/location efficient.
- Our region is green in every sense of the word. I think that creating an eco-friendly structure (with solar and other cost saving, sustainable features) would be fantastic!
- Forward thinking design/building practices/thoughtfulness around energy efficiency and products used. Considering our environment and taking into consideration the aesthetic of our historical community.
- Buildings made out of wood to honor the forests that were here and are still here. The complex could be built entirely out of cross-laminated timber - the first in the region!
- LEED certification

**Natural surroundings/landscaping**

- Blending the environment with the building design and setting.
- Inclusion of trees in building layouts and landscaping
- Some green space.
- Integration of indoor and outdoor space especially covered open air spaces.
- I appreciate the natural Northwest beauty that surrounds the current courthouse complex. If the courthouse is moved, it would be awesome if it incorporated some large fir/maple trees, native plants, etc.
- Connected to its surroundings, i.e., bring the outdoors inside.

- Beautiful landscaping,

### Equity and inclusion

- Equal access to all teams, groups, organizations regardless of economic or social status, gender, age, etc.
- An honoring of diversity - Thurston County is a wonderfully diverse community,
- let's celebrate this!
- Openness and reflective of Thurston County's diversity -- economic, geographic and demographic

### Views

- Views of the water and Capitol would be nice, but those should be lower priority. I would weight a highly functional/efficient Courthouse/Civic Center over "distinctly Thurston County".
- A view of the Capitol!

### Downtown location

- Put it downtown so It's most visible. West side locations are inferior.
- Downtown Olympia site- central to all areas of TC.
- I think a downtown location would help bring economic stability to the area.
- downtown Olympia location; near City and State offices
- Location in close proximity to the state capitol building.

### Other location

- Leaving it in its current beautiful location.
- Stay away from the Plum St area, in a bad earthquake we would lose the 2 critical facilities. City of Olympia city hall and the TC new court house, the bridges will be down. Stay with the Harrison Ave.
- Having it located where it has access for people who drive, ride the bus, walk, cycle.

### Other

- To separate them. Olympia wants a civic center, not the county. A county courthouse is not a place to co-locate a civic center as too many timing and traffic conflicts will occur. The courts are not a business and should not be in a prime business district location, wasting prime business property space. Neither should city halls, but Olympia has already made that mistake. A civic center is a business and needs to be near restaurants and businesses.
- Avoid bland design and cheap construction
- Courts for indoor sports. Events for families of all ages.
- Parking. IF you are going to build a civic center centralize closer to the center(s) of the tri- city area. Quit trying to encroach on what used to be real rural living.
- Have it also be a source of pride for the community; wi-fi accessible; childcare on site; technologically advanced courtrooms.
- If it was a gift from a donor.

### 3. **What additional information do you need to be better informed?**

#### Cost

- Costs and effects on the various sites.
- If not in Olympia, are there any county-owned properties that might be less costly to develop?
- WHO is going to pay for it, how much and why
- Cost to acquire each site, cost to relocate the existing services if the hill top site is to be considered and an environmental and soils report for each site to determine any potential environmental clean-up costs or structural enhancements that may be required for the soils to be buildable

#### Location

- Is there a reason the new court complex MUST be within the city of Olympia?
- Is there not a more central location in east or southeast Olympia or county property that might be a better location?
- Will it help downtown clean up if located there?
- Potential negative impacts on the community or about the location. I hope that the old courthouse/city building on Plum St wouldn't be torn down- it is so architecturally interesting and unique.

#### The need and scope of the project

- What is the entire scope of the project? What is the long-term vision?
- I need to know WHY it's needed NOW. Why you can't wait until you have the Jail situated and fully staffed without mandatory overtime current staff.
- Courthouse needs in terms of rooms, essential spaces, security, etc. Things that an expert in our courts system or courts design would know better than I do.
- Estimated size of new building and number of courtrooms, meeting rooms, etc. Sketches showing different configurations as to height, width and mass, so we can better visualize the most appropriate location.

#### Civic Center

- What do you mean by Civic Center? What uses would be included?
- Why is the civic center idea coming into this discussion?

#### Outreach approach

- I appreciated the mailer that was sent out to alert citizens of this project, and the ability to share my opinions. Thank you.
- Who are the members of the Citizens' Advisory Committee?
- As a member of the CAC I feel I am being well informed - thank you!

#### Other

- Demographic/population information for each location.



- How the site selections are being made, and what priorities are given to the above (and other) criteria.

**Do you have any additional comments or feedback?**

**Location**

- Keeping the current location is the best way to both be and make a statement of sustainability and rebirth!
- Don't build it in downtown Olympia
- The courthouse should stay in the same location.
- Please, please do not build this on the Harrison site. That area is a low traffic area with a beautiful brand-new community right behind it. That would greatly impact that brand-new community. We all bought our houses because of the safe area and this would all change with a courthouse in our backyards and lower our property values. Please leave it at its current location as those that live in that area bought knowing it was there. Changing the dynamics of a community like that is not ok.
- Due to parking constraints, it is my recommendation that the Harrison location be selected as the most feasible option.
- I would hate to see the old city hall torn down. Wouldn't it be better used as a museum space, complemented by the Japanese gardens?

**Other**

- Please consider the history of Thurston County courthouses and create a design that is lasting and can be maintained with pride for decades. A timeless design is more desirable than a design that has features of the moment (orange doors, orange roof, etc.)
- Parking garages should be required at all three sites to decrease the overall footprint of the facility. Thus, decreasing the need for land inside the GMA
- Please do NOT put this new Court House & Civic Center far away from public transit. These new facilities should be within a few steps to all public transit!!!
- Options should include taking the courts function out of the current courthouse now and moving it to an existing space. For example, space could potentially be leased at the mall. This has existing parking, restaurant access, bus service etc. Also, other sites where existing buildings may be available and could be renovated for just the court functions. Then there would be room for other courthouse functions on existing site and potentially room to renovate incrementally.
- Is there anyone who trusts current leadership with this decision? I never meet folks- Republicans OR Democrats who have faith in this Commission or Leadership. This decision would be more Good Ole Boy Business.
- Keep tax payers' costs low, while providing an energy efficient and safe structure.
- I have some feedback re: bathrooms, from two perspectives:
  - 1) The nursing mother - I'd like to see the facility offer comfortable places for employees and visitors to nurse in privacy if they so choose. As well, all bathrooms should offer changing tables.

2) Valuing gender diversity - I'd like to see the county explore best practices for bathrooms in new civic buildings. How can they best honor gender diversity and be inclusive of visitors and employees who are transgender, gender nonconforming, etc.? Does this mean having men's, women's and unisex bathrooms? Does this mean having all bathrooms be unisex? I imagine there are research and best practices in this area.

- Assume this is at least a 100-year decision, preferably through 2150.
- In order to get the public on board, this needs to be a beautifully designed complex that will create civic pride and solve the significant issues of the current courthouse and be an asset to the community for the next 50 years.
- Thank you for this survey.

# XII.

THURSTON COUNTY COURTHOUSE FINAL REPORT

## PUBLIC FEEDBACK

DRAFT





# PUBLIC FEEDBACK: PUBLIC FEEDBACK SUMMARY

## **XII. PUBLIC FEEDBACK**

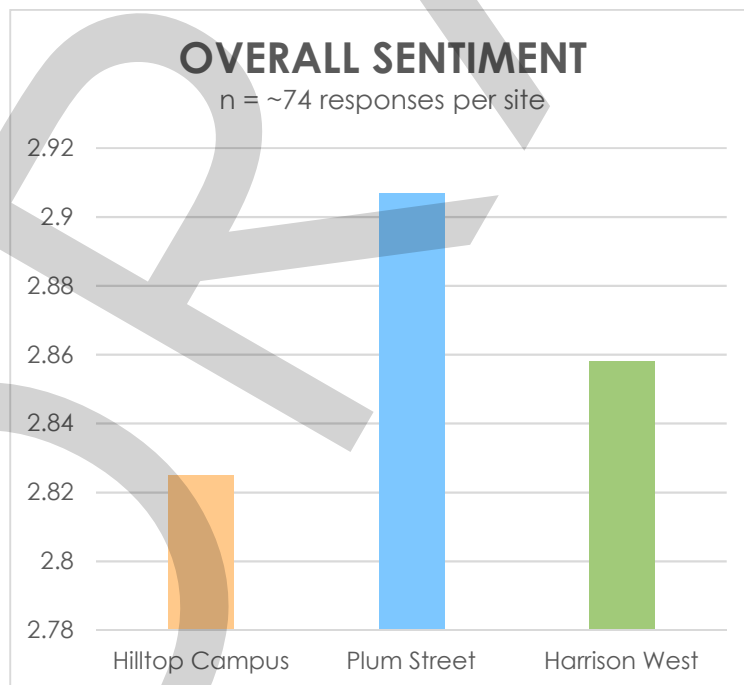
### **Methodology**

About 74 participants who attended various outreach sessions completed self-administered surveys regarding their opinions about each of the three proposed building sites for a new Thurston County Courthouse and Civic Center. Each survey was composed of five Likert scale questions and two narrative questions. The scale ranged from one to four, one being strongly disagree and four being strongly agree. Participant groups included the general public, the Community Advisory Committee (CAC) and County employees.

This data represents the views of the participants and is intended to provide qualitative insight for considering where to build a new County courthouse based on Thurston County residents' sentiment. Results from this small sample may not be representative of the entire Thurston County population.

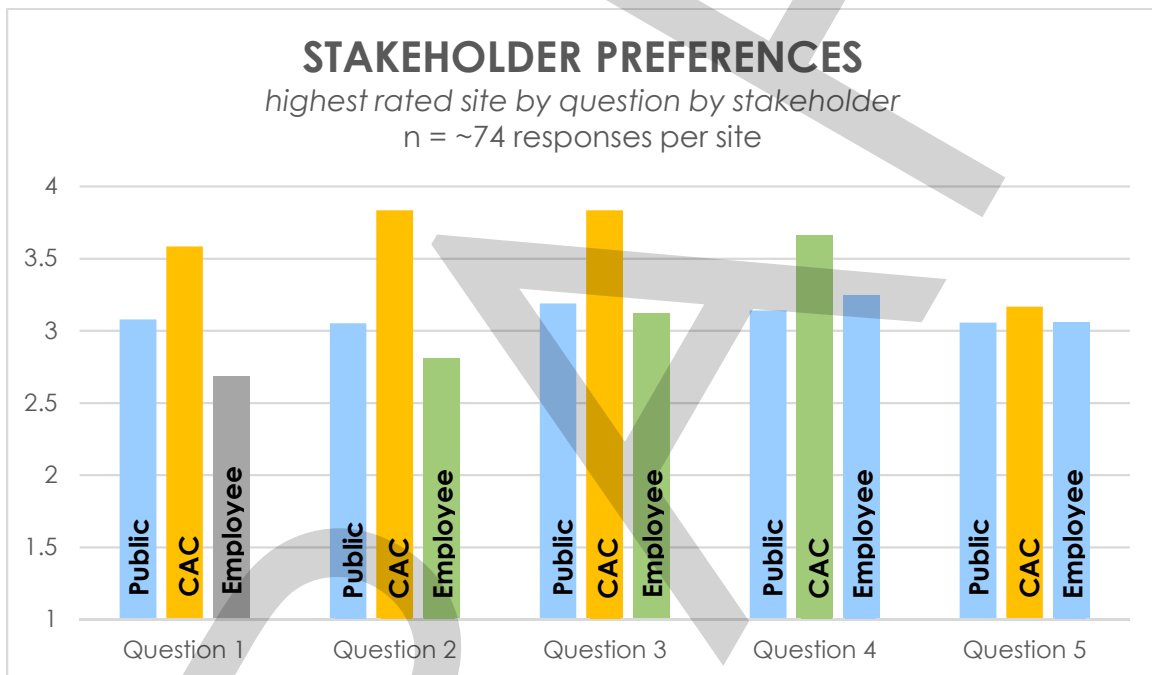
### **Data Analysis**

Out of ~74 responses per site, the Plum Street site received the highest ratings. On a four-point scale, the Plum Street site received an average score of 2.91. The Hilltop Campus site averaged at 2.82 while the Harrison West site received an average of 2.86. 40 percent of all participants responded that they strongly agree the Plum Street meets the criteria of all five questions. Comparatively, 21 percent of respondents strongly agreed that the Hilltop Campus would meet all five criteria, and 25 percent strongly agreed the Harrison West site would meet all five criteria.



24 percent of respondents strongly disagree that a courthouse for the Harrison West site is a great match for the surrounding area. Local residents in the Harrison West area expressed concern about the site being located in a mainly residential area.

Sentiment about the sites varied among stakeholder groups. While respondents from the Citizen Advisory Committee (CAC) generally preferred the Hilltop Campus and Harrison West sites, other respondents strongly preferred the Plum Street site. Employees favored Plum Street for the opportunity to contribute to economic development and civic pride, though they viewed Harrison West to be the best use of property and provide the best workable option. They were divided over whether Plum or Harrison West is the best match for their respective surrounding areas.



4 = Strongly Agree  
3 = Agree  
2 = Disagree  
1 = Strongly Disagree

■ Hilltop Campus  
■ Plum Street  
■ Harrison West  
■ Plum/Harrison tie

Question 1: A courthouse for this site is a great match for the surrounding area.

Question 2: The proposed building scenarios for this site are a great use for this property.

Question 3: The proposed building scenarios for this site will lead to a workable option for the County.

Question 4: Selection of this site would promote economic development in the surrounding neighborhood.

Question 5: A Courthouse on this site would help promote civic pride.



## **Narrative Analysis**

While respondents reported that the greatest negatives for each site are limited parking options and an increase in traffic congestion, there is potential to improve each site.

### Hilltop Campus Site

The Hilltop site offers the greatest opportunity to improve the visitor experience by creating more clearly identified parking connected to the primary entrance, closer access to restaurants and businesses, greater access to transit and clear signage to direct visitors. One person pointed out the benefit of separating restrooms for the public and court staff to increase safety. Two participants saw the opportunity for a convention center or rentable space to generate money to offset costs of the new courthouse.

While some participants believe freeway access is the best at this site, many participants are worried that the proposed building would increase traffic in an already congested area. Others are worried the proposed building would compete with the capitol building and downtown skyline. The site's walkability and distance from the downtown area pose concerns for many people, as does the project's phasing schedule. Several respondents would prefer if the building were constructed in one phase.

Overall, employees want to know how construction would impact them, and they believe this location is not as convenient as the Plum Street location.

### Plum Street Site

The overall sentiment is strong for the Plum Street site, with 18 participants agreeing that this would be the best option for the new courthouse. They believe a courthouse on this site would boost the downtown economy and would be the most convenient for residents to visit. Several respondents noted that the Plum Street site would be the easiest to integrate into the city with nearby transportation services and amenities, as well as access to an urban center.

Many people agree that the existing Yashiro Garden should be integrated into the Plum Street site, if chosen. They say the garden would enhance the visitor experience. Several people noted the positive, calming effects gardens may have on potentially tense conversations between attorneys and their clients.

Participants believe the visitor experience at this site would be improved the most by constructing ample and more secure parking, integrating the existing garden and green space into the final design, making the site accessible for those with disabilities, and making spaces welcoming for members of the LGBTQ community.

Respondents' greatest concerns at the Plum Street site include rising water levels and tsunamis, the homeless population in downtown Olympia and the large scale of the proposed courthouse building. Several participants noted that having a County courthouse on this site might cause confusion between the City of Olympia and Thurston County. A couple of people are concerned about safety issues associated with the homeless population in the downtown area, citing that the location does not feel walkable and safe.

Employees are divided about this site. Half say this site would be great for economic development and the convenience of residents. The other half expressed that having a location so close to downtown would be detrimental, causing unacceptable congestion for commuters in and out of the City of Olympia.

Several participants responded that the Plum Street site would be the most convenient to access nearby amenities, such as the post office, restaurants and downtown businesses.

#### Harrison West Site

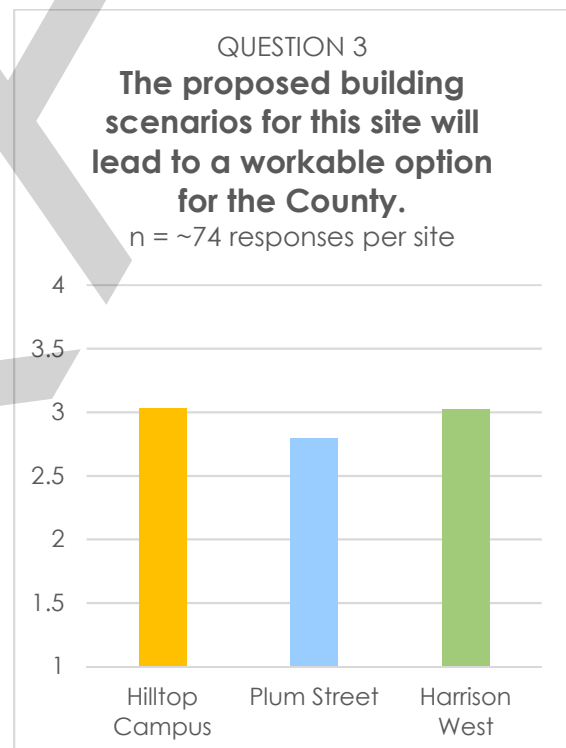
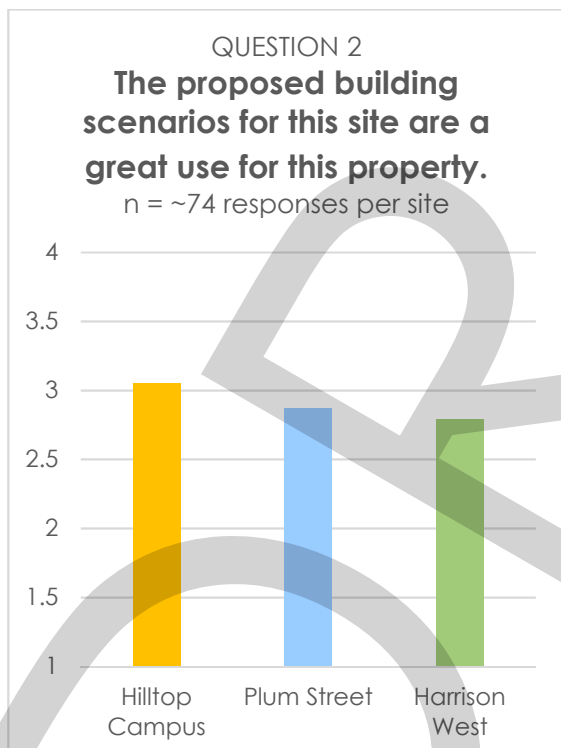
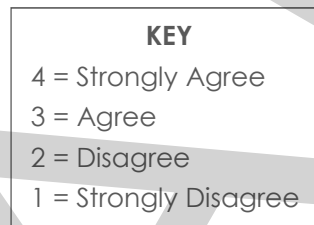
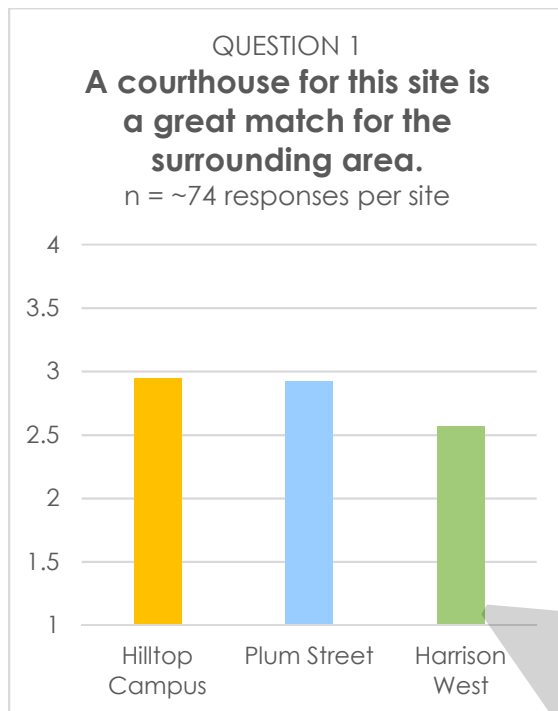
Participants saw several opportunities for an enhanced visitor experience on the Harrison West site. The extensive undeveloped acreage would provide ample space for future expansion, as well as opportunity to address potential issues, such as traffic and the need to expand the road network. Many people would prefer a parking structure over all-surface parking.

Participants would most like to see freeway access completed at the same time as the courthouse to ease congestion and accommodate the inevitable traffic influx. Generally, participants would like to see opportunities for local businesses and restaurants on the site. For several participants, access to green spaces and walking trails would improve the visitor experience.

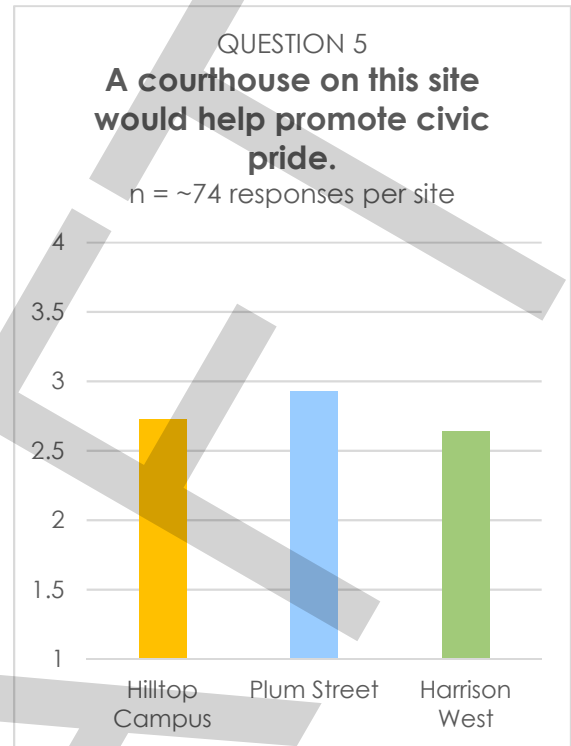
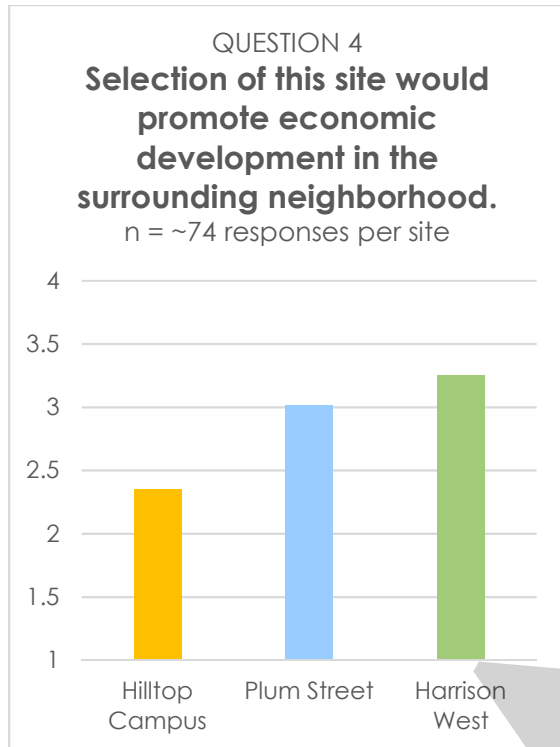
The Harrison West site's biggest weaknesses are its distance from the city center and its location in a highly residential area. Several participants believe that the Harrison West site is too far away for residents in the developing areas of Lacey and Hawk's Prairie. Nearly all employees believe this is not the best option for the next County courthouse due to its distance from the city center, lack of freeway access and preference for building where the County currently owns land.

The two Harrison West neighborhood residents who participated in this survey oppose the site because of potential safety hazards for the community and its children, and the burden on residents who chose the neighborhood to get away from high traffic volumes.

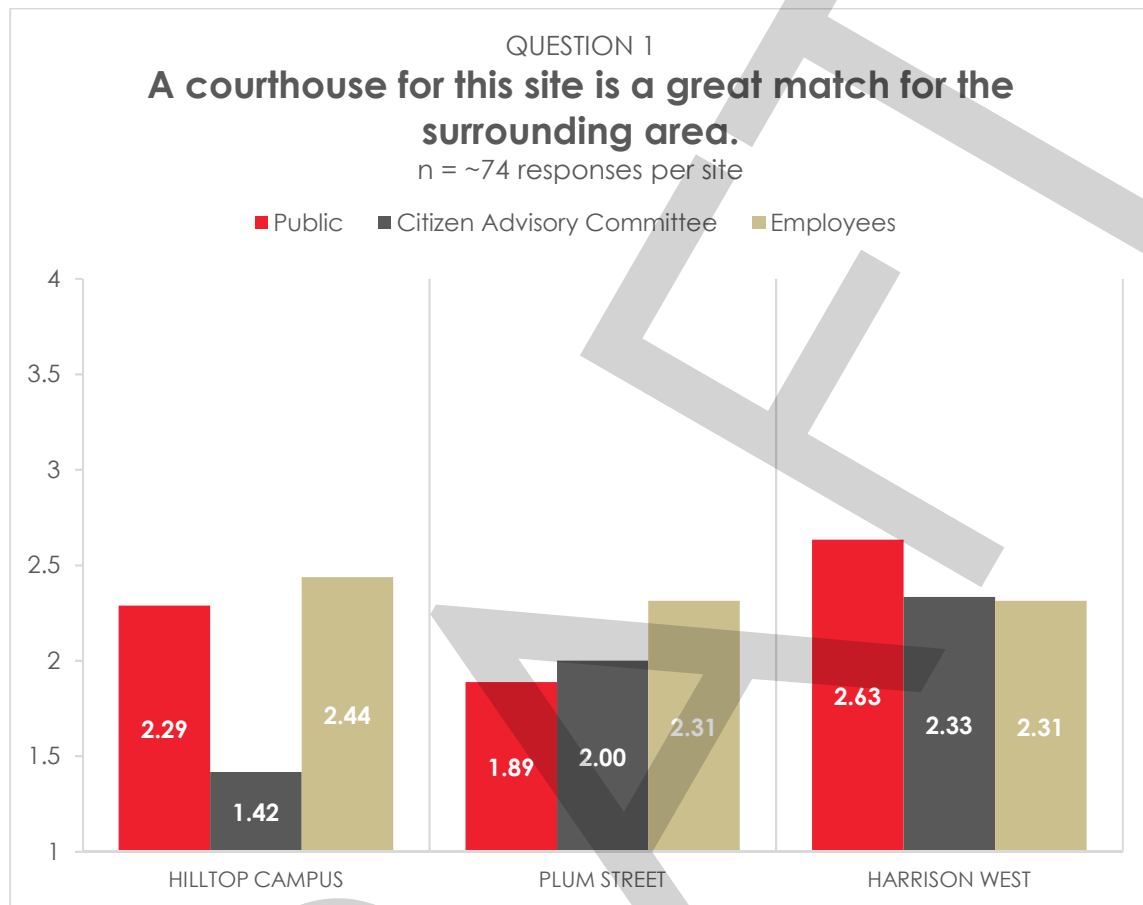
## Site Ratings by Question





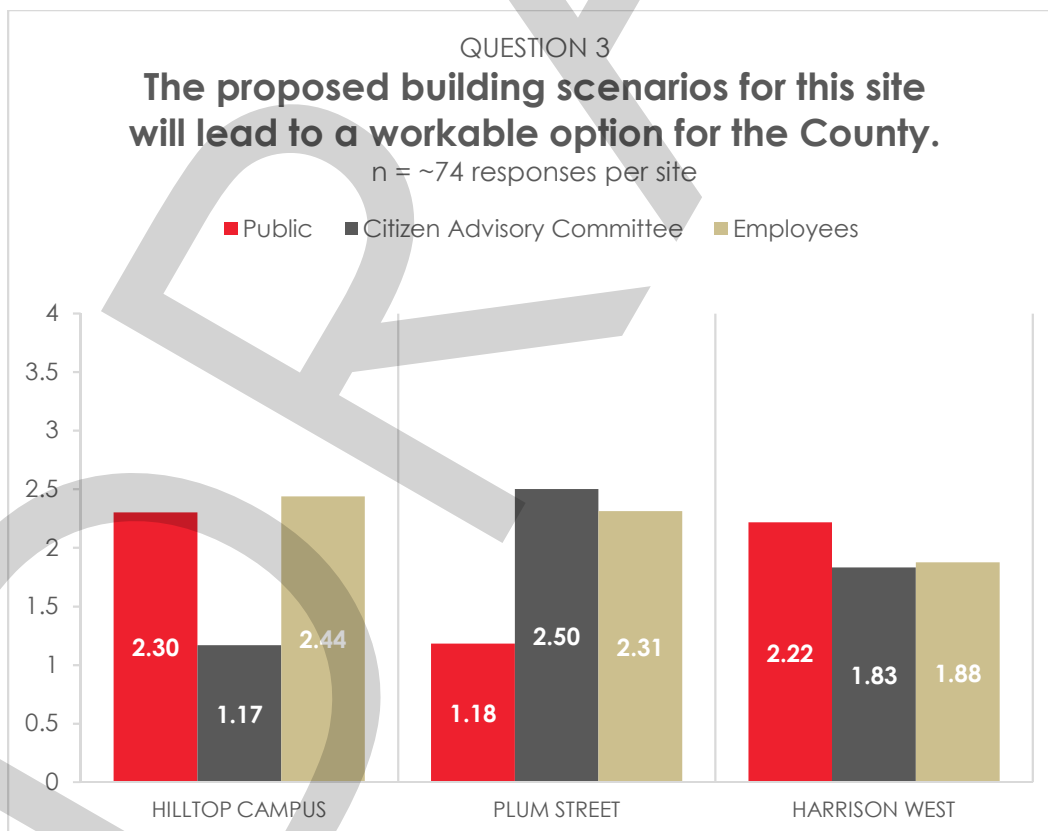
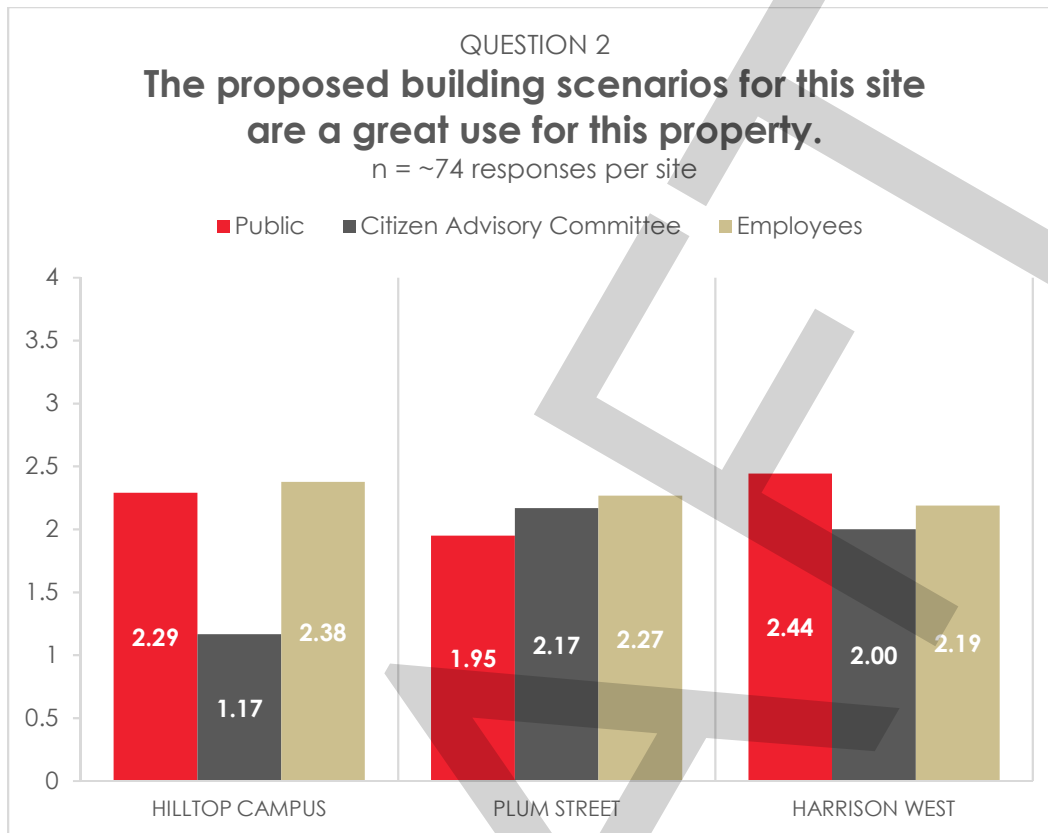


## Site Ratings by Stakeholder by Question

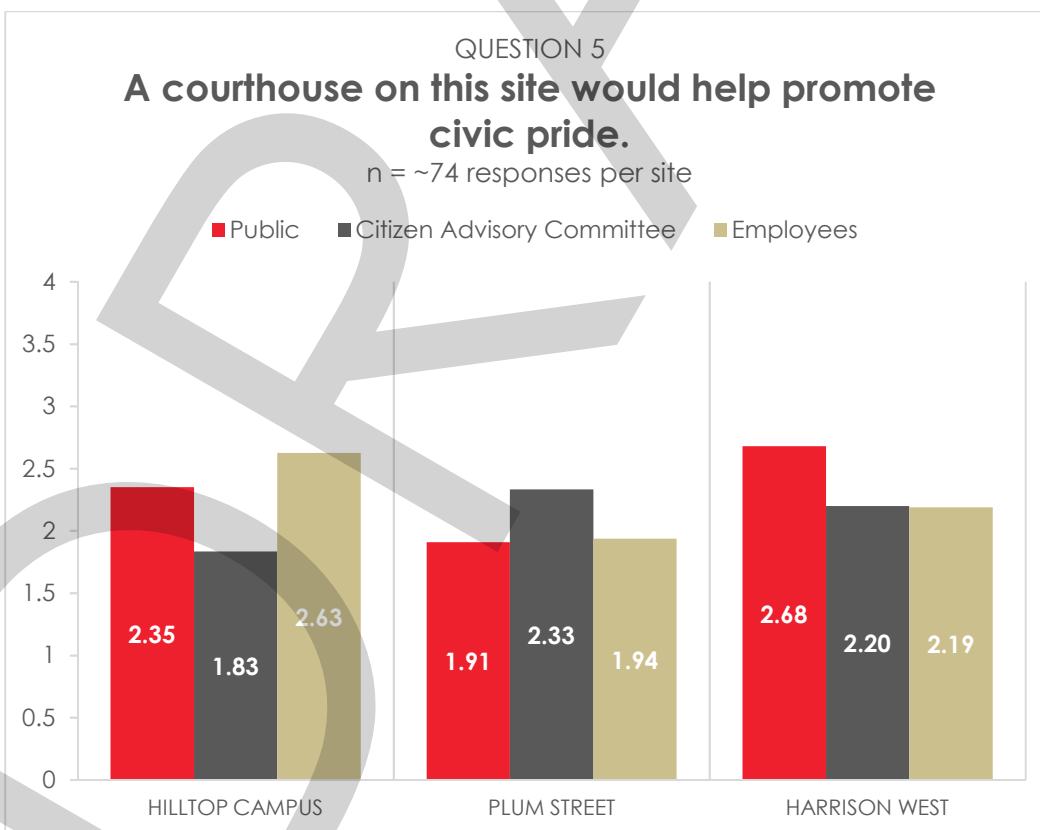
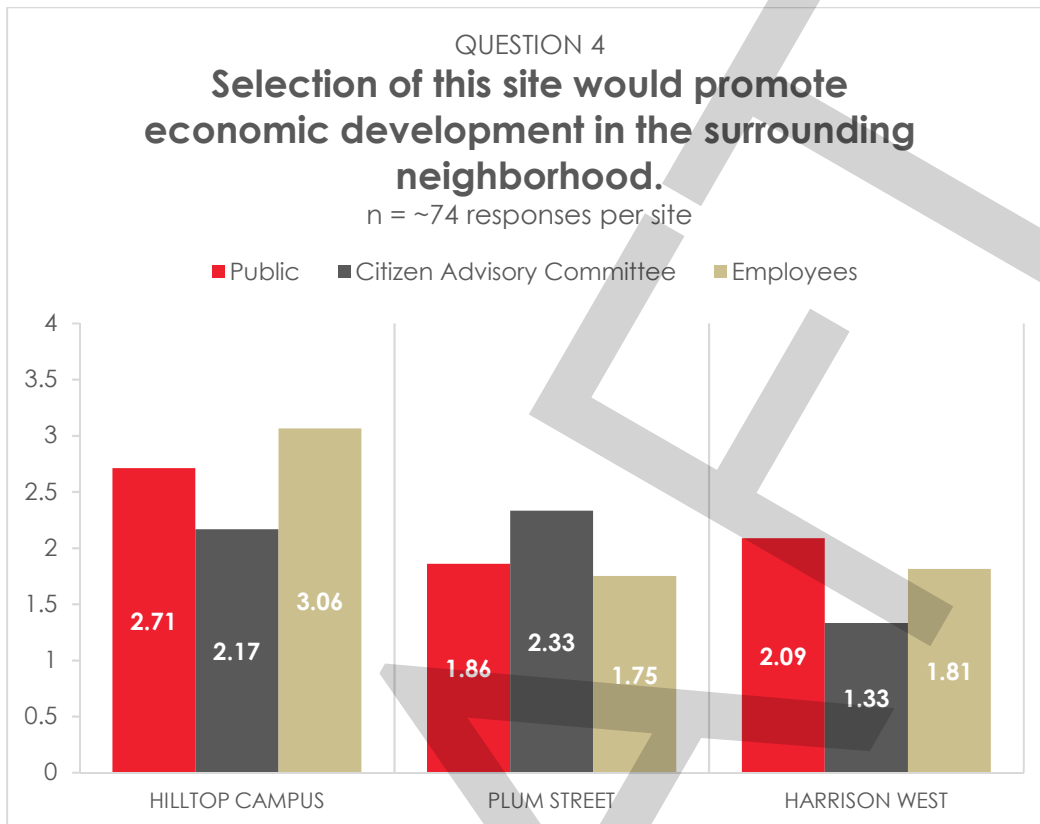


### KEY

- 4 = Strongly Agree
- 3 = Agree
- 2 = Disagree
- 1 = Strongly Disagree







## PUBLIC FEEDBACK: VERBATIM COMMENTS

## XII APPENDIX: VERBATIM COMMENTS

### Question 6: What would improve your experience when visiting a new Courthouse & Civic Center on this site?

#### Q6: HILLTOP CAMPUS

##### Community Advisory Committee

Easy access to parking garage, traffic flow

Rentable space to generate \$ to offset costs (convention center)

I would like to see the courthouse have an attractive ceremonial entrance that all visitors would enter - this entrance could then lead to all buildings in the complex

More parking. More natural organization & signage so it's obvious where to go. Parking connected to primary entrance.

Adequate parking close to buildings. Protect from noise

##### Employees

Better parking

too complicated, too many steps

A view is a low priority and it should be.

Address impact to users / employees during long term area construction.

Better parking, better transit access

More options for restaurants and shopping would help.

This could re-develop the current site, which "works" location-wise but is limited due to current building sizes, conditions, etc. I don't believe it could change surrounding development because development has already occurred around current courthouse.

Alleys to and from the buildings would have to improve.

##### Public

I've never liked this site - too crowded, hard to get to. BUT if most cost-effective, designs would work

Available low-cost meeting space

Parking, consumer services (shops, restaurants, etc.)

Easy access from bus stops and parking to courthouse complex. Incorporate outdoor seating and amenities for public and staff. Design site and buildings to be sustainable, energy efficient and built and designed using environmentally responsible "green" features and methods. This should include all facets of the new courthouse complex - buildings, parking, landscape and maintenance. Parking needs to be addressed.

Ample parking.

Use smart angular parking for full size vehicles to reduce overall parking lot foot print.

Clear Signage

The main barrier being access, I'm not sure much could be done to improve the limited access

Parking is currently terrible

Switchback walking trail down the slope



Distance from public parking areas to buildings

Iconic. Would serve as another orientation point

Utilize the campus to bring everything into the building

I used to work at an office building near the current courthouse. It is a desert, with few services, far from downtown services, it is not conducive to integrated services. It

would require an enormous expansion of services/amenities to be functional

"Create a public space (exterior) park/plaza.

Open, inviting entry/courtyard/plaza"

"It would need a food court, there are no options in walking distance.

Taller buildings with views.

Adequate parking near the building

Food court - more options for food close by

More close parking

Dislocation and distraction of ongoing court and administration business

Not much, I dislike visiting courthouses, admin buildings for other county offices need good signage and close short time (max 1 hr) parking for public access

More parking



## **Q6: PLUM STREET**

### **Community Advisory Committee**

Sadly, the downtown area has a poor reputation with homeless & lawlessness. Major issue that keeps people from wanting to come downtown. Doesn't feel "walkable & safe"

Build out the full site immediately don't wait and do parking in phases

Plenty of street access to & from parking

### **Employees**

parking and uncluttered traffic

Adequate escalator and/or elevator system

Easy access, transit options, grouping of departments that make sense for independent communication

Connecting the new complex to waterfront and downtown (walking/building)

I am wondering about the traffic

Co-locating Olympia Municipal Court. Lots of people go to the wrong court, miss court dates.

Rooftop Japanese garden.

downtown-UGH!

If this was not good enough for the City, why would the County want it?!

I just believe that the limited acreage and increased traffic for the area if the courthouse was built here would be problematic and the worst option of the three sites.

I don't think this is a viable option. It would be expensive and downtown area does not need gaudy tall buildings.

### **Public**

"Don't mess with garden.

Make sure you have enough parking/LGBTQ/gender neutral bathroom options/jail options"

Meeting space

Parking, consumer services nearby (restaurants, shopping, office space)

Like the green space

Parking availability

The existing traffic congestion in the area & limitations for

alleviating it due to the constructions to the N&E would be tough to overcome. The park space being featured would be nice

"Clear, easy parking.

There are more places to eat, etc."

"Walkable, green area onsite. This is why I like Option 1 better. More green is best!"

Ease of access

Ample parking

"Nice open spaces. Keep or plant trees"

Tie it to the transportation hub

Easy parking

Has potential to create an urban campus but massing is very brutal

Location - easy access - convenient

"I think it is the superior site and could be very successfully integrated into the city, with superior amenities, transportation services and the most efficient approach.

Easy on-off freeway access - it best serves citizens"



Linking public space (adjacent gardens) to new plaza/entry

A physical separation of city + county services would lead to less confusion

Access

"Creating a good traffic pattern. Congestion is a real concern.

Security for civic center?"

Clean up the homeless issue

Parking accessibility

Compared to the present site, the Plum Street site could easily contribute and possibly least expensive

"Design site and buildings to be sustainable, energy efficient and built and designed using environmentally responsible ""green"" features and methods. This should include all facets of the new courthouse complex - buildings, parking, landscape and

maintenance. There needs to be easy access to the site.

Incorporate outdoor seating and amenities for public and staff."

Making sure there is adequate parking in the downtown location.

Build higher- or at least plan on adding another 5 floors in the future.

There would need to be improvements to expand capacity for the on-ramp to southbound I-5 / 101. It's already too crowded at 5pm and this would only add to it

This property needs to be redeveloped. Central location is ideal. Ease of access is best at this site

Access is a must. Getting in and out of parking. Getting from parking to destination

Closer to Lacey and rest of county. Better bus service. Build 7 stories up. Consider climate change, earthquakes!

Build high

Already backups on I-5 at this site. Adding more traffic at Plum Street will make it worse

A more spacious site with park nearby and view of Budd Bay

Easy access to neighboring businesses. As a juror, have dining options within walking distance and it's easy for us who work downtown

This would be the most accessible site for all the county population. Great corridor to and from I-5 as well as great accessibility from downtown. I could walk there from my office. Would need accessible parking.

Nothing - it would be hard to get to and you would have to compete with heavy existing downtown and residential traffic!

Meeting space

Parking, consumer services nearby (restaurants, shopping, office space)

#### Q6: Most Frequently Found Words

## A word cloud visualization of the text "parking downtown". The words "parking" and "downtown" are the largest and most prominent, rendered in a large, bold, black sans-serif font. Surrounding these two main words are numerous smaller words in various colors (red, grey, black) and orientations. The words include: "fee", "plenty", "area", "major", "full", "keeps", "homeless", "sadly", "poor", "people", "wait", "build", "immediately", "reputation", "come", "access", "street", and "phases". The words are scattered across the frame, with some appearing in the background and others in the foreground, creating a sense of depth and density. The overall composition is centered, with the two main words dominating the upper and lower portions of the image.

## **Citizens Advisory Committee**

Ease of access for local commerce on freeway.

Location compared to other services in my daily life.

Like how Harrison could incorporate a possible convention center and public events (concerts, etc.)

This site is not a good one - the courthouse, in my view, should be closer to the downtown area - if this site is chosen, a visitor's experience could be improved if the complex is very grand

Parking!

Easy freeway access or egress

## **Employees**

parking

more local businesses and restaurants in the area

covered parking

shops, eating, outdoor area

Please call this a Courthouse Complex. Corrupt Developer (Morris) should not be rewarded for previous

bad acts with Olympia's Planning Commission.

Figure out whether highway off ramp will be built. That should influence: (a) location of where front of complex is located and parking; (b) possible degree of additional improvements to Kaiser and Hanise. Given the current B space/acreage limits of current courthouse site in downtown Olympia site, this larger acreage makes the most sense. This also appears (with off ramp) a decent change/better access for jail transports to new site (vs. current courthouse)

I think structured parking is a better option, if a community concourse is built with it which would promote health businesses, YMCA and other local businesses it would promote growth in the area

## **Public**

Freeway access

Available meeting space (low cost). Highway access

Parking, transit, consumer services (restaurants, parking

professional office space)

Ease of access, mixed use

Too many unknown variables we cannot be certain of corporations and developers. In upcoming recessions and depressions taxpayers will revolt

I'm a resident nearby. Besides daytime accessibility to the park/open space, there's not much activity for me there beyond jury duty

Well maintained, accessible buildings and grounds - plenty of space

Business diversity. Good traffic management & easy access to 101. Public parks. Property set aside for future high tech air traffic

A different location

There would have to be a new on/off ramp at Kaiser Road or the traffic impacts would be too much for surrounding neighborhoods

This is far and away the most inaccessible site and the least desirable site in my opinion. This



would be a horrible location

Like public walking space/mixed use at this location

Great access for both freeway & streets

"Improved access via the proposed Hwy 101 on & off ramps.

Integration of really beautiful exterior spaces that could be very easily accessed by staff & public."

Good signage! I can't find my way around the current courthouse

Environmental consideration: leave old growth trees include open space

Accessibility, parking, traffic flow

"Strengthening public transportation.

Making this site a multipurpose civic site beyond what currently is planned."

There is plenty of room to create a good campus but I'm not sure placing this iconic building type in a residential area is best

Keep courthouse in Downtown Olympia - it just makes practical sense + perception about unified vision for our region

A connection to the local walking trails

"Love all the green spaces. Plenty of room for future expansions."

"Good parking availability.

food services within walking distance."

Too far out of town

"Design site and buildings to be sustainable, energy efficient and built and designed using environmentally responsible ""green"" features and methods. This should include all facets of the new courthouse complex - buildings, parking, landscape and maintenance. There needs to be easy access to the site.

Incorporate outdoor seating and amenities for public and staff. "

Nothing - I can't make sense of this location for

a courthouse; it is convenient for few and would be in a largely residential area. This area is already a congested mess.

More innovative building design - apply above statements.

Increased and closer parking compared to the current Courthouse location. Express bus to downtown Olympia and state offices in Tumwater.

Easy access

Incompatible with existing housing, medical, and commercial use in the area

Lots of clear signage to direct me directly to the dept. I'm looking for Freeway access

Available meeting space (low cost). Highway access

Parking, transit, consumer services (restaurants, parking professional office space)

#### **Q6: Most Frequently Found Words**

[illegible]

## A word cloud visualization of the text 'freeway access' and related terms. The words are arranged in a circular pattern around the central text. The words include: freeway, access, local, experience, guests, parking, concerts, location, incorporate, possible, commerce, view, visitors, ease, public, service, compared, like, daily, convention, life, good, downtown, center, are, improved, progress, grant, closer, complex, chosen, courthouse, easy, Harrison, and location. The words are in various colors and sizes, with 'freeway' and 'access' being the largest and most prominent.

[illegible]

9

## **Q7: HILLTOP CAMPUS**

### **Citizens Advisory Committee**

"All options are great!  
There is no bad option.

The Hilltop site might be nice to have the top floor of the Civic Center have a room for viewing"

site preparedness for emergencies. How accessible is it during an emergency and can it house citizens

I think it is easier to sell to Thurston County voters

### **Employees**

the location is not as convenient as the other locations

Maybe use of existing properties is a good idea. Keep congestion out of downtown.

The location is not as good as Plum St. as far as surrounding amenities, but Hilltop is a lot more scenic and will stand alone as an architectural statement. Plum St. location will be just another tall building.

Terrible use of valuable real estate. Out of the way for customers. Poor

development of neighborhood despite use for 40 years-will not improve by rebuilding.

Please call this a Courthouse Complex. "Civic Center" implies another facility entirely. The taxpayers have already invested in this site.

This is my number two site (Plum is number 1, Harrison way below)

Separate restrooms for public (includes out-of-custody defendants) vs. prosecutor's office personnel & court staff.

### **Public**

Access issues - walkability

Not good. No economic development with current courthouse

"Will it really meet long term needs of the community? I think civic pride will result if complex is responsibly designed and clearly meets needs of county and community. It will be determined in large part by project design, management and budget, not so much where it's located. "

Location is easy to get to and centralized - it's

worked for a long time, so might be best to keep it. The views are nice from a civic standpoint.

Don't particularly like the building designs - I like the modern look but for the price they are a bit bland and already done. Be innovative and make a statement. Blend some traditional sandstone found down town with modern glass atrium space.

Better if all is kept in one area

I am not a fan of this site. There's nothing up there; nothing (business-wise) has been built since the courthouse was built...no place to eat, etc.

Not this option

"I like this site because it reserves the views for the general public.

I would like to see more trails and park space here with county buildings."

"Seismic issues with steep slope a concern.

Does not promote alternative transportation.



Does not help density issues.

Difficult site to police because of extent of wooded area."

Phasing will drive this scenario but if done correct it will create civic pride

"Keep the courthouse in the urban core - Plum St. is a much better option and is consistent with infill goals, reduced carbon footprint, and city-county collaboration.

Let's re-purpose the old City Hall and provide a high-level of service to citizens in the county.

Plum St. site is by far the best choice."

How's the liquefaction risk here?

Seems that it would be more expensive to build on a "working" site. Cost of moving services, etc.

I like the idea of the county using the property they already own due to cost savings and a familiar location. But I'm concerned about it taking so long because of added costs

and worker and services displacement

I am not in favor of the westside/Harrison Ave. option

Access issues - walkability

Not good. No economic development with current courthouse

Feels crowded

Good potential w/ the underground access. Phasing would be a potential issue

Make the top floor observation/conference space so everyone can see the views

++curved building  
++open space  
++lower buildings/parking

Beautiful views

Rectangular buildings are jarring for this site

Love the income possibilities if designed to host public events and conventions

Parking here with offices above <-----

#1 most integrated with surroundings, no phases!

Don't like this

competing w/ capital building / city skyline

Keep the current jail space

Curve is expensive

#3 least desirable of the hilltop options, main structure overwhelms site

Traffic! Traffic! Traffic! How can 2 lane roads handle it?

Nostalgic but not the best

For hilltop I like this one #3!

Best site for all Thurston county

Competing with capital

Regardless of site the building should be signature, beautiful structure that reflects residents can be proud of and can obviously be a civic building for decades, unlike city hall.

Hilltop is best site #5

Cannot work here during construction!

Need to account for non-economic impact to court + other county operations while construction nearby is underway

#### Q7: Most Frequently Found Words

## A word cloud of terms related to the Hilltop Center emergency plan. The words are arranged in a circular pattern, with some words appearing in red and others in black. The words include: Thurston, emergency, easier, citizens, Civic, room, preparedness, emergencies, voters, viewing, Center, mighthouse, great, bad, floor, think, option, nice, can, accessible, top, sell, County, Hilltop, and options.

Plum  
St  
location  
will use

good  
number  
another  
taxpayers  
complex  
may  
map  
architectural  
staff  
congestion  
down  
poor  
house  
separate  
convenient  
idea  
alone  
invested  
improve  
building  
Center  
invested  
improve  
building  
Center  
invested

## **Citizens Advisory Committee**

Will this site allow for a desirable convention center, and the ease of access for citizens during an emergency?

## **Employees**

Having the new facility as close to downtown as possible is a good idea.

I think this is a terrible option. It would lead to terrible traffic in downtown Olympia and add to already terrible traffic in I-5 corridor! Let's not create "Seattle style" commutes to and from. Olympia is lucky enough now to have very little traffic. Let's keep it that way.

The best option, by far.

Much more visible location for civic pride! There may be traffic mitigation on this site.

Sea level rise must be considered. Already inadequate parking in the area. Problems with downtown issues related to homelessness, public perception.

Siting the new facility downtown will help bring new people downtown and boost economy of small

businesses. Many people come from all over western WA to use courthouse. Let's site it to showcase our lovely town.

Separate bathrooms for public vs. prosecutor's office personnel and court staff.

## **Public**

I think this is the obvious option based on accessibility alone. Public who interact with the courthouse for varied reasons typically have other business to take care of in downtown. If they don't have access to convenient transportation, location near other services, etc. is crucial

Seems to be the best option all nearly all perspectives

No brainer - this site is best

So glad Thomas Arch. is behind this project! Will be class act.

I think this is the obvious option based on accessibility alone. Public who interact with the courthouse for varied reasons typically have other business to

take care of in downtown. If they don't have access to convenient transportation, location near other services, etc. is crucial

Seems to be the best option all nearly all perspectives

No brainer - this site is best

Easy access from freeway and/or city streets

Where would the municipal jail be location & how would that affect long term partnerships & costs?

"This is an awesome site. It would really add to downtown, promote economic development, etc. It's very accessible - need to consider accessibility!

It's in the heart of Olympia."

Love the boulders & use of water onsite (option 1)

"Concerned about the existing jail facility? Would another facility be built?

What about the homeless ""tiny house"" project to be sited



behind Japanese Gardens?"

"Worry about traffic congestions getting off I-5 - also I-5 congestions at exits.

Safety of building - vandalism already at downtown buildings including Municipal Court."

Environmental consideration: worried about sea level rise at this site

"Tsunami Ave./water issue.

Suitability for resiliency"

Just build up for courthouse + garage

Thought will need to be considered about secure parking

"I've examined all the materials on pro/con of each site; this site is by far the best - more amenities, bus lines, easy access to site and freeways, consistent with city/county goal to encourage urban infill, and the best option to serve citizens' needs.

Also - smallest carbon footprint due to least driving distance for most people - it would be very unfortunate if the

Harrison site was selected - think of the thousands of additional miles travelled to get to this (Harrison) site - we need to reduce, not expand, our carbon footprint.

There is a perception that county + city government are adversaries - politically. It would be a shame for their differences obstructed selection of the Plum St. site - please put aside differences, collaborate and demonstrate that an urban site - Plum St. - represents a unified vision that is pragmatic and best serves the region's needs.

Practical costs can be contained by re-developing the Plum St. site, it has superior utility, transportation, amenities and local interest in further development of services to support the courthouse staff and citizens. We have a great YMCA 2 blocks from this site - why not leverage these great services and other advantages that already exist on or near the Plum St. site.

Thanks for soliciting public input - much appreciated."

This site seems more appropriate than others. Good location, easy access, near food & shopping

"Is this site vulnerable to sea level rise?"

How's the liquefaction risk here?"

"Will city contribute to construction cost or long-term lease?"

Concern about homeless situation & location of proposed use of area near garden"

"Already have confusion between city + county concerning the public. Comingling courts and other offices will compound this problem.

What about sea level rise in 30 years?"

I'm concerned about increased traffic at this site

The current courthouse site should be the only option for the new court, such a great site and rare piece of property

"Can employees and the public easily access

the site during peak traffic periods?

I think civic pride will result if complex is responsibly designed and clearly meets needs of county and community. It will be determined in large part by project design, management and budget, not so much where it's located.

Overall comment - just call this a something like ""The new Thurston County Courthouse."" The project site will only house services and activities associated with a typical courthouse, so the name seems appropriate. "

The site seems best equipped for access throughout the county. Post office is close, as well as the library and eateries. A boost for downtown, but may add to the parking problems.

Same as above design is bland - make it nicer-resemble surrounding topography of mtns and water.

The site is very wet

Tall buildings seem too big for this site. Is there

sufficient room for professional offices to locate nearby?

site can handle traffic!

This property needs to be redeveloped. Central location is ideal. Ease of access is best at this site. Mixed use if neighborhood. The gardens being incorporated

Would think most of the infrastructure is available. Which site would survive earthquake best? Having worked for a county and in a county facility is the importance of the jail and courtrooms being close to each other. Courts drive the county

This site is most centrally located in the county for access from all areas. Most of growth in the county is on the westside - Lacey / Hawk's Prairie

needed clip board

site is too small, too much additional traffic for this area

Too small of site. Too much traffic for this area to handle

Many elderly friends will not go downtown

because of the homeless problem

I am not in favor of this site. It should only have county courts. The city courts need to be separate

The Plum site makes good use of wasted space. The Yashiro Garden would be improved.

"I think it would be fantastic to have a new courthouse downtown adjacent to the Yashiro Garden. I discovered a great hotel in Lebanon OR. The Boulder Falls Inn Best Western Premier. It is owned and operated by Samaritan Health and adjoins beautiful Japanese garden designed by Hoichi Kurasu. The garden was funded by the Health System's Foundation. In nice weather, Yashiro Garden would be a great alternate space for attorneys to meet with clients. When we opened the Family Court at 28th and RW Johnson 20 years ago it was noticed immediately that cases settled more often and there was less animosity between parties with the abundance of

space, meeting rooms, art on the walls, etc. than at the main campus. Having a courthouse adjacent to a beautiful garden is an opportunity that should not be missed.

It would absolutely [promote economic development in the surrounding neighborhood]. See the attached list of attorneys and their location and their offices from the 1939 Olympia telephone book. The courthouse was then located at 11th and Capitol Way. Before that the old capitol building and before that where the State Theatre is now."

That location will simply be a magnet for homeless use after hours

I don't like the idea of huge buildings in this location

This is a very tall scale

Scale and shapes fit the feel of the site

Confusion between city vs county courts if in same building

Please leave the garden (plus 3 'in agreement' comments

What would happen with the current municipal jail given no contract with the arc

Downtown has a less desirable feel for many citizens. Homelessness & lawlessness reputation of downtown does not promote civic pride.

How do you convince people to go downtown?

Traffic flow concerns-favorite locale.

The on-ramp to sb i-5/101 is already very crowded at 5pm. Would need to increase capacity on that.

This site makes sense in terms of central location in community and access from i-5. Also build on current government functions in the area/downtown.

Best site to compliment current government function.

I oppose mixing city and county services at one site.

Incorporating the gardens would be highly desirable.

I like this location! The property needs to be re-developed. Either of

these options would make for a big improvement.

Build downtown centered community.

Too tall

Owned by city and downtown centered.

Best site

No giant buildings on this site please

Elderly people do not go downtown because of the homeless situation.

I work at this site, where would the employees go while building on this site?

This site clearly the best option. A no brainer.

No brainer plum site this site

By far the best for court users. Best access.

Response to comment "how do you convince people to go downtown?" I don't see this as a relevant issue. People are often required to visit county files/courts, so they don't need "convincing"





## **Q7: HARRISON WEST**

### **Citizens Advisory Committee**

Concerns around how accessible the building would be in a natural disaster and be able to host citizens in emergencies

It seems so far away

### **Employees**

Terrible option for access by people of limited means. Extremely hard to get to by bus or walking.

Disparate impact on surrounding residential area. Traffic issues. Expense of infrastructure-no guarantee of new onramp. County should not own commercial frontage on Harrison Ave. Further for south county residents.

I hate the idea of a new construction instead of infill, and county courthouse too far from city core.

(a) Restroom: certainly more than we have here! (b) separate bathrooms for prosecutors/staff (and defense attorneys?) and courthouse staff from the public. The potential

dangers of prosecutors and defendants (and prosecution witnesses) in the same bathroom has been recognized elsewhere.

Separate/locking bathrooms are already built at FJC.

### **Public**

Best solution

Bad idea

This site makes the least sense for a county courthouse/admin. It is located on the far west side of the city far from the current and proposed growth in the county on the east side - Lacey, Hawk's Prairie. If a new ramp were not built off 101 when it would open Harrison Avenue would be way over capacity and have low levels of service. It is currently largely surrounded by residential (low density) development, not a good fit for a large county facility.

Blank slate, seems to be most flexible option.

Public/private opportunities look really appealing. Don't do all surface parking. Bite that bullet and build a structure. Great freeway

access and even better when interchange arrives. Great options for community space

Mixed is highly desirable

Cost is most important, ability to address future needs is second

The off ramp from 101 HAS to be built right away for access

Option 1 is preferred. Ramps to/from Highway 101 must be expedited

Traffic is always the least thought out. We can't build a project like this and hope DOT catches up within 10 years...that's much too late. You need to get the traffic on/off 101 directly to Kaiser, otherwise it's too messy. And it needs to be finished (on/off ramps) prior to the completion of the building

Huge issue: traffic impacts will be huge. On/off ramps to/from Kaiser need to be built as part of this project, along with improvements on Harrison to the west through the Evergreen Parkway. Traffic in last 5 years with increased housing is already

causing significant problems.

I am a resident in the subdivision across the street and I believe the traffic situation would become a burden for property owners who moved there for ease of convenience to amenities. Additionally, the increased traffic / cars / people would create safety issues for many of the young children in our neighborhood. I believe Plum Street would be better.

Traffic is always a problem, more and more frequent bus routes directly to & from transit center needed. Don't stop at the mall.

It (future high-tech air traffic) sounds far off, but reality may come sooner than imaginable

I am a resident and oppose this site because it would increase traffic, congestion and pose a safety risk for the neighborhoods nearby

Who knows [if this site would promote economic development in the surrounding neighborhood]?  
Surrounding

neighborhood is residential. Is that what the neighborhood wants?

"Out of way, far for most of county. Too much surface parking."

Wide open space to design a beautiful complex

While the site presents a "workable option" the civic purpose of a courthouse is not accommodated by a site outside of the downtown area. The very nature of a courthouse and what it represents is best expressed in the city center or close to it

"So. Much. Parking!!! The surface parking option, though less expensive, really takes away from the feel. It feels like a giant strip mall. It does fit in with the surrounding area, but not in a positive way.

I'm also concerned about the reliance on the yet to be built on/off ramp."

LEED Certified construction please

"Best site from a resiliency standpoint.

A balance between urban & suburban."

If this is really an option, more thought is needed

Too much surface parking

"Downtown site has superior public transportation, amenities, ease of access, and a much smaller carbon footprint.

Let's demonstrate City-County collaboration, not separation via geographical location."

"This site does not seem appropriate for adjacent housing/neighborhoods.

The ""food"" options are a stretch...a food truck & a bar."

How's the liquefaction (risk) here?

"Transit concern. Would county move to pay for the new off-ramp. Best potential for the future. This choice is a loss of property tax. I think trying to put a civic center in the same location is too much - put the civic center on the Hilltop."

"Really like the opportunity for mixed



space. Doesn't feel boxed in."

It's away from the mess in downtown Olympia

"I think civic pride will result if complex is responsibly designed and clearly meets needs of county and community. It will be determined in large part by project design, management and budget, not so much where it's located.

Overall comment - just call this a something like ""The new Thurston County Courthouse."" The project site will only house services and activities associated with a typical courthouse, so the name seems appropriate. "

It's big but Tumwater would be a better location than there...

This is the ideal location for a new county complex, close to the freeway, away from downtown traffic limitations and I-5 congestion issues. Also will allow for support for smaller business which are beginning to develop on the West side of Thurston County

Best option of the three

"Needs to be easily accessible for traffic from the outlying courtyards, lots of parking.

None of the three sites look good to me.

Too hard to access from an already overloaded freeway."

This location is too far on the west side for convenient access.

Public would have to fight the hwy 5-hwy 101 interchange to get there from east and south county

This is my favorite site. Option 2 with the parking structure is a better use of land

Good for future helio/vtol pad-good arrival-departure routes.

Too much surface parking

Like the mall between public + private feels collaborative

Freeway access needs to be added at the same time

I really like the town square feel

Like the town square, would like to see it

further developed with an amphitheater for public events

Retail space could be used for expansion of county facilities in distant future.

Best site

Stormwater should be best practices to date, not limited to 2007 vesting

Allison springs municipal water supply close

Need better auto infrastructure at time of opening

Not positive for adjacent housing & neighborhoods

This site will create too much traffic.

Structured parking way better than surface parking

Include a west Olympia timberland regional library (w/o homeless)

Plan for 50-100 years

Traffic may be significant issue but there is space available to address this.

Security @ night?

Not centrally located in the county. Makes no sense given current

location of government/civic functions in downtown. Not compatible with the low density single family development.

You've got more room - spread out buildings so they don't tower over surroundings.

Too much traffic, crowding, earthquakes, + high taxes! Good for developers!

Mixed use is highly attractive, add to all options.

Need separation of

bedroom community to new structure

Possibility of a community concourse which would allow a good balance of government and private business





# VIII.

THURSTON COUNTY COURTHOUSE FINAL REPORT

## CONSULTANT REPORTS

DRAFT

# CONSULTANT REPORTS: CIVIL



## TECHNICAL MEMORANDUM

**TO:** Amos Callender, Project Manager  
Thomas Architecture Studios

**FROM:** Amy M. Head, PE

**DATE:** November 30, 2018

**PROJECT #:** 1835.11

**SUBJECT:** Thurston County Courthouse and Civic Center Site Study

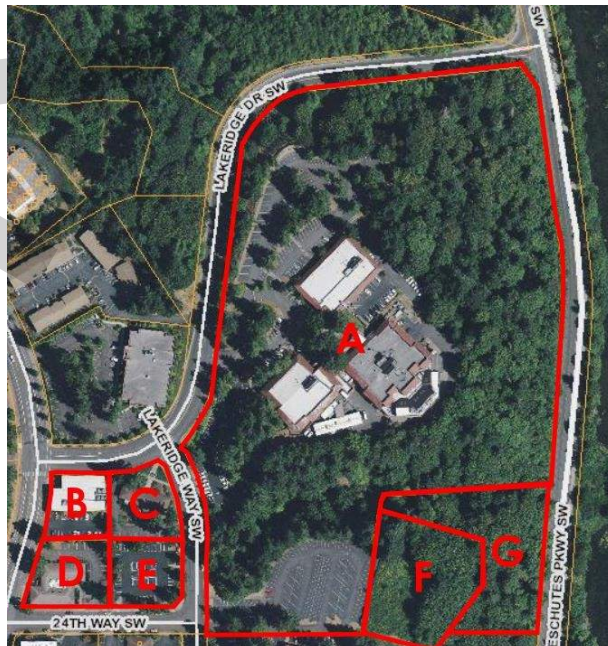
**ATTACHMENTS:** Thurston County GeoData Contour Maps, SCS Soil Survey Maps, City of Olympia Utility Maps, Developer Agreement Harrison Ave. Parcel

### PURPOSE

The purpose of this memorandum is to discuss three potential sites for the future development of the Thurston County Courthouse and Civic Center. The sites include the potential redevelopment of the existing courthouse site on Lakeridge Drive including an adjacent site across Lakeridge Way (Hilltop Campus Site), the existing City of Olympia courthouse site and surrounding parcels on Plum Street (Plum Street Site) and a site on Harrison Avenue on the west side of Olympia (Harrison West Site). The following is a general summary of opportunities and constraints of each site area based on local knowledge and experience, some minor research and a meeting with the city. Specific and detailed research of each site area was not performed.

### HILLTOP CAMPUS SITE

The hilltop campus site includes the existing courthouse site (parcel A) located on Lakeridge Drive and overlooks Capital Lake. This site is characterized by a steep slope bordering the north, south and east site limits. This site also includes parcels B, D and E located west of parcel A. Sites C and G are not currently part of the project.



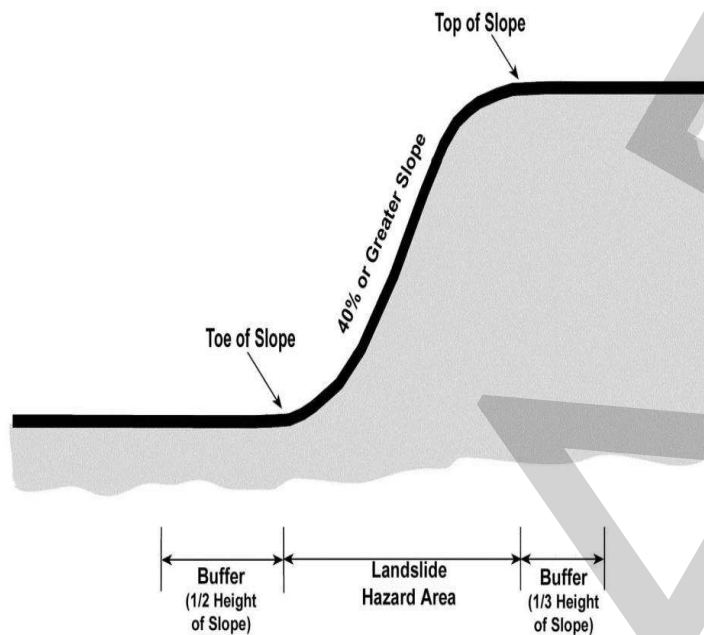




## Soils and Topography

### Soils

According to the SCS soils survey (soils data provided by the Natural Resource Conservation Service), site soils consist largely of Alderwood gravelly sandy loam and Everett very gravelly sandy loam. There also are small areas of Dystric Xerochrepts and Cagey Loamy Sand. All these soils are moderately to well-drained but are vulnerable to sliding when saturated. Any redevelopment of the site would need to focus on preventing subsurface flows toward the slope area. The existing drainage systems would need testing to determine if pipes are water tight and all drainage is captured and conveyed away from the slope.



### Topography

A topographic survey was not prepared. Topographic information was determined using Thurston County GeoData mapping.

According to county mapping, Parcels A, F and G are located at the top of a steep slope area. The height of the slope ranges but is as much as 136 feet in height. Parcel G appears to be located completely within the slope area and is likely not useable.

The steep slope area is also considered a landslide hazard critical area and would be subject to the requirements of Olympia Municipal Code Section 18.32. The required buffer width is the greater amount of either:

- the minimum distance recommended by the engineering geologist or geotechnical engineer
- at the top of the landslide hazard area, a distance one-third the height of the slope
- at that bottom of the landslide hazard, one-half the height of the slope
- 50 feet in all directions from a seep (a portion of the slope that has groundwater flow through the face of the slope).

For this site, the buffer required from the top of the slope is approximately 45 feet (based on 136-foot slope height) and could be greater should a geotechnical analysis require additional distance. Generally, maintenance and repair of existing facilities is exempted from buffering requirements. Remodeling or replacement of existing structures may also be exempted but is subject to evaluation by the city. Any new construction would be subject to buffering. Buffer reductions up to 50% can be requested on a case by case basis.



According to Thurston County mapping, slopes within the constructed areas of parcel A are generally gentle with some steeper slopes between buildings 1 and 3. Site elevations in parking lots A and B (north side of building 1) generally range from elevations 158 to 146 feet. Elevations in parking lots I, F, and E (adjacent to building 3) generally range from elevations 138 to 128 feet. Overall site elevations range from elevation 128 to 158 feet.

Parcel F and the adjacent parking lot on parcel A have generally gentle slopes with elevations ranging from approximately 130 to 156 feet. This area is separated from the remainder of parcel A by a steep ravine.

Parcels B-E are in a relatively flat area with site elevations ranging from 158 to 162 feet and do not have the same landslide hazard area concerns of parcels A, F and G.

The site including all parcels (A-G) are well above current sea level and are not at risk for future flooding should the seal levels rise.

## Parking

Parking at parcel A is insufficient for current demand and the potential for the addition of spaces is limited. Expansion of existing lots would be limited by the adjacent steep slopes; therefore, reconfiguration of lots A, B and C would largely consist of removing vegetated spaces to allow for restriping. Removal of green space would have to be balanced with the City's landscaping and stormwater requirements. Should any building footprint removal be proposed as part of the redevelopment, this would be an additional place where potential parking could be added.

Parking is also available offsite including additional lots and on-street parking, but these areas are also filled. Lot G has some undeveloped adjacent space, but this space is subject to slope buffering and is largely unusable.

Given the site constraints, maximizing parking would likely require the construction of structured parking.

## Access

Parcel A has three points of access, one on Lakeridge Drive and two on Lakeridge Way. Access to the site is limited by the steep slope areas so additional access points would not be feasible. Site circulation within the site is challenging as it includes both one-way and two-way circulation. When parking is limited at the site, cars are often forced to make multiple loops through the entire parking lot. The parking lot to the south is also separated from the main parking area by the large ravine.

Parcels F and G are accessed from parcel A. Currently there are multiple points of access to parcels B-D from the adjacent roads. Depending on the site layout, these existing access points can be used, or new access proposed. New access points will need to consider site distance from adjacent intersections.



The site is in an area of well-developed streets with sidewalks. It is not known if any modifications to the frontages would be required. It is understood that closure of a portion of the portion of Lakeridge Way that separates parcel A from parcels B-E is being considered. In discussion with the city they indicated that this might not be allowed due to the block spacing requirements of the city code.

## **Transit**

The hilltop campus site is served by Intercity Transit.

## **ADA Accessibility**

Site upgrades will require compliance with ADA and connectivity between structures will be required. This may be challenging between some structures due to site elevation changes on some parcels. Given the flat topography of parcels B-E, ADA accessibility would be less challenging for those parcels.

## **Utilities**

### Water and Sewer

The site including all parcels are already served with water and sewer utilities per the attached mapping provided by the city.

Water lines are located within adjacent streets including Lakeridge Drive, Lakeridge Way and Evergreen Park Drive as well as along the south side of parcels A and F. Water is also looped along the existing courthouse site on parcel A.

Sewer lines are located within the adjacent streets including Lakeridge Drive, Lakeridge Way and Evergreen Park Drive. A sewer line is stubbed to the existing courthouse site on parcel A.

The current capacity, size and status of existing water and sewer systems is not known. Adequacy of fire hydrant coverage and overall fire flow availability would need to be assessed as part of any redevelopment.

### Gas/Power/Communication

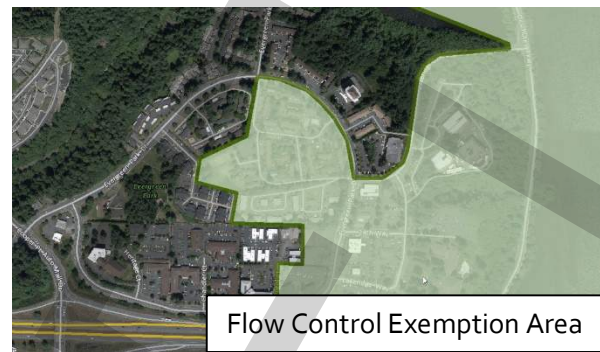
Mapping of franchise utilities were not obtained. As this area is currently developed, it assumed that existing systems are available for use. The capacity of these existing facilities is not known. Adequacy of these facilities would need to be assessed as part of any redevelopment.





## Stormwater

Based on mapping provided by the City of Olympia, the site is within a flow control exemption area. Flow control is the requirement to manage the quantity of stormwater water flow that exits a site and is typically achieved with the construction of stormwater detention or retention pond. Therefore, for purposes of stormwater management, the focus will be water quality treatment.



Based on City of Olympia utility mapping, drainage from Parcels A and F is collected in a catch basin and pipe system and is then directed to the ravine area that runs between Parcels A and F where it ultimately flows into Capitol Lake. Based on the age of this system, it is not likely that any water quality treatment is provided.

City stormwater mapping does not show any on-site piping for Parcels B-E with the exception of a small amount of pipe on Parcel E. The buildings on parcels B-D were built in the 1970's. Given this age, it is likely that stormwater systems consist of catch basin and pipe that connects to the drainage pipe in the neighboring streets. Parcel E had a paving project in the late 1990's so some type of treatment system is likely in place for this site.

Retrofitting the existing systems with water quality facilities would be required for any redevelopment project. Water quality treatment requires smaller facilities than flow control systems. In addition, the City of Olympia recently updated their stormwater manual to emphasize the use of low impact design (LID) strategies for stormwater management. LID is a practice of mimicking natural processes including infiltration, evaporation and transpiration in the management of stormwater. LID systems include facilities such as rain gardens, bioretention swales, pervious pavement and similar facilities. Parcel A already employs some LID strategies such as the large landscape areas within the parking areas and the presence of mature vegetation. Site planning should account for LID requirements.

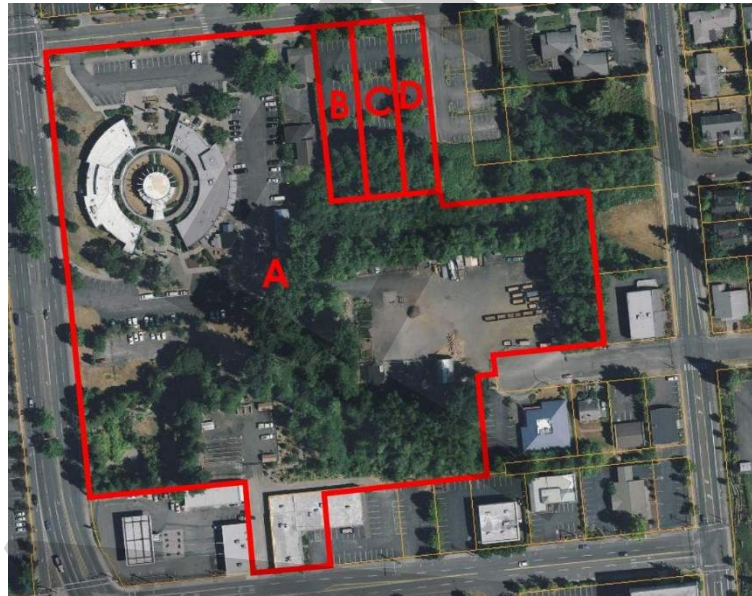
## Summary

To summarize, the existing courthouse site will likely have limitations due to its proximity to a steep slope/landslide hazard area. The site is within a stormwater flow exemption area; therefore, stormwater management needs will be limited to providing water quality treatment. Utilities are available although upgrades could be needed to meet current fire flow and coverage requirements. The site has good access to adjacent streets but circulation through Parcel A is poor. Circulation between existing buildings will be challenging to meet current ADA requirements due to slope constraints. Insufficient parking is currently available and surface parking lot expansion options are limited. Structured parking would likely be needed for the site.



## PLUM STREET SITE

The Plum Street site includes the existing City of Olympia courthouse site (parcel A) located on Plum Street between Union Avenue and 8<sup>th</sup> Avenue. Potential sites for future consideration are three small parcels (B-D) on 8<sup>th</sup> Avenue.



## Soils and Topography

### Soils

According to the SCS soils survey (soils data provided by the Natural Resource Conservation Service), site soils consist largely of Xerothents and Yelm Fine Sandy Loam. Xerothents is well drained and Yelm Fine Sandy Loam is moderately well drained.

Based on the City's pre-submission conference notes for another project proposed on the site and recent experience on an adjacent site, the site will likely have high groundwater.

### Topography

A topographic survey was not prepared. Topographic information was determined using Thurston County GeoData mapping. Elevations vary from a low of approximately 12 feet to a high of about 38 feet. Slopes on developed portions of the site are generally flat. The site generally slopes from east to west, Eastside Street to Plum Street. There is a generally low area with steeper slopes that bisects the site. It appears this could be a drainage ditch or stream. County mapping does not indicate this area is a critical area such as a stream, wetland, or similar feature. However, in discussion with the city they have identified this area as a wetland area which will require buffering. They indicated that areas of existing development would likely define the buffer limits but additional encroachment into the area would not be allowed.

A portion of the site is below elevation 16 but these areas are within the wetland and therefore, the sea level rise requirements of the City of Olympia would probably not apply for any structures. Utilities may require special design to eliminate the potential for infiltration of flood waters.

## Parking

Parking is currently provided via surface parking lots on all parcels. There is also a large paved area on Parcel A that could also be used for parking. Based on programmatic needs of the facility, the currently available parking would not likely serve parking demand. Sufficient space is not available to meet parking demand on-site with surface parking. Therefore, a parking structure would be needed.



## Access

Parcel A currently has 3 points of access including one on 8<sup>th</sup> Avenue, one on Plum Street and one on 10<sup>th</sup> Avenue. Circulation is poor on Parcel A as the parking adjacent to the existing courthouse is separated from the other available parking area by the wetland. A bridge connection is currently available, but it is unclear if this would be sufficient for planned improvements.

Parcels B-D have no separate access from adjacent streets. They are currently accessed from an adjacent parcel.

The site is in an area of well-developed streets with sidewalks. It is not known if any modifications to the frontages would be required. In discussion with the city they indicated that this might not be allowed due to the block spacing requirements of the city code.

## Transit

The Plum Street site is served by Intercity Transit.

## ADA Accessibility

Generally, site slopes are fairly flat, so ADA accessibility should be readily achieved.

## Utilities

### Water and Sewer

The site including all parcels are already served with water and sewer utilities per the attached mapping provided by the city.

Water lines are located within adjacent streets including Plum Street, 8<sup>th</sup> Avenue, Eastside Street and Union Avenue. Water is stubbed in several locations to Parcel A. Redevelopment of the site will likely require on-site water looping.

Sewer lines are located within the adjacent streets including 8<sup>th</sup> Avenue and Plum Street. A sewer line is stubbed to Parcel A from Plum Street.

The current capacity, size and status of existing water and sewer systems is not known. Adequacy of fire hydrant coverage and overall fire flow availability would need to be assessed as part of any redevelopment.

### Gas/Power/Communication

Mapping of franchise utilities were not obtained. As this area is currently developed, it assumed that existing systems are available for use. The capacity of these existing facilities is not known. Adequacy of these facilities would need to be assessed as part of any redevelopment.





## Stormwater

The site is also part of the downtown Olympia flow control exemption area. Therefore, for purposes of stormwater management, the focus will be water quality treatment. However, if connection to the wetland is proposed, then the city will likely require that current flows to the wetland area not be exceeded. Therefore, some amount of flow control may be necessary if proposed flows from the project exceed existing flows to the wetland.

Based on City of Olympia utility mapping, drainage from all parcels are collected in a catch basin pipe system and then either directed to the wetland area that bisects the site or do the drainage system in the adjacent street. It appears that part of Parcel A drains to a combined storm and sewer system. Often the city will require that combined systems be separated as part of redevelopment projects. It is anticipated that connection to the combined system would not be allowed with this project. Given this, it is even more likely that some type of on-site flow control system may be required as all flows would now have to connect to the wetland when currently only part of the site flows to the wetland. Given the high groundwater that is likely present and space considerations, the likely system necessary would be an underground detention vault.

Retrofitting the existing systems with water quality facilities would be required for any redevelopment project. Water quality treatment requires smaller facilities than flow control systems. As discussed with the Hilltop Site, LID systems would likely be required by the city. Site planning should account for LID requirements

## Summary

To summarize, the Plum Street site will likely have limitations due to the wetland area that bisects the site. This wetland limits the developable area and will potentially increase the stormwater requirements for the site despite it being within a flow control exempt area. Utilities are available although upgrades could be needed including the potential to remove storm connections to combined sewer/storm systems. The site has good access to adjacent streets but circulation between areas of Parcel A is poor due to the wetland. Structured parking would likely be needed for the site.



## HARRISON WEST SITE

The Harrison West site includes four largely undeveloped parcels that extend from Harrison Avenue to 7<sup>th</sup> Avenue on the west side of Olympia. This site has a currently active Developer Agreement with the City of Olympia that vests development of this project to the code in place at the time of the inception of the agreement (2007). This agreement expires in 2021. The Developer Agreement was established based on a mixed used development. It was not investigated whether there would be limitations or modifications required for a courthouse development.

### Soils and Topography

#### Soils

According to the SCS soils survey (soils data provided by the Natural Resource Conservation Service), site soils consist largely of Alderwood gravelly sandy loam which is moderately well drained.

#### Topography

A topographic survey was not prepared. Topographic information was determined using Thurston County GeoData mapping.

Elevations vary from a low of approximately 166 feet to a high of about 184 feet. Slopes on most of the site are generally flat with a steeper area on the east side of parcel A. The site generally slopes from south to north towards Harrison Ave. on parcels A and B. Parcels C and D slope to the south towards 7<sup>th</sup> Avenue and are part of a closed drainage basin.

### Parking

The Harrison West Site has the most developable area of the three sites under consideration. Given this, it has the most potential to have parking to be provided totally by surface parking.

### Access

The site currently has an access from Harrison Avenue. Based on the Development Agreement currently in place for the project (signed June 2017) and per the city's block spacing requirements, road extensions will be required through the site for any development. The currently planned road extensions per the developer agreement required 1 north/south road through the site and 1 east/west road extension (site plan shown). Any site layout would need to incorporate road connections to provide for site circulation and to meet the city's block spacing requirements. If the development





occurs during the vesting period of the Developer Agreement, the approved roads have a narrower width than standard city roads.

In the future, access could be improved with a new interchange. The City of Olympia is planning a new half diamond interchange on the east side of Kaiser Road, with the westbound off-ramp to the north of US 101 and the eastbound on-ramp to the south of the highway. The westbound off-ramp would be a one-lane roadway as it exits from US 101, then widen to two lanes near the intersection with Kaiser Road. The eastbound on-ramp would be one lane.

Additional improvements associated with the interchange would include improvements along Kaiser Road from 7<sup>th</sup> Avenue SW to about 500 feet south of the bridge over US-101. Kaiser Road would be widened primarily on the west side to remain within the City of Olympia's right-of-way. Kaiser Road would have two through lanes (one in each direction) with new left-turn pockets at the new eastbound on-ramp to US 101 and to 7th Avenue SW. The 5-foot-wide bicycle lanes would traverse the length of the Kaiser Road improvements. There would be 10-foot-wide sidewalks with tree planters north of the bridge, and 6-foot-wide sidewalks with 8-foot-wide planter strips south of the bridge. In addition, the west side of the existing Kaiser Road bridge would be widened by approximately 23 feet to provide for new 5-foot-wide bicycle lanes and 6-foot-wide sidewalks on the bridge.



This project has environmental approval and has been through the interchange justification report (IJR) process. The project does not currently have construction funding and is not likely to be constructed for 8-10 years.

This planned interchange would increase accessibility to the site and improve mobility and transit.

## Transit

The Harrison site is just outside the Intercity Transit service area. The closest transit stop is further east on 7<sup>th</sup> Avenue by Capital Medical Center. However, in discussions with the city it was learned that Intercity Transit currently has requested to add stops along Kaiser Road and Harrison Avenue directly adjacent to the site.





## ADA Accessibility

Site slopes should allow for easy grading to accommodate the slope requirements of ADA accessibility.

## Utilities

### Water and Sewer

The site including all parcels are already served with water and sewer utilities per the attached mapping provided by the city. Water lines are located within adjacent streets including Harrison Avenue, Kaiser Road and 7<sup>th</sup> Avenue. Water is extended to Parcel A. Development of the site will likely require on-site water looping. Sewer lines are located within the adjacent streets including Harrison Avenue, Kaiser Road and 7<sup>th</sup> Avenue. A sewer line is extended to Parcel A from Harrison Avenue.

The current capacity, size and status of existing water and sewer systems is not known. Adequacy of fire hydrant coverage and overall fire flow availability would need to be assessed as part of any redevelopment. Construction of water and sewer will be vested to the City of Olympia Development Guidelines in force in 2007 per the Developer Agreement if construction occurs during the vesting period of the agreement.

### Gas/Power/Communication

Mapping of franchise utilities were not obtained. As the site is surrounded by developed areas, it assumed that existing systems are available for use. The capacity of these existing facilities is not known. Adequacy of these facilities would need to be assessed as part of any redevelopment.

## Stormwater

Per the Developer Agreement, development of this site will need to meet all of the requirements of the 2005 City of Olympia Drainage Design and Erosion Control Manual (DDECM). If the development were to occur after the vesting period of the agreement, then the current City of Olympia Drainage Design and Erosion Control would apply. This would include providing flow and water quality control as well as integrating low impact development strategies. Based on prior experience with parcel A, site soils should have good drainage although the good draining soils may be fairly deep. Parcels B-D are part of a closed basin and are not anticipated to have good draining soils. It is anticipated that drainage from these parcels would need to connect to any system designed for parcel A. Any facility proposed will have to be a 100% infiltration facility.

## Summary

To summarize, the Harrison West Site offers the most developable area and potential to provide parking in surface lots only. The site has a current Developer Agreement that vests development to codes in place in 2007 that is in force until 2021. The stormwater facilities will need to be more extensive when compared to the other two sites and any system would likely need to be deep to get to the well-drained soils. This site will also require the extension of streets, which would also likely include utility extensions, through the site. Therefore, this site likely has the most flexibility and fewest constraints but will potentially have higher infrastructure costs.

# Hilltop Site

## Legend

- Parcel Boundaries
- Contours 2ft 1996 (NGVD 29)
- Roads - Major
- Major Roads
- Ramp
- I 5; US 101
- Roads (Large Scale)
- Railroads
- County Border

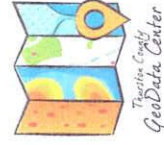
Scale 1: 5,335



Map Created Using GeoData Public Website

Published: 9/7/2018

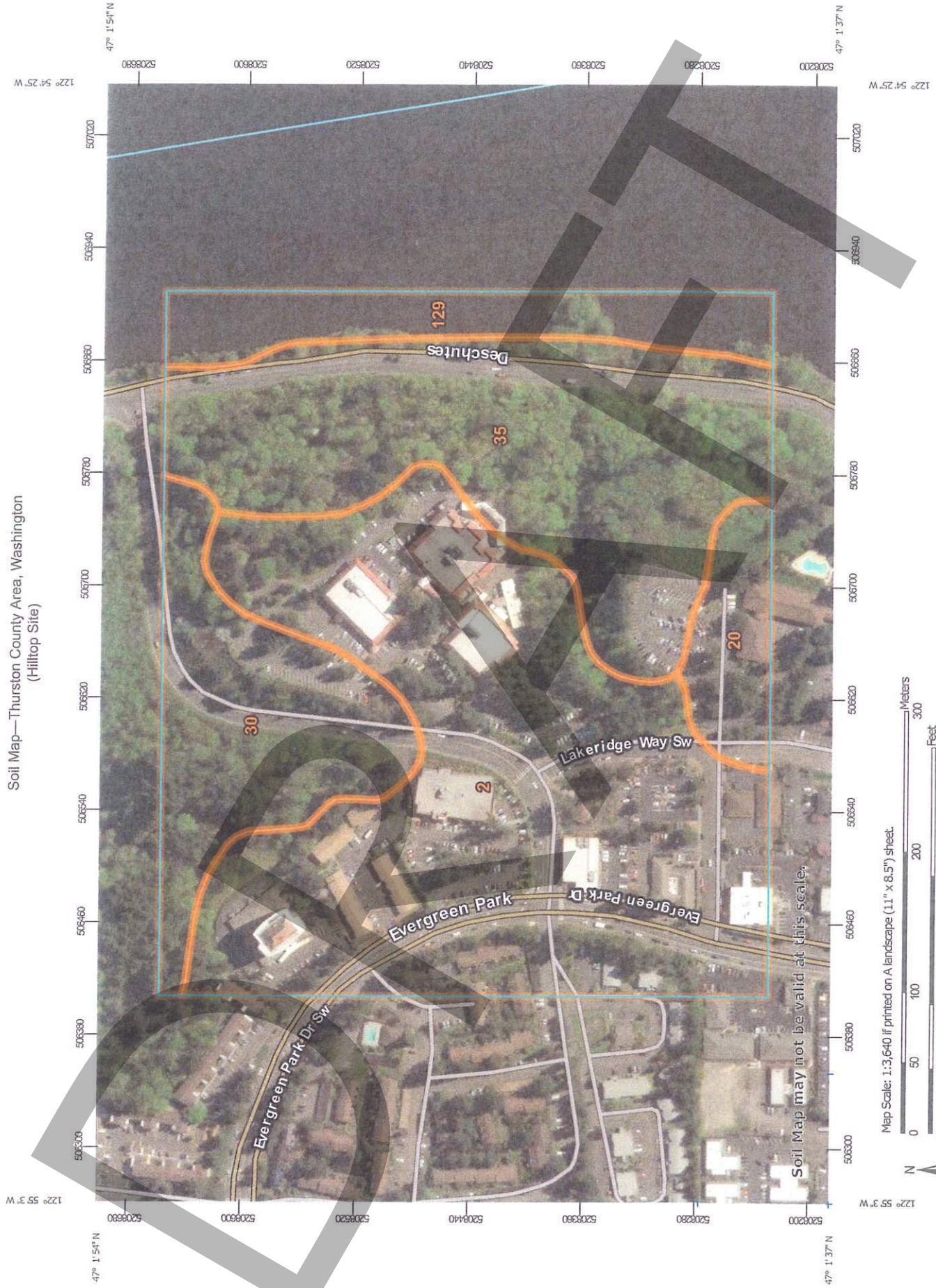
Note:



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# Soil Map—Thurston County Area, Washington (Hilltop Site)



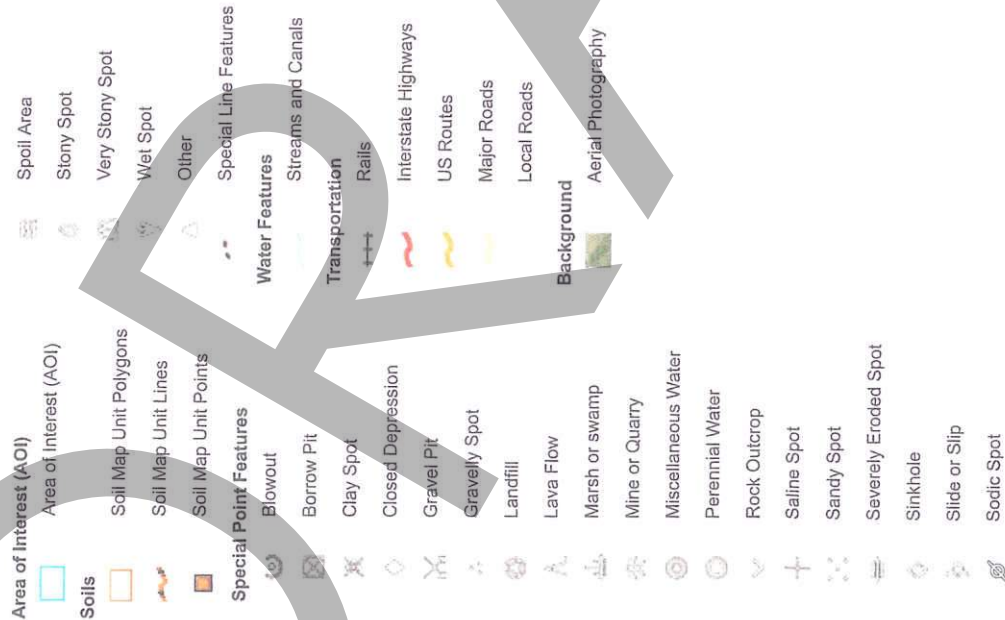
Map Scale: 1:3,640 if printed on A landscape (11" x 8.5") sheet.



Map projection: Web Mercator Corner coordinates: WGS84 Edge tics: UTM Zone 10N WGS84



## MAP LEGEND



## MAP INFORMATION

The soil surveys that comprise your AOI were mapped at 1:24,000.

Warning: Soil Map may not be valid at this scale.

Enlargement of maps beyond the scale of mapping can cause misunderstanding of the detail of mapping and accuracy of soil line placement. The maps do not show the small areas of contrasting soils that could have been shown at a more detailed scale.

Please rely on the bar scale on each map sheet for map measurements.

Source of Map: Natural Resources Conservation Service

Web Soil Survey URL:

Coordinate System: Web Mercator (EPSG:3857)

Maps from the Web Soil Survey are based on the Web Mercator projection, which preserves direction and shape but distorts distance and area. A projection that preserves area, such as the Albers equal-area conic projection, should be used if more accurate calculations of distance or area are required.

This product is generated from the USDA-NRCS certified data as of the version date(s) listed below.

Soil Survey Area: Thurston County Area, Washington  
Survey Area Data: Version 11, Feb 22, 2018

Soil map units are labeled (as space allows) for map scales 1:50,000 or larger.

Date(s) aerial images were photographed: Mar 29, 2016—Oct 10, 2016

The orthophoto or other base map on which the soil lines were compiled and digitized probably differs from the background imagery displayed on these maps. As a result, some minor shifting of map unit boundaries may be evident.

## Map Unit Legend

Map Unit Symbol	Map Unit Name	Acres in AOI	Percent of AOI
2	Alderwood gravelly sandy loam, 8 to 15 percent slopes	24.9	46.4%
20	Cagey loamy sand	2.3	4.4%
30	Dystic Xerochrepts, 60 to 90 percent slopes	7.3	13.7%
35	Everett very gravelly sandy loam, 30 to 50 percent slopes	14.8	27.6%
129	Water	4.2	7.9%
<b>Totals for Area of Interest</b>		<b>53.5</b>	<b>100.0%</b>

## Thurston County Area, Washington

### 2—Alderwood gravelly sandy loam, 8 to 15 percent slopes

#### Map Unit Setting

*National map unit symbol:* 2t626

*Elevation:* 50 to 800 feet

*Mean annual precipitation:* 20 to 60 inches

*Mean annual air temperature:* 46 to 52 degrees F

*Frost-free period:* 160 to 240 days

*Farmland classification:* Prime farmland if irrigated

#### Map Unit Composition

*Alderwood and similar soils:* 85 percent

*Minor components:* 15 percent

*Estimates are based on observations, descriptions, and transects of the mapunit.*

#### Description of Alderwood

##### Setting

*Landform:* Hills, ridges

*Landform position (two-dimensional):* Shoulder

*Landform position (three-dimensional):* Nose slope, talf

*Down-slope shape:* Convex, linear

*Across-slope shape:* Convex

*Parent material:* Glacial drift and/or glacial outwash over dense glaciomarine deposits

##### Typical profile

*A - 0 to 7 inches:* gravelly sandy loam

*Bw1 - 7 to 21 inches:* very gravelly sandy loam

*Bw2 - 21 to 30 inches:* very gravelly sandy loam

*Bg - 30 to 35 inches:* very gravelly sandy loam

*2Cd1 - 35 to 43 inches:* very gravelly sandy loam

*2Cd2 - 43 to 59 inches:* very gravelly sandy loam

##### Properties and qualities

*Slope:* 8 to 15 percent

*Depth to restrictive feature:* 20 to 39 inches to densic material

*Natural drainage class:* Moderately well drained

*Capacity of the most limiting layer to transmit water (Ksat):* Very low to moderately low (0.00 to 0.06 in/hr)

*Depth to water table:* About 18 to 37 inches

*Frequency of flooding:* None

*Frequency of ponding:* None

*Available water storage in profile:* Very low (about 2.7 inches)

##### Interpretive groups

*Land capability classification (irrigated):* None specified

*Land capability classification (nonirrigated):* 4s

*Hydrologic Soil Group:* B



*Forage suitability group:* Limited Depth Soils (G002XN302WA),  
Limited Depth Soils (G002XS301WA), Limited Depth Soils  
(G002XF303WA)

*Hydric soil rating:* No

#### Minor Components

##### Everett

*Percent of map unit:* 5 percent

*Landform:* Kames, eskers, moraines

*Landform position (two-dimensional):* Shoulder, footslope

*Landform position (three-dimensional):* Crest, base slope

*Down-slope shape:* Convex

*Across-slope shape:* Convex

*Hydric soil rating:* No

##### Indianola

*Percent of map unit:* 5 percent

*Landform:* Kames, eskers, terraces

*Landform position (three-dimensional):* Tread

*Down-slope shape:* Linear

*Across-slope shape:* Linear

*Hydric soil rating:* No

##### Shalcar

*Percent of map unit:* 3 percent

*Landform:* Depressions

*Landform position (three-dimensional):* Dip

*Down-slope shape:* Concave

*Across-slope shape:* Concave

*Hydric soil rating:* Yes

##### Norma

*Percent of map unit:* 2 percent

*Landform:* Depressions, drainageways

*Landform position (three-dimensional):* Dip

*Down-slope shape:* Concave, linear

*Across-slope shape:* Concave

*Hydric soil rating:* Yes

## Data Source Information

Soil Survey Area: Thurston County Area, Washington

Survey Area Data: Version 11, Feb 22, 2018

## Thurston County Area, Washington

### 20—Cagey loamy sand

#### Map Unit Setting

*National map unit symbol:* 2nd8d

*Elevation:* 330 to 980 feet

*Mean annual precipitation:* 40 to 60 inches

*Mean annual air temperature:* 50 degrees F

*Frost-free period:* 165 to 195 days

*Farmland classification:* Prime farmland if irrigated

#### Map Unit Composition

*Cagey and similar soils:* 85 percent

*Minor components:* 5 percent

*Estimates are based on observations, descriptions, and transects of the mapunit.*

#### Description of Cagey

##### Setting

*Landform:* Terraces

*Parent material:* Sandy glacial drift

##### Typical profile

*H1 - 0 to 6 inches:* loamy sand

*H2 - 6 to 28 inches:* loamy sand

*H3 - 28 to 60 inches:* fine sand

##### Properties and qualities

*Slope:* 0 to 4 percent

*Depth to restrictive feature:* More than 80 inches

*Natural drainage class:* Moderately well drained

*Capacity of the most limiting layer to transmit water (Ksat):* High to very high (5.95 to 19.98 in/hr)

*Depth to water table:* About 18 to 30 inches

*Frequency of flooding:* None

*Frequency of ponding:* None

*Available water storage in profile:* Low (about 4.0 inches)

##### Interpretive groups

*Land capability classification (irrigated):* None specified

*Land capability classification (nonirrigated):* 3w

*Hydrologic Soil Group:* A

*Forage suitability group:* Seasonally Wet Soils (G002XS201WA)

*Hydric soil rating:* No

#### Minor Components

##### Mckenna

*Percent of map unit:* 5 percent

*Landform:* Depressions

## Thurston County Area, Washington

### 30—Dystric Xerochrepts, 60 to 90 percent slopes

#### Map Unit Setting

*National map unit symbol:* 2nd8r

*Elevation:* 0 to 3,280 feet

*Mean annual precipitation:* 50 inches

*Mean annual air temperature:* 50 degrees F

*Frost-free period:* 180 days

*Farmland classification:* Not prime farmland

#### Map Unit Composition

*Dystric xerochrepts and similar soils:* 85 percent

*Minor components:* 5 percent

*Estimates are based on observations, descriptions, and transects of the mapunit.*

#### Description of Dystric Xerochrepts

##### Setting

*Landform:* Escarpments

*Parent material:* Colluvium and glacial till

##### Typical profile

*H1 - 0 to 4 inches:* very gravelly sandy loam

*H2 - 4 to 30 inches:* very gravelly sandy loam

*H3 - 30 to 34 inches:* very gravelly sandy loam

##### Properties and qualities

*Slope:* 60 to 90 percent

*Depth to restrictive feature:* 20 to 72 inches to densic material

*Natural drainage class:* Well drained

*Capacity of the most limiting layer to transmit water (Ksat):* Very low to moderately low (0.00 to 0.06 in/hr)

*Depth to water table:* More than 80 inches

*Frequency of flooding:* None

*Frequency of ponding:* None

*Available water storage in profile:* Very low (about 2.1 inches)

##### Interpretive groups

*Land capability classification (irrigated):* None specified

*Land capability classification (nonirrigated):* 7e

*Hydrologic Soil Group:* C

*Hydric soil rating:* No

#### Minor Components

##### Skipopa

*Percent of map unit:* 5 percent



## Thurston County Area, Washington

### 35—Everett very gravelly sandy loam, 30 to 50 percent slopes

#### Map Unit Setting

*National map unit symbol:* 2t62d

*Elevation:* 30 to 900 feet

*Mean annual precipitation:* 35 to 91 inches

*Mean annual air temperature:* 48 to 52 degrees F

*Frost-free period:* 180 to 240 days

*Farmland classification:* Not prime farmland

#### Map Unit Composition

*Everett and similar soils:* 80 percent

*Minor components:* 20 percent

*Estimates are based on observations, descriptions, and transects of the mapunit.*

#### Description of Everett

##### Setting

*Landform:* Kames, eskers, moraines

*Landform position (two-dimensional):* Backslope

*Landform position (three-dimensional):* Side slope

*Down-slope shape:* Convex

*Across-slope shape:* Convex

*Parent material:* Sandy and gravelly glacial outwash

##### Typical profile

*O<sub>i</sub> - 0 to 1 inches:* slightly decomposed plant material

*A - 1 to 3 inches:* very gravelly sandy loam

*B<sub>w</sub> - 3 to 24 inches:* very gravelly sandy loam

*C<sub>1</sub> - 24 to 35 inches:* very gravelly loamy sand

*C<sub>2</sub> - 35 to 60 inches:* extremely cobbly coarse sand

##### Properties and qualities

*Slope:* 30 to 50 percent

*Depth to restrictive feature:* More than 80 inches

*Natural drainage class:* Somewhat excessively drained

*Capacity of the most limiting layer to transmit water (Ksat):* High  
(1.98 to 5.95 in/hr)

*Depth to water table:* More than 80 inches

*Frequency of flooding:* None

*Frequency of ponding:* None

*Available water storage in profile:* Low (about 3.2 inches)

##### Interpretive groups

*Land capability classification (irrigated):* None specified

*Land capability classification (nonirrigated):* 7e

*Hydrologic Soil Group:* A

*Hydric soil rating:* No

## Minor Components

### Alderwood

*Percent of map unit:* 10 percent

*Landform:* Hills, ridges

*Landform position (two-dimensional):* Backslope

*Landform position (three-dimensional):* Nose slope, side slope, talf

*Down-slope shape:* Convex, linear

*Across-slope shape:* Convex

*Hydric soil rating:* No

### Indianola

*Percent of map unit:* 10 percent

*Landform:* Kames, eskers, terraces

*Landform position (two-dimensional):* Backslope

*Landform position (three-dimensional):* Side slope

*Down-slope shape:* Linear

*Across-slope shape:* Linear

*Hydric soil rating:* No

## Data Source Information

Soil Survey Area: Thurston County Area, Washington

Survey Area Data: Version 11, Feb 22, 2018



# City of Olympia



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Parcel Fabric Polygons

Parcel\_Lines

Road Frontage

Standard Boundary

Assessor Line (approximate)

Areas Needing Work

waValve (Zone)

Zone

waValve (Blow-off)

Blow-off, City of Olympia

Blow-off, Other Ownership

waValve (Butterfly)

Dr. Unknown

East

North

South

waHydrant

No, City of Olympia

No, Other Ownership

waHydrant (FDC)

Yes

waScada

waTank

waSamplingStation

waReservoir

waPumpStation

waProductionWell

waManhole

waFitting

Cap

Plug

waBackflowDevice

waVault

waPipe (City Owned/Maintained)

<Null> - City of Olympia

Fire, City of Olympia

Main, City of Olympia

Service/Irrigation, City of Olympia

waPipe (Private/Jurisdiction)

<Null> - Non-City Owned

Fire, Non-City Owned

Main, Non-City Owned

Service/Irrigation, Non-City Owned

waCasing

waAbandonedPipe

waChlorinePipe

1:2,400

0 0.0275 0.055 0.11 mi

0 0.045 0.09 0.18 km

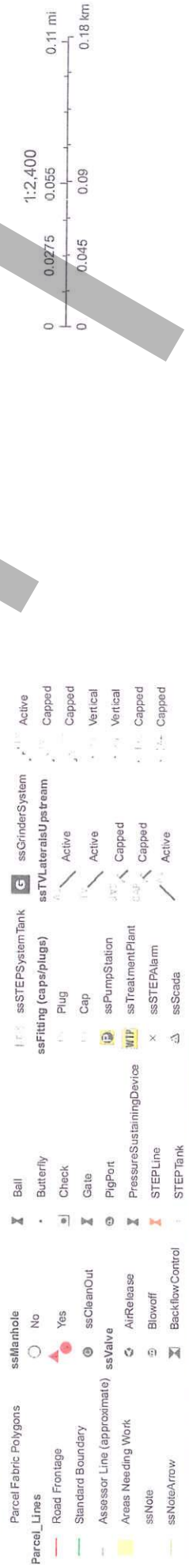
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# City of Olympia

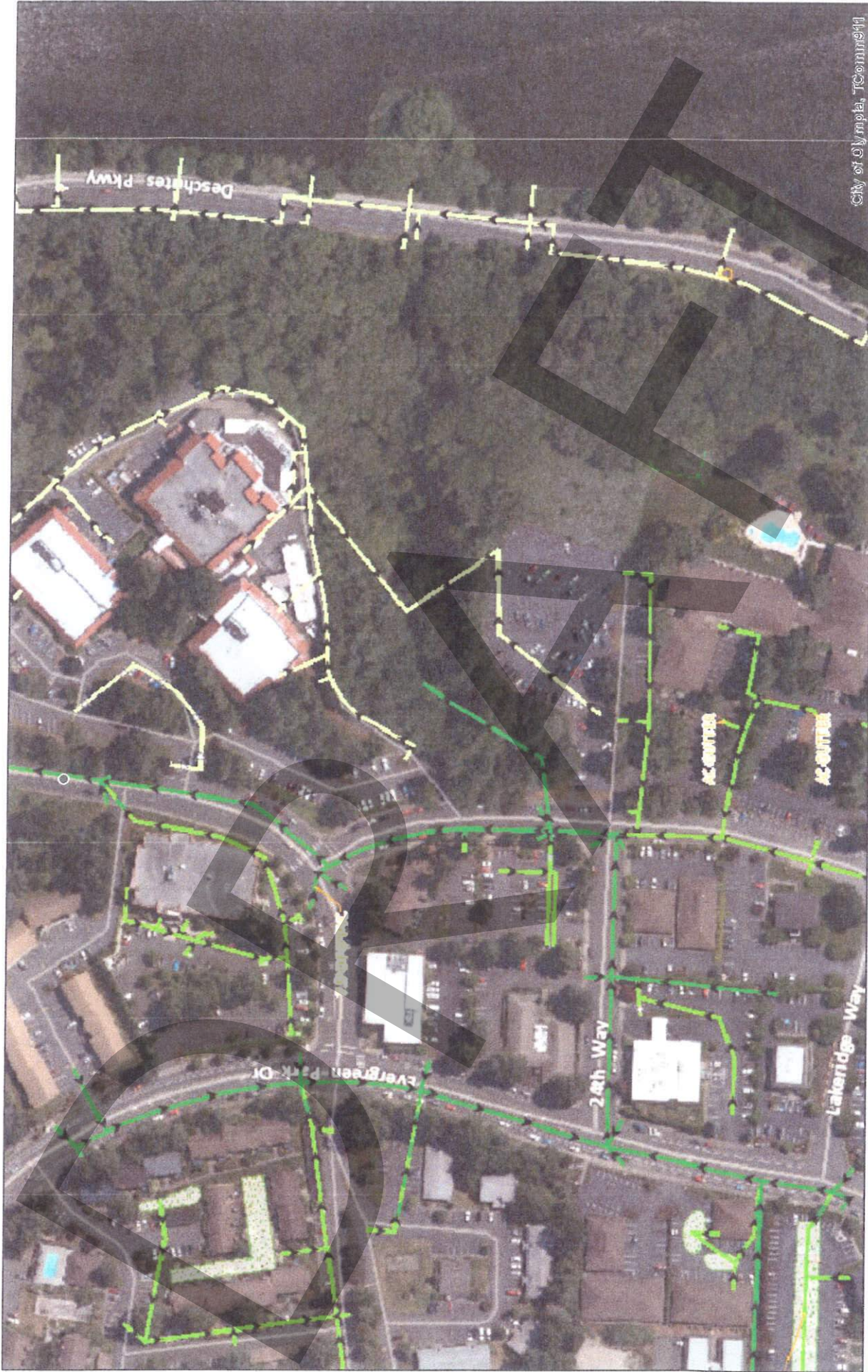


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swNote

swNoteArrow



# Plum Street Site

## Legend

- Parcel Boundaries
- Contours 2ft 1996 (NGVD 29)
- Roads - Major
- Major Roads
- Ramp
- I 5; US 101
- Roads (Large Scale)
- Railroads
- County Border

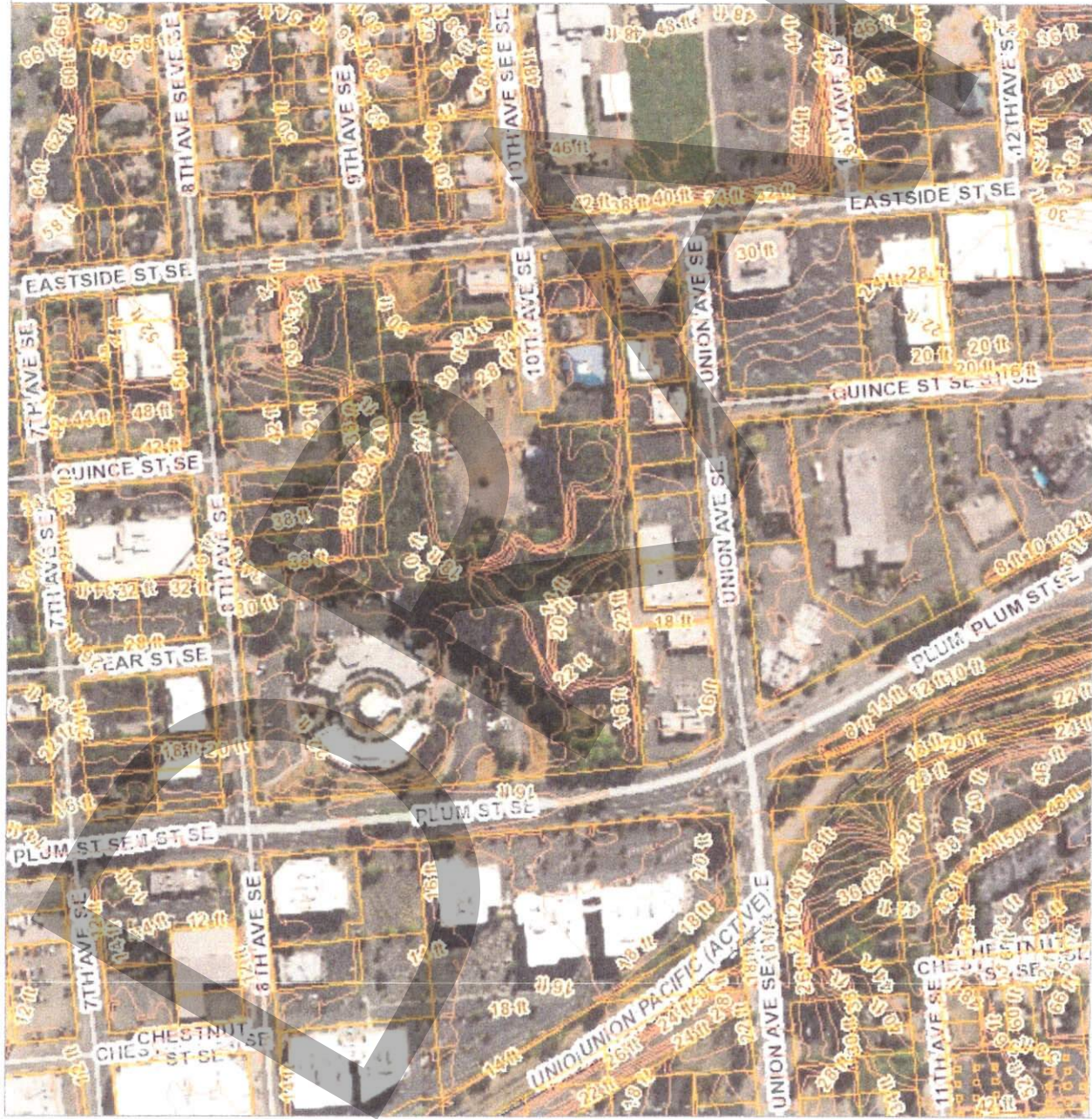
Scale 1"=5,335'



Map Created Using GeoData Public Website

Published: 9/7/2018

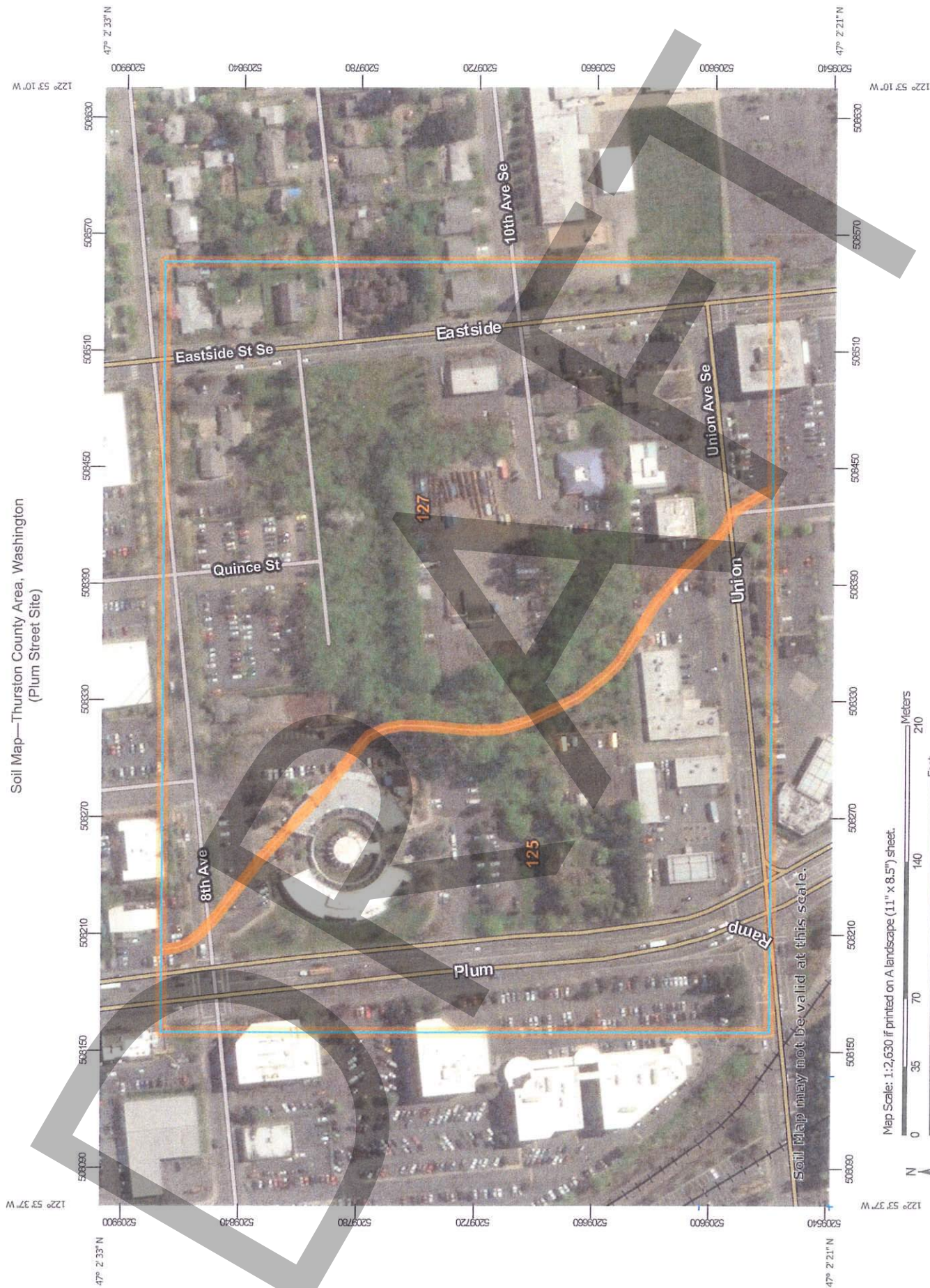
Note:



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Soil Map—Thurston County Area, Washington  
(Plum Street Site)

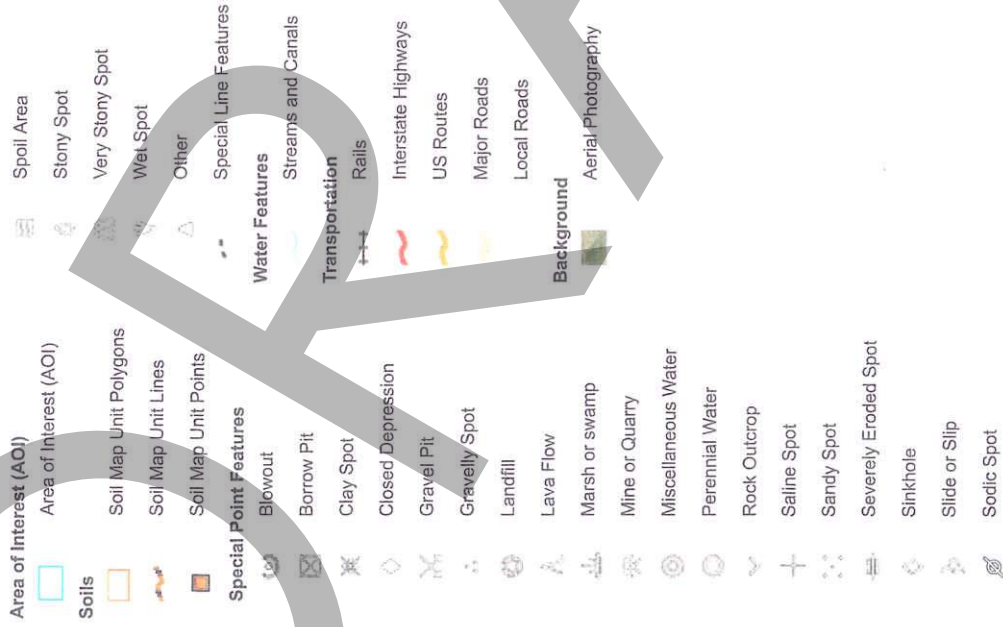


Map Scale: 1:2,630 if printed on A landscape (11" x 8.5") sheet.



Map projection: Web Mercator Corner coordinates: WGS84 Edge tics: UTM Zone 10N WGS84

## MAP LEGEND



## MAP INFORMATION

The soil surveys that comprise your AOI were mapped at 1:24,000.

Warning: Soil Map may not be valid at this scale.

Enlargement of maps beyond the scale of mapping can cause misunderstanding of the detail of mapping and accuracy of soil line placement. The maps do not show the small areas of contrasting soils that could have been shown at a more detailed scale.

Please rely on the bar scale on each map sheet for map measurements.

Source of Map: Natural Resources Conservation Service  
Web Soil Survey URL:  
Coordinate System: Web Mercator (EPSG:3857)

Maps from the Web Soil Survey are based on the Web Mercator projection, which preserves direction and shape but distorts distance and area. A projection that preserves area, such as the Albers equal-area conic projection, should be used if more accurate calculations of distance or area are required.

This product is generated from the USDA-NRCS certified data as of the version date(s) listed below.

Soil Survey Area: Thurston County Area, Washington  
Survey Area Data: Version 11, Feb 22, 2018

Soil map units are labeled (as space allows) for map scales 1:50,000 or larger.

Date(s) aerial images were photographed: Mar 29, 2016—Oct 10, 2016

The orthophoto or other base map on which the soil lines were compiled and digitized probably differs from the background imagery displayed on these maps. As a result, some minor shifting of map unit boundaries may be evident.



## Map Unit Legend

Map Unit Symbol	Map Unit Name	Acres in AOI	Percent of AOI
125	Xerorthents, 0 to 5 percent slopes	12.1	39.8%
127	Yelm fine sandy loam, 3 to 15 percent slopes	18.4	60.2%
Totals for Area of Interest		30.5	100.0%



## Thurston County Area, Washington

### 125—Xerorthents, 0 to 5 percent slopes

#### Map Unit Setting

*National map unit symbol:* 2nd87

*Elevation:* 0 to 2,620 feet

*Mean annual precipitation:* 30 to 60 inches

*Mean annual air temperature:* 39 to 50 degrees F

*Frost-free period:* 150 to 200 days

*Farmland classification:* Not prime farmland

#### Map Unit Composition

*Xerorthents and similar soils:* 100 percent

*Estimates are based on observations, descriptions, and transects of the mapunit.*

#### Description of Xerorthents

##### Setting

*Landform:* Tidal flats

*Parent material:* Sandy and loamy cut and fill material

##### Typical profile

*H1 - 0 to 60 inches:* variable

##### Properties and qualities

*Slope:* 0 to 5 percent

*Depth to restrictive feature:* More than 80 inches

*Natural drainage class:* Somewhat excessively drained

*Depth to water table:* About 24 inches

*Frequency of flooding:* None

*Frequency of ponding:* None

##### Interpretive groups

*Land capability classification (irrigated):* None specified

*Land capability classification (nonirrigated):* 7s

*Hydric soil rating:* No

## Data Source Information

Soil Survey Area: Thurston County Area, Washington

Survey Area Data: Version 11, Feb 22, 2018

## Thurston County Area, Washington

### 127—Yelm fine sandy loam, 3 to 15 percent slopes

#### Map Unit Setting

*National map unit symbol:* 2nd89

*Elevation:* 80 to 980 feet

*Mean annual precipitation:* 30 to 60 inches

*Mean annual air temperature:* 50 degrees F

*Frost-free period:* 170 to 200 days

*Farmland classification:* Farmland of statewide importance

#### Map Unit Composition

*Yelm and similar soils:* 85 percent

*Minor components:* 3 percent

*Estimates are based on observations, descriptions, and transects of the mapunit.*

#### Description of Yelm

##### Setting

*Landform:* Outwash terraces

*Parent material:* Glacial outwash

##### Typical profile

*H1 - 0 to 8 inches:* fine sandy loam

*H2 - 8 to 46 inches:* fine sandy loam

*H3 - 46 to 60 inches:* loamy sand

##### Properties and qualities

*Slope:* 3 to 15 percent

*Depth to restrictive feature:* More than 80 inches

*Natural drainage class:* Moderately well drained

*Capacity of the most limiting layer to transmit water (Ksat):* High  
(1.98 to 5.95 in/hr)

*Depth to water table:* About 18 to 36 inches

*Frequency of flooding:* None

*Frequency of ponding:* None

*Available water storage in profile:* High (about 11.0 inches)

##### Interpretive groups

*Land capability classification (irrigated):* 4e

*Land capability classification (nonirrigated):* 3e

*Hydrologic Soil Group:* B

*Forage suitability group:* Soils with Moderate Limitations  
(G002XS601WA)

*Hydric soil rating:* No

#### Minor Components

##### Skipopa

*Percent of map unit:* 3 percent



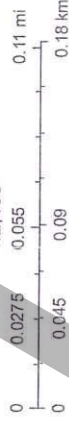
# City of Olympia



9/7/2018 3:59:08 PM



1:2,400



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# City of Olympia



9/7/2018 3:55:33 PM

Parcel Fabric Polygons

Parcel\_Lines

Road Frontage

Standard Boundary

Assessor Line (approximate)

Areas Needing Work

City\_AddressPoints

ssNote

ssManhole

ssValve

ssCleanOut

ssRelease

ssBlowoff

BackflowControl

Ball

Butterfly

Check

Gate

PigPort

PressureSustainingDevice

STEPLine

ssNoteArrow

No

Yes

ssCleanOut

ssValve

ssRelease

ssBlowoff

ssSTEPSystemTank

ssFitting (caps/plugs)

Plug

Cap

ssPumpStation

ssTreatmentPlant

ssSTEPAlarm

ssScada

ssGrinderSystem

ssTVLateralsUpstream

Active

Active

Capped

Capped

Active

Active

Capped

Capped

Vertical

Vertical

Capped

Capped

1:1,200

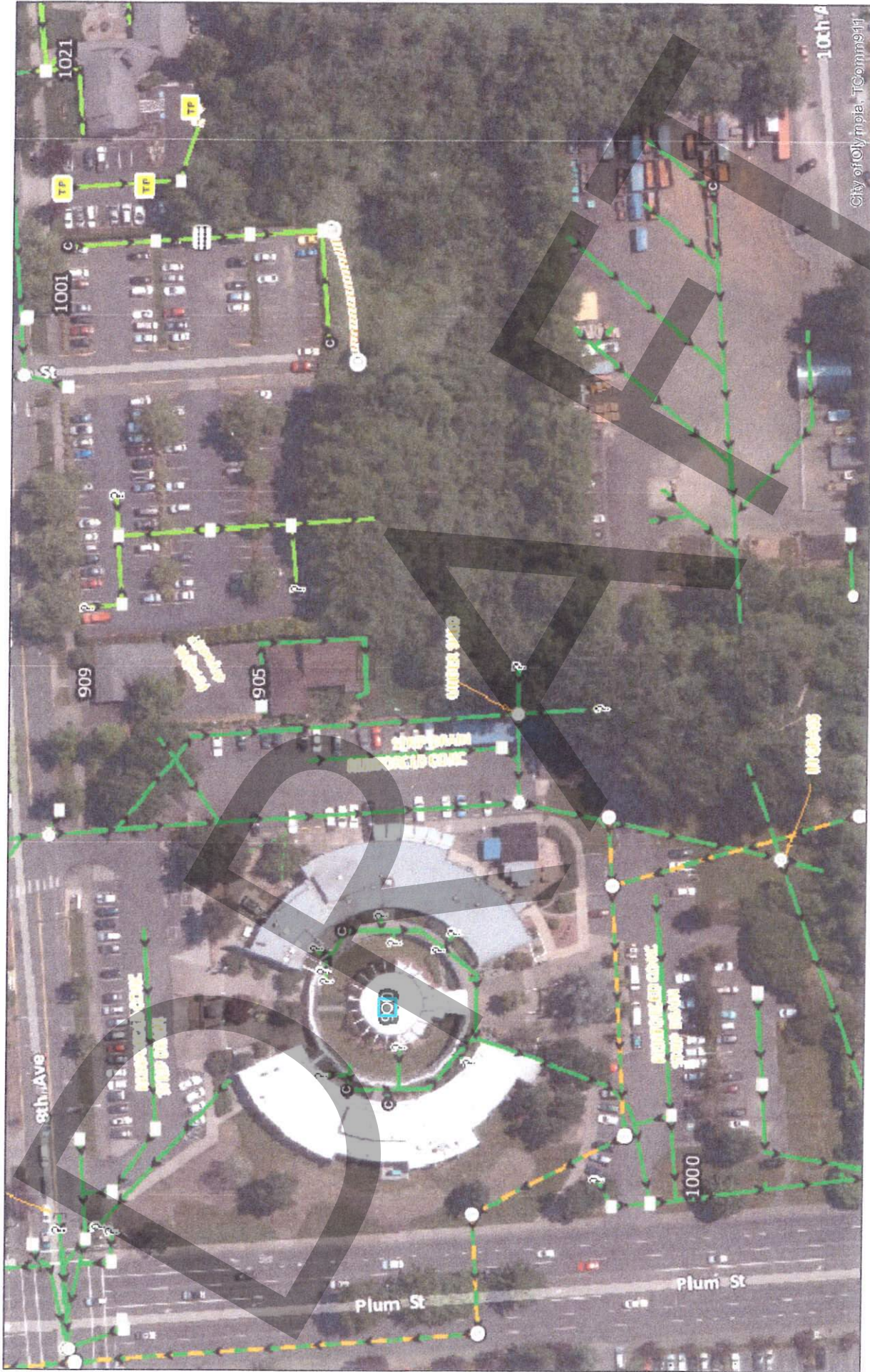
0 0.0125 0.025 0.045 0.05 mi

0 0.0225 0.045 0.09 km

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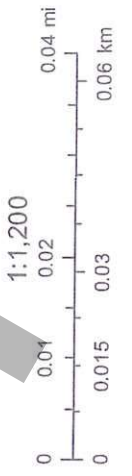


# City of Olympia



9/18/2018 3:50:52 PM

- City\_AddressPoints
- swNoteArrow
- swCatchBasin
- swNote
- swServicePoint (Roof/Yard Drains)
- swCleanOut



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# Harrison Site

## Legend

- Parcel Boundaries
- Contours 2ft 1996 (NGVD 29)
- Roads - Major
- Major Roads
- Ramp
- I 5; US 101
- Roads (Large Scale)
- Railroads
- County Border

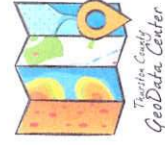
Scale 1: 5,335



Map Created Using GeoData Public Website

Published: 9/7/2018

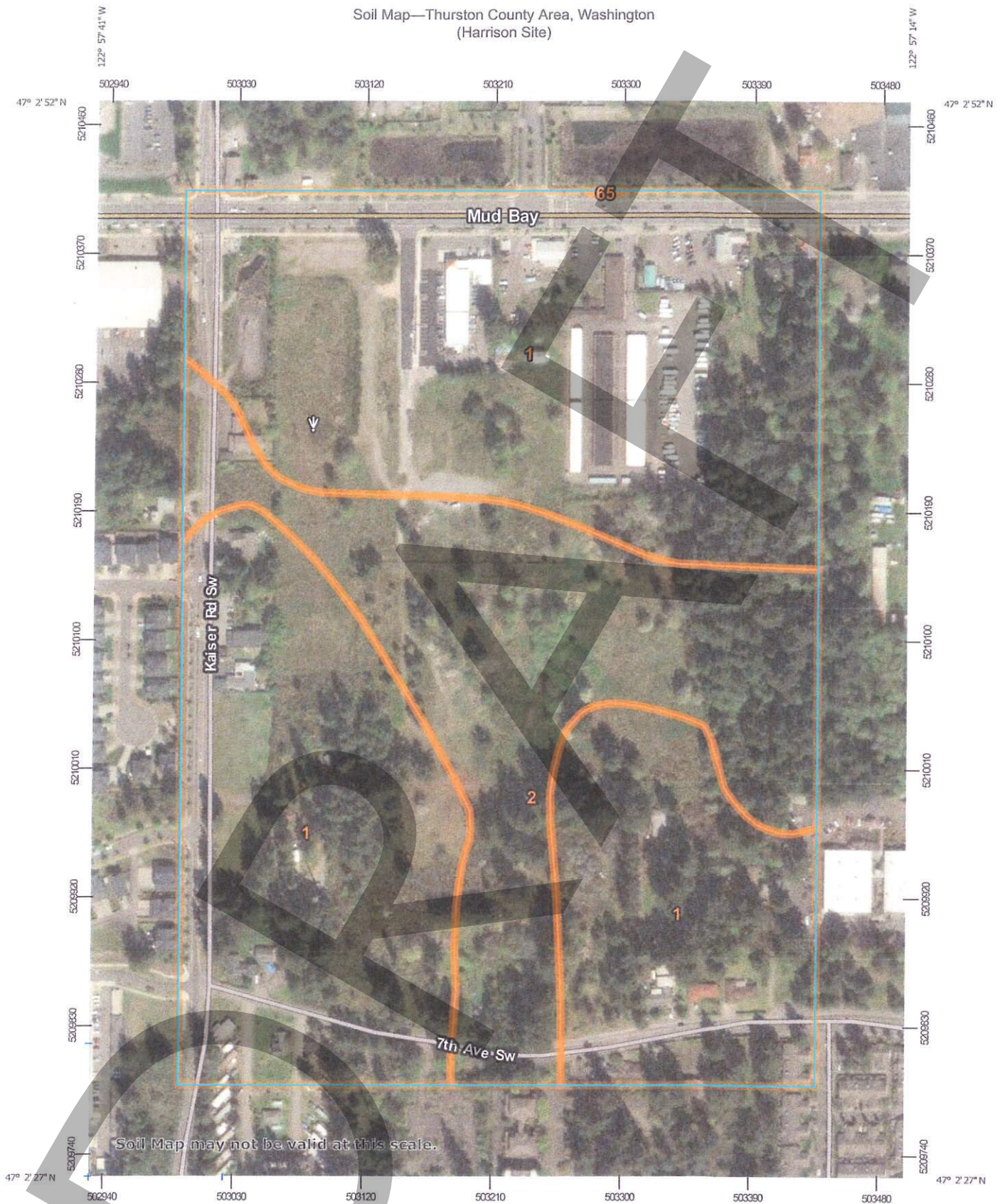
Note:



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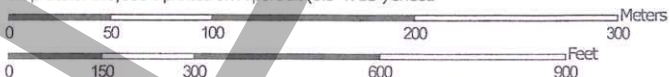


Soil Map—Thurston County Area, Washington  
(Harrison Site)



Soil Map may not be valid at this scale.

Map Scale: 1:3,660 if printed on A portrait (8.5" x 11") sheet.



Map projection: Web Mercator Corner coordinates: WGS84 Edge ticks: UTM Zone 10N WGS84

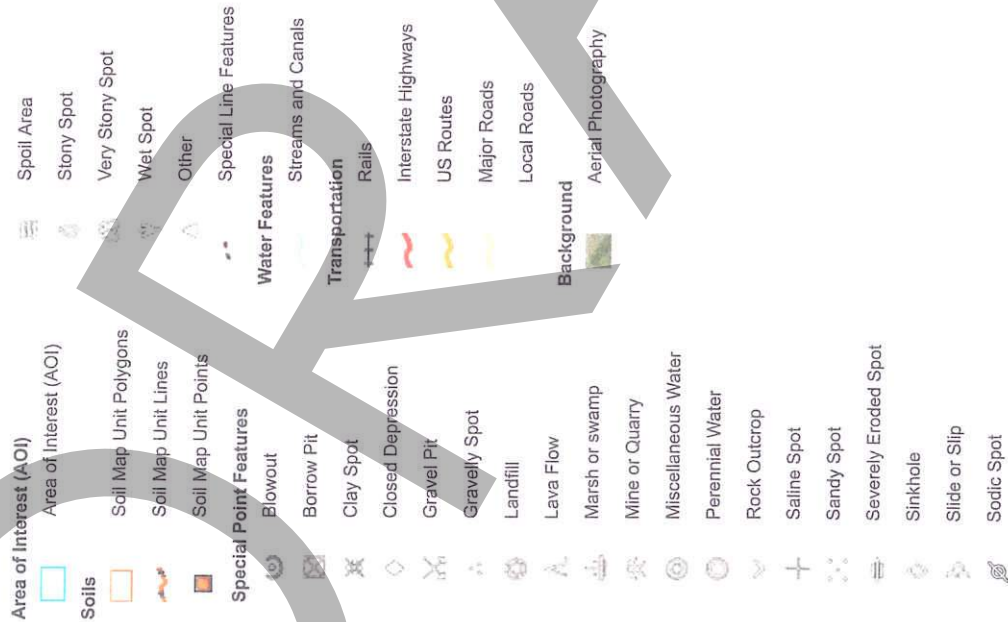


Natural Resources  
Conservation Service

Web Soil Survey  
National Cooperative Soil Survey

9/7/2018  
Page 1 of 3

## MAP LEGEND



## MAP INFORMATION

The soil surveys that comprise your AOI were mapped at 1:24,000.

**Warning:** Soil Map may not be valid at this scale.

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Please rely on the bar scale on each map sheet for map measurements.

Source of Map: Natural Resources Conservation Service

Web Soil Survey URL:

Coordinate System: Web Mercator (EPSG:3857)

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This product is generated from the USDA-NRCS certified data as of the version date(s) listed below.

Soil Survey Area: Thurston County Area, Washington

Survey Area Data: Version 11, Feb 22, 2018

Soil map units are labeled (as space allows) for map scales 1:50,000 or larger.

Date(s) aerial images were photographed: Apr 1, 2016—Sep 27, 2016

The orthophoto or other base map on which the soil lines were compiled and digitized probably differs from the background imagery displayed on these maps. As a result, some minor shifting of map unit boundaries may be evident.



## Map Unit Legend

Map Unit Symbol	Map Unit Name	Acres in AOI	Percent of AOI
1	Alderwood gravelly sandy loam, 0 to 8 percent slopes	51.2	74.3%
2	Alderwood gravelly sandy loam, 8 to 15 percent slopes	17.7	25.6%
65	McKenna gravelly silt loam, 0 to 5 percent slopes	0.0	0.0%
Totals for Area of Interest		68.9	100.0%



## Thurston County Area, Washington

### 1—Alderwood gravelly sandy loam, 0 to 8 percent slopes

#### Map Unit Setting

*National map unit symbol:* 2t625

*Elevation:* 50 to 800 feet

*Mean annual precipitation:* 25 to 60 inches

*Mean annual air temperature:* 48 to 52 degrees F

*Frost-free period:* 160 to 240 days

*Farmland classification:* Prime farmland if irrigated

#### Map Unit Composition

*Alderwood and similar soils:* 85 percent

*Minor components:* 15 percent

*Estimates are based on observations, descriptions, and transects of the mapunit.*

#### Description of Alderwood

##### Setting

*Landform:* Hills, ridges

*Landform position (two-dimensional):* Summit

*Landform position (three-dimensional):* Crest, talf

*Down-slope shape:* Convex, linear

*Across-slope shape:* Convex

*Parent material:* Glacial drift and/or glacial outwash over dense glaciomarine deposits

##### Typical profile

*A - 0 to 7 inches:* gravelly sandy loam

*Bw1 - 7 to 21 inches:* very gravelly sandy loam

*Bw2 - 21 to 30 inches:* very gravelly sandy loam

*Bg - 30 to 35 inches:* very gravelly sandy loam

*2Cd1 - 35 to 43 inches:* very gravelly sandy loam

*2Cd2 - 43 to 59 inches:* very gravelly sandy loam

##### Properties and qualities

*Slope:* 0 to 8 percent

*Depth to restrictive feature:* 20 to 39 inches to densic material

*Natural drainage class:* Moderately well drained

*Capacity of the most limiting layer to transmit water (Ksat):* Very low to moderately low (0.00 to 0.06 in/hr)

*Depth to water table:* About 18 to 37 inches

*Frequency of flooding:* None

*Frequency of ponding:* None

*Available water storage in profile:* Very low (about 2.7 inches)

##### Interpretive groups

*Land capability classification (irrigated):* None specified

*Land capability classification (nonirrigated):* 4s

*Hydrologic Soil Group:* B

*Forage suitability group:* Limited Depth Soils (G002XN302WA),  
Limited Depth Soils (G002XF303WA), Limited Depth Soils  
(G002XS301WA)  
*Hydric soil rating:* No

#### Minor Components

##### Mckenna

*Percent of map unit:* 5 percent  
*Landform:* Depressions, drainageways  
*Landform position (three-dimensional):* Dip  
*Down-slope shape:* Concave, linear  
*Across-slope shape:* Concave  
*Hydric soil rating:* Yes

##### Everett

*Percent of map unit:* 5 percent  
*Landform:* Kames, eskers, moraines  
*Landform position (two-dimensional):* Summit, shoulder  
*Landform position (three-dimensional):* Crest, interfluvium  
*Down-slope shape:* Convex  
*Across-slope shape:* Convex  
*Hydric soil rating:* No

##### Shalcar

*Percent of map unit:* 3 percent  
*Landform:* Depressions  
*Landform position (three-dimensional):* Dip  
*Down-slope shape:* Concave  
*Across-slope shape:* Concave  
*Hydric soil rating:* Yes

##### Norma

*Percent of map unit:* 2 percent  
*Landform:* Depressions, drainageways  
*Landform position (three-dimensional):* Dip  
*Down-slope shape:* Concave, linear  
*Across-slope shape:* Concave  
*Hydric soil rating:* Yes

## Data Source Information

**Soil Survey Area:** Thurston County Area, Washington  
**Survey Area Data:** Version 11, Feb 22, 2018

## Thurston County Area, Washington

### 2—Alderwood gravelly sandy loam, 8 to 15 percent slopes

#### Map Unit Setting

*National map unit symbol:* 2t626

*Elevation:* 50 to 800 feet

*Mean annual precipitation:* 20 to 60 inches

*Mean annual air temperature:* 46 to 52 degrees F

*Frost-free period:* 160 to 240 days

*Farmland classification:* Prime farmland if irrigated

#### Map Unit Composition

*Alderwood and similar soils:* 85 percent

*Minor components:* 15 percent

*Estimates are based on observations, descriptions, and transects of the mapunit.*

#### Description of Alderwood

##### Setting

*Landform:* Hills, ridges

*Landform position (two-dimensional):* Shoulder

*Landform position (three-dimensional):* Nose slope, talf

*Down-slope shape:* Convex, linear

*Across-slope shape:* Convex

*Parent material:* Glacial drift and/or glacial outwash over dense glaciomarine deposits

##### Typical profile

*A - 0 to 7 inches:* gravelly sandy loam

*Bw1 - 7 to 21 inches:* very gravelly sandy loam

*Bw2 - 21 to 30 inches:* very gravelly sandy loam

*Bg - 30 to 35 inches:* very gravelly sandy loam

*2Cd1 - 35 to 43 inches:* very gravelly sandy loam

*2Cd2 - 43 to 59 inches:* very gravelly sandy loam

##### Properties and qualities

*Slope:* 8 to 15 percent

*Depth to restrictive feature:* 20 to 39 inches to densic material

*Natural drainage class:* Moderately well drained

*Capacity of the most limiting layer to transmit water (Ksat):* Very low to moderately low (0.00 to 0.06 in/hr)

*Depth to water table:* About 18 to 37 inches

*Frequency of flooding:* None

*Frequency of ponding:* None

*Available water storage in profile:* Very low (about 2.7 inches)

##### Interpretive groups

*Land capability classification (irrigated):* None specified

*Land capability classification (nonirrigated):* 4s

*Hydrologic Soil Group:* B



*Forage suitability group:* Limited Depth Soils (G002XN302WA),  
Limited Depth Soils (G002XS301WA), Limited Depth Soils  
(G002XF303WA)  
*Hydric soil rating:* No

#### Minor Components

##### Everett

*Percent of map unit:* 5 percent  
*Landform:* Kames, eskers, moraines  
*Landform position (two-dimensional):* Shoulder, footslope  
*Landform position (three-dimensional):* Crest, base slope  
*Down-slope shape:* Convex  
*Across-slope shape:* Convex  
*Hydric soil rating:* No

##### Indianola

*Percent of map unit:* 5 percent  
*Landform:* Kames, eskers, terraces  
*Landform position (three-dimensional):* Tread  
*Down-slope shape:* Linear  
*Across-slope shape:* Linear  
*Hydric soil rating:* No

##### Shalcar

*Percent of map unit:* 3 percent  
*Landform:* Depressions  
*Landform position (three-dimensional):* Dip  
*Down-slope shape:* Concave  
*Across-slope shape:* Concave  
*Hydric soil rating:* Yes

##### Norma

*Percent of map unit:* 2 percent  
*Landform:* Depressions, drainageways  
*Landform position (three-dimensional):* Dip  
*Down-slope shape:* Concave, linear  
*Across-slope shape:* Concave  
*Hydric soil rating:* Yes

## Data Source Information

**Soil Survey Area:** Thurston County Area, Washington  
**Survey Area Data:** Version 11, Feb 22, 2018

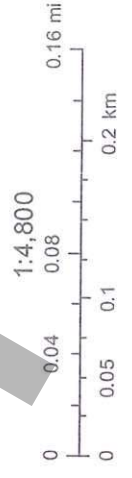


# City of Olympia



9/19/2018 3:09:41 PM

- wa Tank
- waPumpStation
- waManhole
- waReservoir
- waProductionWell
- waVault



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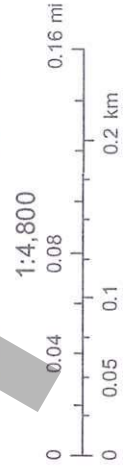


# City of Olympia



9/18/2018 3:48:42 PM

- swHotspot
- swOpenChannel
- ssGravityMain (combined)
- swForceMain
- Yes



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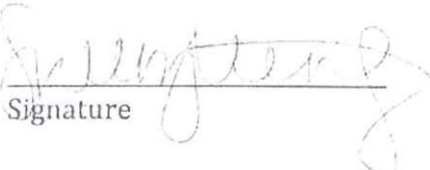
City of Olympia | Capital of Washington State

P.O. Box 1967, Olympia, WA 98507-1967

olympiawa.gov

Date: 6/12/17

As owner of MPH Holdings LLC, I hereby acknowledge receipt of the attached resolution, Resolution number 1881, have reviewed it and agree to its terms.

  
Signature

**MAYOR:** Cheryl Selby, **MAYOR PRO TEM:** Nathaniel Jones, **CITY MANAGER:** Steven R. Hall  
**COUNCILMEMBERS:** Jessica Bateman, Clark Gilman, Julie Hankins, Jeannine Roe, Jim Cooper

RESOLUTION NO. M-1881

A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF OLYMPIA, WASHINGTON,  
APPROVING AMENDMENT NO. 2 TO THE DEVELOPMENT AGREEMENT BY AND  
BETWEEN THE CITY OF OLYMPIA AND MPH HOLDINGS, LLC FOR THE DEVELOPMENT OF  
REAL PROPERTY

WHEREAS, on July 10, 2007, the City of Olympia (the City) and MPH Holdings, LLC (the Developer) entered into a *Development Agreement for the Development of Certain Property* (the Agreement); and

WHEREAS, on March 31, 2009, the Parties entered into Amendment No. 1 to the Agreement, under which the time period of validity of the Agreement was extended; and

WHEREAS, pursuant to Section 22 of the Agreement, the parties wish to amend the terms of the Agreement in certain areas; and

WHEREAS, this amendment complies with RCW 6.70B.170 - .210; and

WHEREAS, pursuant to RCW 36.70B.200, the Olympia City Council held a public hearing on June 6, 2017, and considered testimony from the public and City staff on the proposed amendment to the Agreement's terms in certain areas; and

WHEREAS, this proposed amendment complies with RCW 6.70B.170 - .210;

NOW, THEREFORE, THE OLYMPIA CITY COUNCIL DOES HEREBY RESOLVE as follows:

1. Approval of Amendment to Development Agreement. In accordance with RCW 36.70B.200, *Amendment No. 2 to the Development Agreement By and Between the City of Olympia and MPH Holdings, LLC for the Development of Certain Property* attached hereto and incorporated herein as Exhibit A, is hereby approved.
2. Conditions of Approval. The Development Agreement shall be null and void and no longer in legal effect unless the following occurs:

The Developer shall sign *Amendment No. 2 to the Development Agreement By and Between the City of Olympia and MPH Holdings, LLC for the Development of Certain Property* within ten (10) days of approval by the City of Olympia City Council. Also within ten (10) days of approval by the City of Olympia City Council, the Developer shall sign an acknowledgement that they have reviewed and agree to the terms in this Resolution. In addition, the Developer shall submit a request for deviation from the EDDS pertinent to Exhibit B to the City Engineer within ninety (90) days of their signature of *Amendment No. 2 to the Development Agreement By and Between the City of Olympia and MPH Holdings, LLC for the Development of Certain Property*. If an EDDS deviation request, consistent with Exhibit B to the Agreement, is not approved by the City Engineer within one hundred fifty (150) days of their signature (ninety days plus sixty days), the Development Agreement shall be null and void and of no legal effect. As part of the EDDS

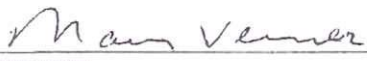
deviation review, the City Engineer, in their discretion, may seek additional clarifications and more detail consistent with Exhibit B.

3. City Manager Authority. The City Manager is directed and authorized to execute on behalf of the City of Olympia Amendment No. 2 to the Development Agreement By and Between the City of Olympia and MPH Holdings, LLC for the Development of Certain Property, and to make any minor modifications as may be required and are consistent with the intent of the attached Amendment No. 2, or to correct any scrivener's errors.
4. Severability. The provisions of this Resolution are declared separate and severable. In any provision of this Resolution or its application to any person or circumstance is held invalid, the remainder of this Resolution or application of the provision to other persons or circumstances, shall be unaffected.
5. Ratification. Any act consistent with the authority and prior to the effective date of this Resolution is hereby ratified and confirmed.

PASSED BY THE OLYMPIA CITY COUNCIL this 1<sup>st</sup> day of June 2017.

  
MAYOR

ATTEST:

  
CITY CLERK

APPROVED AS TO FORM:

  
DEPUTY CITY ATTORNEY



**AMENDMENT NO. 2  
TO DEVELOPMENT AGREEMENT BY AND BETWEEN  
THE CITY OF OLYMPIA  
AND MPH HOLDINGS, LLC,  
FOR THE DEVELOPMENT OF CERTAIN PROPERTY**

This Amendment No. 2 is effective as of the date of the last authorizing signature affixed hereto. The parties ("Parties") to this Amendment No. 2 are the CITY OF OLYMPIA, a Washington municipal corporation (the "City"), and MPH HOLDINGS, LLC, a Washington limited liability company (the "Developer").

**RECITALS**

1. On July 10, 2007, the City and the Developer entered into a Development Agreement for the Development of Certain Property (the "Agreement").
2. The Agreement covered certain properties and provided that the term of the Agreement was to run for a defined period, unless extended or terminated as provided in the Agreement.
3. Subsequent to approval of the Agreement, the Developer has applied for and has been granted various permits for installation of improvements upon the covered property, including street, water, sewer, and stormwater improvements (the "Permits"). That installation continues and these permits remain valid at this time. A list of the Permits is attached hereto as Exhibit C, said list being incorporated herein by this reference. The City Engineer has approved certain modifications to the Permits, which constitute a net upgrade to the public health, safety and welfare. This recital does not grant, expand or reduce any authority under these permits. It is simply an acknowledgment of permits already granted.
4. On March 31, 2009, the Parties entered into Amendment No. 1 to the Agreement, under which the time period of validity of the Agreement was extended.
5. Since execution of Amendment No. 1, in 2013 the Developer submitted certain conceptual documents which proposed a realignment of the streets within the covered property, including an element which through the inclusion of an additional property, would provide for extension of the north-south roadway to 7<sup>th</sup> Avenue. Thereafter, the Developer applied for an amendment to the then applicable City of Olympia Zoning Code (the "Zoning Code") so as to broaden the authorized uses within the general area of the covered property. Subsequent to that application, the City recommended that the Developer integrate its specific request with the zoning review for the entire area then under consideration by the City. The Developer did so.

6. Such review by the City of what has been referred to as the "Kaiser-Harrison Opportunity Area" led to the adoption of area-wide amendments to the Zoning Code and the Olympia Comprehensive Plan (the "Comprehensive Plan").
7. These amendments to the Zoning Code implement the authorization of more extensive use classifications for the entire area. This expansion is believed by both Parties to be reflective of the goal to achieve the best uses of the properties subject to this Amendment, which includes the land subject to the Agreement.
8. With the finalization of zoning review, the Developer reactivated its review of potential modifications in the provisions of the Agreement, which would result in the most effective and mutually beneficial development of the covered property.
9. The Parties have discussed a variety of factors, including the basic road layout. It was agreed to add a parcel on the southerly boundary of the original site so as to assure a direct connection for the north-south main street to 7<sup>th</sup> Avenue.
10. Based upon all applicable factors, the City and the Developer find it appropriate to amend the Agreement's terms in certain areas.

**NOW, THEREFORE, THE PARTIES AGREE AS FOLLOWS:**

1. The following shall constitute amendments to the identified sections and provisions of the original Agreement and Amendment No. 1 thereto:
  - A. Section 1 of the Agreement, *The Project*, is hereby replaced to read as follows:

Section 1: *The Project*. The Project is the development and use of the Property, consisting of approximately 24 acres in the City of Olympia. The Project is proposed to be a multi-use project consistent with the Zoning Code provisions in effect as of the date of the execution of this Amendment No. 2. It is anticipated that MPH will be seeking approval of one or more Binding Site Plans covering the property.
  - B. Section 4 of the Agreement, *Exhibits*, is hereby replaced to read as follows:

Section 4: *Exhibits*. The exhibits to the Agreement adopted in 2007 shall be replaced and succeeded by the following exhibits reflecting the legal description

of the covered properties and one or more maps showing various elements of the anticipated layout of the Property:

Exhibit A - Provides the legal description of the property subject to this Agreement, including the additional parcel.

Exhibit B - Road system layout and street section views, including basic location of road improvements, the final construction of which shall be subject to approval of individual Binding Site Plans. This Amendment does not certify or pre-approve Exhibit B as a Binding Site Plan or for any other land use review or permits, except the Permits that have been previously approved.

Exhibit C - List of prior permits.

Exhibit D - Phasing.

- C. Section 7 of the Agreement, *Term of Agreement*, is hereby replaced to read as follows:

Section 7: *Term of Agreement*. This Agreement shall be deemed to have commenced upon the effective date of the adopting Ordinance approving the original of this Agreement, and upon signature of this Amendment No. 2 by both parties and shall continue in full force and effect until January 1, 2022, unless extended or terminated as provided herein. Following the expiration of the term or any extension thereof, or if sooner terminated, this Agreement and the amendments thereto shall have no force and effect, subject, however, to post-termination obligations of the Developer or Landowner.

- D. Section 9 of the Agreement, *Permitted Uses, Development Standards*, is hereby replaced to read as follows:

Section 9: *Permitted Uses, Development Standards, & Phasing*. Whether developed in one phase or a series of phases as anticipated by Section 13, the property may be used for such uses and structure sizes as may be permitted within the development under the provisions of the Zoning Code provisions



in effect as of the date of the execution of this Amendment No. 2. Except for the agreed street improvements and any associated storm water and/or City of Olympia Engineering Design and Development Standards (the "EDDS") revisions required for said street improvements as set forth in this Amendment No. 2 and its Exhibits, the EDDS standards in effect as of the date of the Agreement and any permits or approvals previously granted, [a] the permitted uses, [b] the density and intensity of use, [c] the maximum height and size of proposed buildings, [d] provisions for reservation and dedication of land or payment of fees in lieu of dedication for public purposes, [e] as noted above, the existing Land Use Regulations relating to, among other items, the construction, installation and extension of public improvements, and [f] the development guidelines and standards for and applicable to the development of the Subject Property shall be those in effect as of the date of the approval of this Amendment No. 2 to the Agreement. This does not include any building or fire code that is state mandated (See RCW 19.27.031), impacts fees, mitigation fees, or any other fees or charges.

- E. A NEW SECTION 29, *Street and Roadway Improvements; Vesting*, is hereby added to the Agreement to read as follows:

New Section 29: *Street and Roadway Improvements; Vesting*. The parties agree to the street improvements shown in the attached Exhibit B. These administrative approvals are contingent upon approval by the Olympia City Council of this Development Agreement. These improvements differ from those previously approved in permits under the original Agreement; therefore, the Developer agreed as part of their administrative approval to submit revisions to those permits that are consistent with the attached Exhibit B. This Agreement also acknowledges the contingent administrative approval of these revisions to valid permits, which will not affect the vested rights of Developer to those permits. This Agreement simply acknowledges the vesting that is granted by permits that are not before the Olympia

City Council. This Agreement also acknowledges that the Permits were authorized to be amended in what is a City Engineer's deviation approval, because that deviation is an improvement to what the City of Olympia Community Planning and Development Department has determined to be already vested.

The parties acknowledge there is an existing ten-foot utility easement adjacent to the right-of-way on the west side of the north-south connector street (Smithfield Street) between Harrison Avenue and 7th Avenue, as shown in Exhibit B. Developer shall construct and maintain a four-foot wide hard surface suitable for pedestrian travel in the easternmost three feet of this easement immediately adjacent to the right-of-way, together with the westernmost one foot of the right-of-way. This area may be used as part of a small plaza, outdoor seating or other amenities provided for business customers and pedestrians. In combination with the four-foot sidewalk within the right-of-way, this will effectively create an eight-foot wide pedestrian walkway that will serve the adjacent businesses.

The remaining seven-foot wide portion of the utility easement may be used for landscaping and small commercial uses designed primarily to cater to pedestrians, as consistent with the terms of the easement. These uses may include, but are not limited to, vendors, newsstands, flowers, and cafes. This portion of the easement may not be used for fences or other features which form visual barriers or block views to street wall windows.

F. Section 17 of the Agreement, *Annexation & "Preferred Lease Zone"* shall be deleted.

2. All remaining provisions of the Development Agreement by and Between the City of Olympia and MPH Holdings, LLC, for the Development of Certain Property dated July 10, 2007, as modified by Amendment No. 1 dated March 31, 2009, and not here amended or

supplemented shall remain as written in said Agreement, and shall continue in full force and effect.

3. This Amendment No. 2 may be executed in a number of identical counterparts which, taken together, shall constitute collectively one agreement; but in making proof of this Amendment, it shall not be necessary to produce or account for more than one such counterpart. Additionally, (i) the signature pages taken from separate individually executed counterparts of this Amendment may be combined to form multiple fully-executed counterparts; and (ii) a facsimile signature or an electronically scanned signature, where permitted, shall be deemed to be an original signature for all purposes. All executed counterparts of this Amendment shall be deemed to be originals, but all such counterparts, when taken together, shall constitute one and the same agreement.

<b>MPH HOLDINGS, LLC:</b>	<b>CITY OF OLYMPIA:</b>
By <u>[Signature]</u>	<u>[Signature]</u>
Signature	Steven R. Hall, City Manager
Print Name <u>Shelley Hentges</u>	Date <u>6-13-2017</u>
Title <u>member</u>	
Date <u>6/12/17</u>	APPROVED AS TO FORM:
	<u>Darren Nienaber</u>
	Deputy City Attorney



## EXHIBIT "A"

### WESTCAP DESCRIPTION

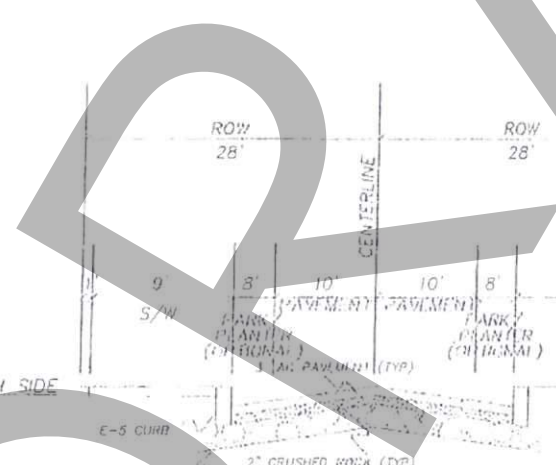
Parcel A of City of Olympia Boundary Line Adjustment No. BLA 08 0072 OL, as recorded June 26, 2008 under Auditor's File No. 4019857, records of Thurston County, Washington.  
EXCEPTING THEREFROM that portion conveyed to the City of Olympia for Harrison Avenue per Auditor's File No. 4136288, records of Thurston County, Washington.

### 7<sup>th</sup> AVENUE PROPERTY DESCRIPTION

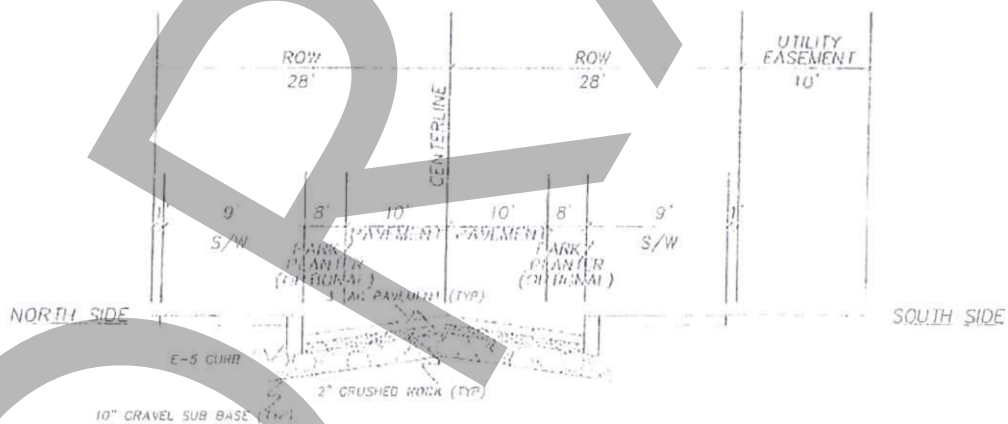
Parcel C of Boundary Line Adjustment No. BLA-0039, as recorded July 8, 1982 under Auditor's File No. 8207080043, records of Thurston County, Washington.

COMMERCIAL COLLECTOR  
NORTH-SOUTH ROAD  
SMITHFIELD ST.

COMMERCIAL COLLECTOR  
NORTH-SOUTH ROAD  
SMITHFIELD ST.



AMENDMENT NO. 2 TO DEVELOPMENT AGREEMENT BY AND BETWEEN THE CITY OF OLYMPIA AND MPH HOLDINGS, LLC, FOR THE DEVELOPMENT OF CERTAIN PROPERTY – Page 8





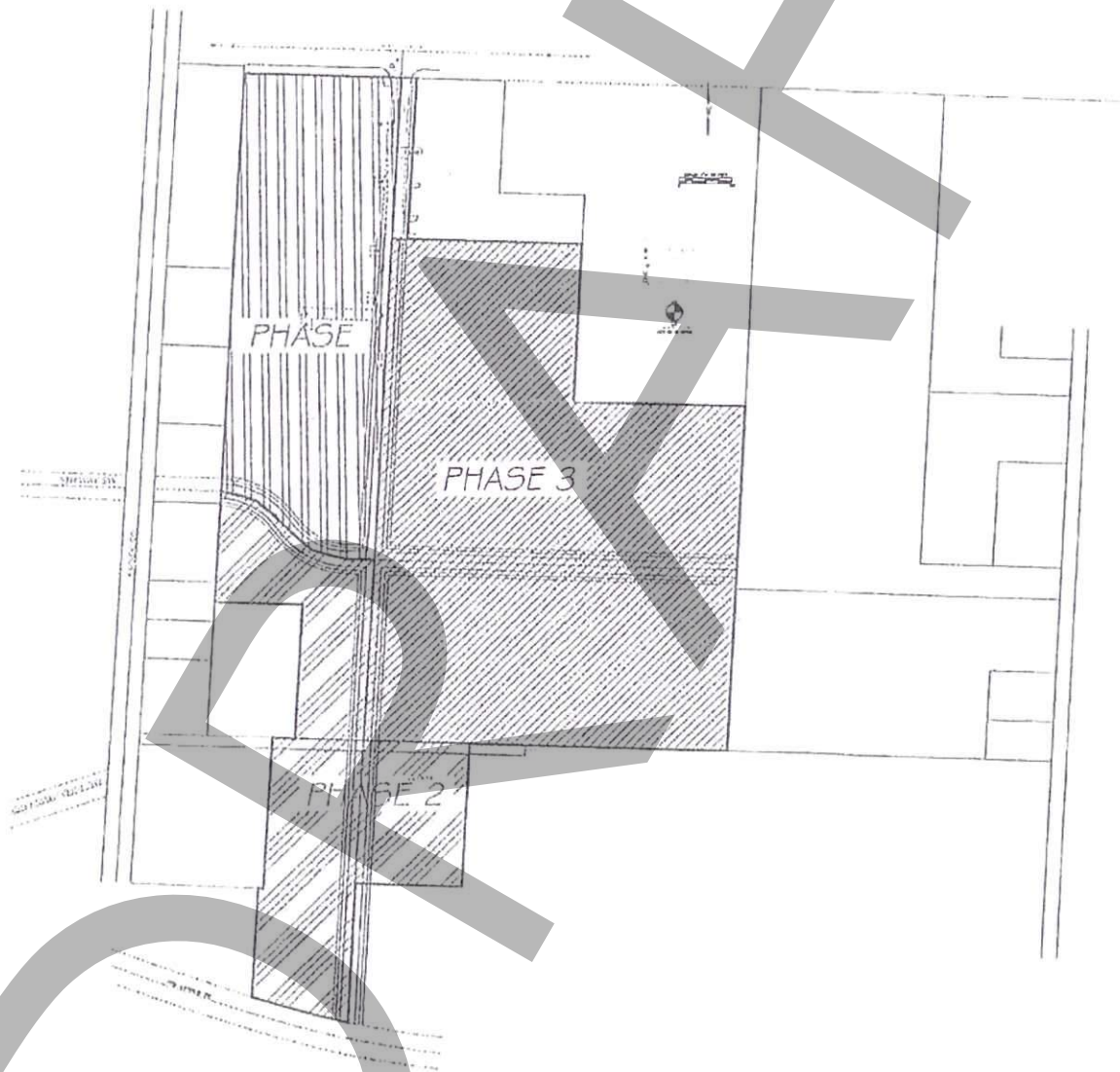


## EXHIBIT "C"

The following permits are active for your project at 4501 Harrison Avenue NW:

08-2218-ACCC-0 –	Engineering application
08-2218-ENSP-	Engineering Plat
08-2218-SEWR-0-	Sewer Main Permit
08-2218-Site-0	Engineering Site Work
08-2218-STRE-0	Street Improvements
08-2218-STRM-0	Stormwater Improvements
08-2218-WATR-0	Water Main Permit

PHASING  
EXHIBIT "D"



**DEVELOPMENT AGREEMENT  
BY AND BETWEEN THE CITY OF OLYMPIA  
AND MPH HOLDINGS, LLC, FOR THE  
DEVELOPMENT OF CERTAIN PROPERTY**

THIS DEVELOPMENT AGREEMENT is made and entered into this \_\_\_\_\_ day of \_\_\_\_\_, 2007, by and between the City of Olympia, an optional code Washington municipal corporation, hereinafter the "City," and MPH HOLDINGS, LLC, a limited liability company organized under the laws of the State of Washington, hereinafter " MPH" or "Developer."

**RECITALS**

1. The Washington State Legislature has authorized the execution of a development agreement between a local government and a person having ownership or control of real property within its jurisdiction pursuant to RCW 36.70B.170(1)).
2. Under that same law, local governments may also enter into a development agreement for real property outside its boundaries as part of a proposed annexation or service agreement. MPH and the City have discussed the commencement of annexation proceedings for the subject property.
3. A development agreement made pursuant to that authority must set forth the development standards and other provisions that shall apply to, govern, and vest the development, use, and mitigation of the development of the real property for the duration specified in the agreement.
4. For the purposes of this development agreement, "development standards" includes, but is not limited to, all of the standards listed in RCW 36.70B.170(3), except as further provided herein.
5. The City and MPH recognize development agreements must be consistent with the applicable development regulations



adopted by a local government planning under chapter 36.70A RCW.

6. This Development Agreement will be by and between the City of Olympia and the Developer (hereinafter the "Development Agreement"), relates to the development known as \_\_\_\_\_, which is located on Mud Bay Road/Harrison Avenue (hereinafter the "Property") with the Developer.

Now, therefore, the parties hereto agree as follows:

### **General Provisions**

**Section 1:** *The Project.* The Project is the development and use of the Property, consisting of approximately 19 acres adjoining the City of Olympia. The proposal describes the Project as a multi-use commercial project. It is anticipated that MPH will be seeking approval of a Binding Site Plan upon the annexation of the property.

**Section 2:** *The Subject Property.* The Project site is legally described in Exhibit A, attached hereto and incorporated herein by this reference.

**Section 3:** *Definitions.* As used in this Development Agreement, the following terms, phrases and words shall have the meanings and be interpreted as set forth in this Section.

"Adopting Ordinance" means the Ordinance which approves this Development Agreement, as required by RCW 36.70B.200.

"Certificate of occupancy" means either a certificate issued after inspections by the City authorizing a person(s) in possession of property to dwell or otherwise use a specified building or dwelling unit, or the final inspection if a formal certificate is not issued.

"Council" means the duly elected legislative body governing the City of Olympia.

"Director" means the City's Community Development Director.

"Effective Date" means the effective date of the Adopting Ordinance.

"EDDS" means the Engineering Design and Development Standards" adopted by the City of Olympia. See OMC 12.02.020.

"Existing Land Use Regulations" means the ordinances adopted by the City Council of Olympia in effect on the Effective Date, including the adopting ordinances that govern the permitted uses of land, the density and intensity of use, and the design, improvement, construction standards, and specifications applicable to the development of the Subject Property, including, but not limited to the Comprehensive Plan, the City's Official Zoning Map and development standards, SEPA, Concurrency Ordinance, and all other ordinances, codes, rules, and regulations of the City establishing standards in relation to the division of land, whether through the subdivision process, the EDDS, the binding site plan process, or otherwise, and building standards. This does not include any building or fire code that is state-mandated (See RCW 19.27.031), impact fees, mitigation fees, or any other fees or charges.

"Landowner" is the party who has acquired any portion of the Subject Property from the Developer who, unless otherwise released as provided in this Agreement, shall be subject to the applicable provisions of this Agreement. The "Developer" is MPH Holdings, LLC.

"Project" means the anticipated development of the Subject Property, as specified in Section 1 and as provided for in all associated permits/approvals, and all incorporated exhibits.

**Section 4:** *Exhibits.* Exhibits to this Agreement are as follows:

Exhibit A - legal description of the Subject Property.

Exhibit B - Map showing Development Phases, including design of improvements consistent with current requirements of the City. This development agreement does not certify or pre-approve exhibit B as a binding site plan or for any other land use review or permits.

**Section 5:** *Parties to Development Agreement.* The parties to this Agreement are:



The "City" is the City of Olympia, the mailing address of which is P. O. Box 1967, Olympia, Washington 98507.

As indicated above, the "Developer" or Owner is a private enterprise which owns the Subject Property, and whose mailing address is Post Office Box 11221, Olympia, Washington 98508.

The "Landowner." From time to time, as provided in this Agreement, the Developer may sell or otherwise lawfully dispose of a portion of the Subject Property to a Landowner who, unless otherwise released, shall be subject to the applicable provisions of this Agreement related to such portion of the Subject Property.

**Section 6:** *Project is a Private Undertaking.* It is agreed among the parties that the Project is a private development and that the City has no interest therein except as authorized in the exercise of its governmental functions.

**Section 7:** *Term of Agreement.* This Agreement shall commence upon the effective date of the Adopting Ordinance approving this Agreement, and shall continue in force for a period of 6 years from the date of permit issuance not to exceed 8 years in total, unless extended or terminated as provided herein. Following the expiration of the term or any extension thereof, or if sooner terminated, this Agreement shall have no force and effect, subject however, to post-termination obligations of the Developer or Landowner.

**Section 8:** *Vested Rights of Developer.* During the term of this Agreement, unless sooner terminated in accordance with the terms hereof, in developing the Subject Property consistent with the Project described herein, Developer is assured, and the City agrees, that the development rights, obligations, terms and conditions specified in this Agreement, are fully vested in the Developer under the existing land use regulations and may not be changed or modified by the City, except as may be expressly permitted by, and in accordance with, the terms and conditions of this Agreement, including the Exhibits hereto, or as expressly consented thereto by the Developer. This does not include any building or fire code that is state-mandated (See RCW 19.27.031), impact fees, mitigation fees, or any other fees or charges.



**Section 9:** *Permitted Uses, Development Standards, & Phasing.* Whether developed in one phase or a series of phases as anticipated by Section 13, (a) the permitted uses, (b) the density and intensity of use, (c) the maximum height and size of proposed buildings, (d) provisions for reservation and dedication of land, (e) as noted above, the existing Land Use Regulations relating to among other items, the construction, installation and extension of public improvements, (f) the EDDS, and (G) development guidelines and standards for and applicable to the development of the Subject Property shall be those in effect as of the date of this Agreement, whether set forth in this Agreement, or in the permits and approvals, if any, identified herein, and all exhibits incorporated herein. This does not include any building or fire code that is state-mandated (See RCW 19.27.031), impact fees, mitigation fees, or any other fees or charges.

In addition, Developer hereby covenants and agrees that the development area shall not be used for single family or multifamily, except that mixed used buildings that have a residential component (rental, condo or other type of residential) are allowed. Such covenant shall run with the land and be binding on Developer's successors and assigns for the term of this agreement.

**Section 10:** *Modifications.* Any modifications from the approved permits or the exhibits attached hereto requested by MPH may be approved in accordance with the provisions of the City's code and under the existing Land Use Regulations, and shall not require an amendment to this Agreement.

**Section 11:** *Financing of Public Facilities.*

11.1. Developer acknowledges and agrees that it shall participate in the funding of its pro-rata share of the costs of public improvements to be financed thereby, in accordance with the provisions of this Agreement and SEPA. At the request of the Developer, the City shall consider the use of a local improvement district and other similar project-related public financing mechanisms for financing the construction, improvement, or acquisition of public infrastructure, facilities, lands and improvements to serve the Subject Property, whether located within or outside the Subject Property.

11.2. The City shall consider a latecomer's agreement when proposed by the Developer.

**Section 12:** *Existing Land Use Fees and Impact Fees.*

Land use fees and impact fees adopted by the City by ordinance as of the Effective Date of this Agreement may be increased by the City, and applicable to permits and approvals for the Subject Property, as long as such fees apply to similar applications and projects in the City.

All impact fees shall be paid as set forth in the approved permit or approval, or as addressed in the Olympia Municipal Code.

**Section 13:** *Phasing of Development.*

The parties acknowledge that the most efficient and economic development of the Subject Property depends upon numerous factors, such as market orientation and demand, interest rates, competition and similar factors, and that generally it will be most economically beneficial to the ultimate purchasers of the Subject Property to have the rate of development determined by the Developer. However, the parties also acknowledge that, because the Development will be phased, certain amenities associated with the Project must be available to all phases of the Project, in order to address health, safety and welfare of the residents. Therefore, the parties agree that the improvements associated with the Project shall be constructed and developed according to the design guidelines set forth upon Exhibit B, whether the development and construction is carried out in one phase or a series of phases as anticipated by Section 4 and this section.

**Section 14:** *Dedication of Public Lands.*

Except as otherwise provided herein, the Developer shall dedicate all public lands required in the permits/approvals. Rights-Of-Way shall be dedicated to the City concurrently with final plat approval by the City for any phase of the development. As to such dedications, the Developer agrees to dedicate any or all road rights-of-way without expense to the City.

**Section 15:** *Default.*

Subject to extensions of time by mutual consent in writing, failure or delay by either party or Landowner not released from this Agreement, to perform any term or provision of this Agreement shall constitute a default. In the event of alleged default or breach of any



terms or conditions of this Agreement, the party alleging such default or breach shall give the other party or Landowner not less than thirty (30) days notice in writing, specifying the nature of the alleged default and the manner in which said default may be cured. During this thirty (30) day period, the party or Landowner charged shall not be considered in default for purposes of termination or institution of legal proceedings.

After notice and expiration of the thirty (30) day period, if such default has not been cured or is not being diligently cured in the manner set forth in the notice, the other party or Landowner to this Agreement may, at its option, institute legal proceedings pursuant to this Agreement. In addition, the City may decide to file an action to enforce the City's Codes, and to obtain penalties and costs as provided in the Olympia Municipal Code or state law for violations of this Development Agreement and the Code.

**Section 16: Termination.** This Agreement shall expire and/or terminate as provided below:

16.1. This Agreement shall expire and be of no further force and effect if the development contemplated in this Agreement and all of the permits and/or approvals issued by the City for such development are not substantially underway prior to expiration of such permits and/or approvals. Nothing in this Agreement shall extend the expiration date of any permit or approval issued by the City for any development.

16.2. This Agreement shall expire and be of no further force and effect if the Developer does not construct the Project substantially as contemplated by the design documents identified in this Agreement, and submits applications for development of the Property that are inconsistent with such permits, approvals and with this agreement.

16.3. This Agreement shall terminate upon the expiration of the term identified in Section 7 or when the Subject Property has been fully developed, whichever first occurs, and all of the Developer's obligations in connection therewith are satisfied as determined by the City. Upon termination of this Agreement, the City shall record a notice of such termination in a form satisfactory to the City Attorney that the Agreement has been terminated.



16.4. At the Option of the Developer, this Agreement shall terminate in the event that the property covered by the Agreement is not annexed into the City of Olympia by the day of \_\_\_\_\_, 2008, upon the Developer giving the City written notice of its desire to so terminate.

16.5. If not earlier terminated, it shall terminate as provided upon the passage of the time periods set forth in §16.

**Section 17:** *Annexation & "Preferred Lease Zone" Provisions*

17.1. Pursuant to City sponsorship, the process to annex the area of which the property is a part has been commenced. The City agrees that it shall timely carry forth the procedures in relation to the consideration of the annexation of the property covered by this Agreement.

17.2. The City shall review the matter of recommending to Division of Real Estate Services of the State Department of General Administration that the property be included within the "Preferred Lease Zone" area maintained by the Division.

17.3. In the event that the Developer submits a request to annex the property, the City agrees that it shall process such request in a timely manner.

**Section 18:** *Effect upon Termination on Developer Obligations.* Termination of this Agreement as to the Developer of the Subject Property or any portion thereof shall not affect any of the Developer's obligations to comply with the City Comprehensive Plan and the terms and conditions or any applicable zoning code(s) or subdivision map or other land use entitlements approved with respect to the Subject Property, any other conditions of any other development specified in the Agreement to continue after the termination of this Agreement or obligations to pay assessments, liens, fees or taxes.

**Section 19:** *Effects of Termination on City.* Upon any termination of this Agreement as to the Developer of the Subject Property, or any portion thereof, the entitlements, conditions of development, limitations on fees and all other

terms and conditions of this Agreement shall no longer be vested hereby with respect to the property affected by such termination.

**Section 20:** *Assignment and Assumption.* The Developer shall have the right to sell, assign, or transfer this Agreement with all their rights, title, and interests therein to any person, firm or corporation at any time during the term of this Agreement.

**Section 21:** *Covenants Running with the Land.* The conditions and covenants set forth in this Agreement and incorporated herein by the Exhibits shall run with the land and the benefits and burdens shall bind and inure to the benefit of the parties. The Developer, Landowner and every purchaser, assignee or transferee of an interest in the Subject Property, or any portion thereof, shall be obligated and bound by the terms and conditions of this Agreement, and shall be the beneficiary thereof and a party thereto, but only with respect to the Subject Property, or such portion thereof, sold, assigned or transferred to it. Any such purchaser, assignee or transferee shall observe and fully perform all of the duties and obligations of a Developer contained in this Agreement, as such duties and obligations pertain to the portion of the Subject Property sold, assigned or transferred to it.

**Section 22:** *Amendment to Agreement; Effect of Agreement on Future Actions.*

22.1. This Agreement may be amended by mutual consent of all of the parties, provided that any such amendment shall follow the process established by law for the adoption of a development agreement.

22.2. Nothing in this Agreement shall prevent the City Council from making any amendment to its Comprehensive Plan, Zoning Code, Official Zoning Map or development regulations affecting the Subject Property during term of this agreement to the extent required by a serious threat to public health and safety.

22.3. So long as mutually agreed upon, nothing in this Development Agreement shall prevent the City Council from making any amendments of any type to the Comprehensive Plan, Zoning Code, Official Zoning Map or development regulations



relating to the Subject Property upon bases other than those set out in 22.2. In the absence of such mutual agreement by the Parties, any such amendment may not become effective earlier than the termination date of this agreement.

**Section 23:** Releases. Developer, and any subsequent Landowner, may free itself from further obligations relating to the sold, assigned, or transferred property, provided that the buyer, assignee or transferee expressly assumes the obligations under this Agreement as provided herein.

**Section 24:** Notices. Notices, demands, correspondence to the City and Developer shall be sufficiently given if dispatched by pre-paid first-class mail to the addresses of the parties as designated in Section 5. Notice to the City shall be to the attention of both the City Manager and the Director of Community Planning and Development. Notices to subsequent Landowners shall be required to be given by the City only for those Landowners who have given the City written notice of their address for such notice. The parties hereto may, from time to time, advise the other of new addresses for such notices, demands or correspondence.

**Section 25:** Applicable Law and Attorneys' Fees. This Agreement shall be construed and enforced in accordance with the laws of the State of Washington. Venue for any action shall lie in Thurston Superior Court or the U.S. District Court for Western Washington.

**Section 26:** Third Party Legal Challenge. In the event any legal action or special proceeding is commenced by any person or entity other than a party or a Landowner to challenge this Agreement or any provision herein, the City may elect to tender the defense of such lawsuit or individual claims in the lawsuit to Developer and/or Landowner(s). In such event, Developer and/or such Landowners shall hold the City harmless from and defend the City from all costs and expenses incurred in the defense of such lawsuit or individual claims in the lawsuit, including but not limited to, attorneys' fees and expenses of litigation, and damages awarded to the prevailing party or parties in such litigation. The Developer and/or Landowner shall not settle any lawsuit without the consent of the City. The City shall act in good faith and shall not unreasonably withhold consent to settle.



**Section 27: Specific Performance.** The parties specifically agree that damages are not an adequate remedy for breach of this Agreement, and that the parties are entitled to compel specific performance of all material terms of this Development Agreement by any party in default hereof.

**Section 28: Severability.** If any phrase, provision or section of this Agreement is determined by a court of competent jurisdiction to be invalid or unenforceable, or if any provision of this Agreement is rendered invalid or unenforceable according to the terms of any statute of the State of Washington which became effective after the effective date of the ordinance adopting this Development Agreement, and either party in good faith determines that such provision or provisions are material to its entering into this Agreement, that party may elect to terminate this Agreement as to all of its obligations remaining unperformed.

IN WITNESS WHEREOF, the parties hereto have caused this Development Agreement to be executed as of the dates set forth below:

**MPH HOLDINGS, LLC:**

By \_\_\_\_\_  
Its Managing Member

**CITY OF OLYMPIA:**

By Mark Runtel  
Its Mayor

ATTEST:

By Debbie Gruempel  
City Clerk

APPROVED AS TO FORM:

By Darren Menche-ACA  
City Attorney

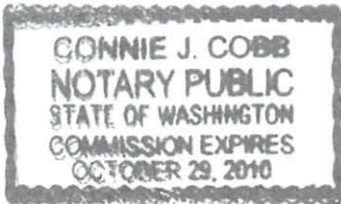
STATE OF WASHINGTON )

COUNTY OF THURSTON ) : ss.

I certify that I know or have satisfactory evidence that Mark Foutch ~~and~~ \_\_\_\_\_ signed this instrument, on oath stated that they were authorized to execute the instrument and acknowledged it as the Mayor ~~and Clerk-treasurer, respectively,~~ of the City of Olympia, to be the free and voluntary act of such party for the uses and purposes mentioned in the instrument.

DATED:

July 11, 2007



Connie J. Cobb

NOTARY PUBLIC IN AND FOR THE STATE  
OF WASHINGTON, Residing at: Olympia  
My appointment expires: 10/29/10

STATE OF WASHINGTON )

: ss.

COUNTY OF THURSTON )

I certify that I know or have satisfactory evidence that James A. Morris signed this instrument, on oath stated that he was authorized to execute the instrument and acknowledged it as the Managing Member of MPH Holdings, LLC, to be the free and voluntary act of such party for the uses and purposes mentioned in the instrument.

DATED: \_\_\_\_\_

\_\_\_\_\_  
NOTARY PUBLIC IN AND FOR THE STATE  
OF WASHINGTON, Residing at:  
My appointment expires:

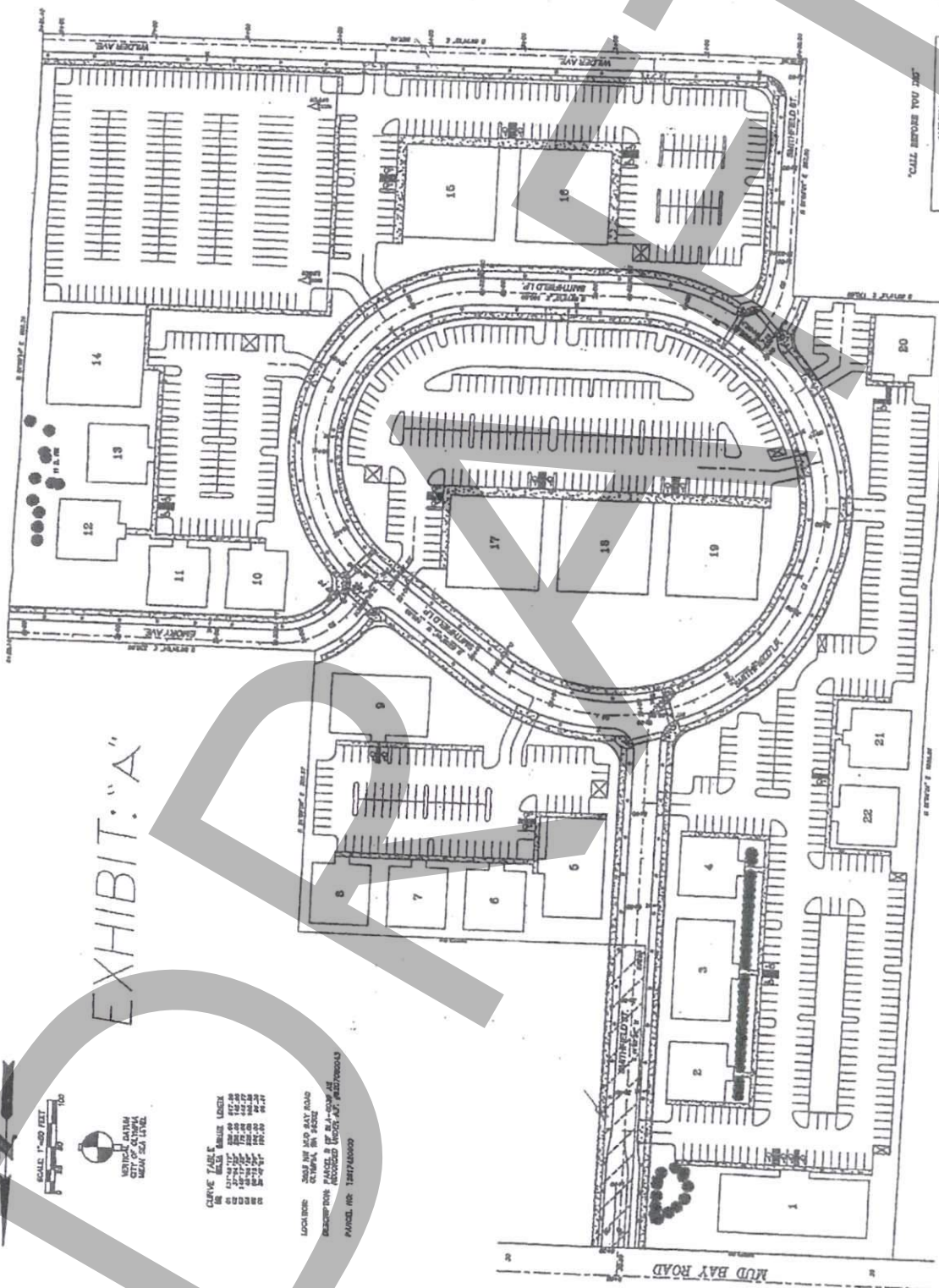


EXHIBIT: "A"



VERTICAL DATUM  
CITY OF CLEVELAND  
MEAN SEA LEVEL

INCH	MILLI-METER	MINUTE	LENGTH
01	5.21	48" 11"	238.40
02	5.37	54" 12"	250.00
03	5.48	57" 12"	260.00
04	5.63	60" 12"	270.00
05	5.80	63" 12"	280.00
06	5.97	66" 12"	290.00
07	6.13	69" 12"	300.00
08	6.30	72" 12"	310.00
09	6.46	75" 12"	320.00
10	6.63	78" 12"	330.00
11	6.79	81" 12"	340.00
12	6.96	84" 12"	350.00
13	7.12	87" 12"	360.00
14	7.29	90" 12"	370.00
15	7.45	93" 12"	380.00
16	7.62	96" 12"	390.00
17	7.78	99" 12"	400.00
18	7.95	102" 12"	410.00
19	8.11	105" 12"	420.00
20	8.28	108" 12"	430.00
21	8.44	111" 12"	440.00
22	8.61	114" 12"	450.00
23	8.77	117" 12"	460.00
24	8.94	120" 12"	470.00
25	9.10	123" 12"	480.00
26	9.27	126" 12"	490.00
27	9.43	129" 12"	500.00
28	9.60	132" 12"	510.00
29	9.76	135" 12"	520.00
30	9.93	138" 12"	530.00
31	10.09	141" 12"	540.00
32	10.26	144" 12"	550.00
33	10.42	147" 12"	560.00
34	10.59	150" 12"	570.00
35	10.75	153" 12"	580.00
36	10.92	156" 12"	590.00
37	11.08	159" 12"	600.00
38	11.25	162" 12"	610.00
39	11.41	165" 12"	620.00
40	11.58	168" 12"	630.00
41	11.74	171" 12"	640.00
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43	12.07	177" 12"	660.00
44	12.24	180" 12"	670.00
45	12.40	183" 12"	680.00
46	12.57	186" 12"	690.00
47	12.73	189" 12"	700.00
48	12.90	192" 12"	710.00
49	13.06	195" 12"	720.00
50	13.23	198" 12"	730.00
51	13.39	201" 12"	740.00
52	13.56	204" 12"	750.00
53	13.72	207" 12"	760.00
54	13.89	210" 12"	770.00
55	14.05	213" 12"	780.00
56	14.22	216" 12"	790.00
57	14.38	219" 12"	800.00
58	14.55	222" 12"	810.00
59	14.71	225" 12"	820.00
60	14.88	228" 12"	830.00
61	15.04	231" 12"	840.00
62	15.21	234" 12"	850.00
63	15.37	237" 12"	860.00
64	15.54	240" 12"	870.00
65	15.70	243" 12"	880.00
66	15.87	246" 12"	890.00
67	16.03	249" 12"	900.00
68	16.20	252" 12"	910.00
69	16.36	255" 12"	920.00
70	16.53	258" 12"	930.00
71	16.69	261" 12"	940.00
72	16.86	264" 12"	950.00
73	17.02	267" 12"	960.00
74	17.19	270" 12"	970.00
75	17.35	273" 12"	980.00
76	17.52	276" 12"	990.00
77	17.68	279" 12"	1000.00
78	17.85	282" 12"	1010.00
79	18.01	285" 12"	1020.00
80	18.18	288" 12"	1030.00</

LOCK BOX: 3505 N. MIDWAY ROAD  
OLYMPIA, WA 98502

DELIVERED FROM: PACEL 8 OF 8-A-0038 AS  
NOTICED UNDER AF. 98257000-03

PACEL NO: 781748000





# CONSULTANT REPORTS: MEP

Here is our report of our review of the mechanical and electrical systems for the Thurston County Courthouse.

## **MECHANICAL**

### **Air Handling Units**

The building is served by a variety of HVAC units. The equipment includes:

- Multiple rooftop packaged heat pumps
- Indoor multizone units, with hydronic heating and cooling
- Multiple rooftop HVAC units with hydronic heating and cooling
- Rooftop hydronic heating and cooling dual duct units
- Rooftop heat recovery units with hydronic heating and cooling
- Indoor heat recovery units with hydronic heating and cooling
- Kitchen Make-up Air Unit with hydronic heating



**Figure 1 Typical Rooftop HVAC Equipment**

The units typically match the ages of the various building additions/revisions; except that a couple of Building 3 units were replaced as part of a reroof and HVAC upgrade project in 2006. The other existing equipment is past its useful life, has poor energy efficiency, has insufficient access for routine maintenance and should be scheduled for immediate replacement.

Several of the existing air handling units and associated ductwork are no longer allowed by the Washington State Energy Code. A significant remodel would trigger the requirement to replace the existing systems with code allowed systems; air handling units, terminal units, and distribution piping and ductwork would have to be replaced.



## Hydronic System

The hydronic system is a hot water heating type, using three central gas fired boilers, located in the basement of Building 3. The units were manufactured by De Dietrich in 2002. Each has 1895 MBH input capacity. The boilers appear to be in good condition. These are considered low efficiency boilers as compared to new condensing type. The majority of the time, only one boiler operates at a time.

The piping system is a mix of plastic pipe and black steel or copper above the roof of Building 3. Because the majority of the piping is plastic, the temperature of the heating hot water is limited to 120 deg F; air handling unit coils were sized based on higher temperatures.

Since the original low-efficiency light fixtures were removed, the reduced heat gain from the lights has resulted in some zones that are incapable of maintaining the heating setpoint.



**Figure 2 Gas Fired Boiler**

## HVAC Controls

The existing controls are a combination of original pneumatics, some Direct Digital Control (DDC) type, and various stand-alone systems. Boilers, chiller/cooling tower and pumps controls are DDC. Most air handling units are also DDC but some were converted to stand alone as part of various remodels. Terminal units are primarily pneumatic.

Existing controls are unable to provide consistent zone temperature control resulting in hot and cold spots throughout the buildings.

## Plumbing Piping

The domestic water piping mains to each building are of galvanized steel; distribution piping is a mix of galvanized and copper, and is in poor condition. Pipes have been patched in multiple locations. Water pressure is poor, and occupants will not drink the water.

Drainage piping is sluggish and susceptible to clogs.

### **Water Heaters**

Domestic hot water is generated by tank-type electric water heater in each building. Circulation pumps maintain hot water circulation through the building domestic hot water system. Water heater capacities provide adequate hot water. Water heaters in Buildings 1 and 2 are in fair condition; the water heaters in Building 3 are aged and in poor condition.



**Figure 4 Bldg 3 Water Heater and Circulation Pumps**

### **Plumbing Fixtures**

Many are original to the building construction. Several faucets and flush valves have failed. The existing fixtures do not have any of the water efficiency devices of modern fixtures (i.e. low flow urinal flush valves, dual flush water closet flush valves, low flow faucet aerators).

### **Fire Suppression**

All three buildings are currently fire sprinklered. According to a 2016 survey report, the sprinkler system does not extend into all detention areas of Building 3 and many of the heads in detention areas are not the detention type.

## **ELECTRICAL**

### **Power Distribution**

The main distribution system installed in 1977 feeding Buildings 1, 2 & 3 is currently located in the basement of Building 3. This is a primary feed service from the utility with a single meter and single disconnect located on the high voltage system. This feeds three 1,000 KVA (3 MVA) transformers connected in parallel to feed the 480/277V 4,000 amp main complex switchboard MDS. The 480V

system branches out to other panels and transformers located within Building 3, and to main switchboards located in Buildings 1 & 2.

Another 500KVA utility transformer feeds the 208-120V distribution switchboard in Building 3.

Building 1 & 2 distribution switchboards (1,000 amps & 1,200 amps respectively) located in the buildings electrical rooms are fed from the switchboard located in Building 3. This is a 480V system with step down transformers to 208-120V systems in these buildings.

The way the current distribution system is configured, if there were any problems with the electrical system in Building 3, this has the potential for Buildings 1 & 2 to lose power.

High voltage cables to the campus are currently being replaced with new by PSE.

The entire complex distribution system is original and is at the end of its life expectancy. Panels are at capacity with little room to add additional circuits. It is recommended to replace the entire system with a more reliable distribution system with independent power to each building.

### **Generator**

The emergency generator is currently 500KW and was replaced 15 years ago with a used Cummins surplus generator. This generator supports mainly Building 3 with connections to mechanical equipment, lighting, power and refrigeration. There is a feed to Building 1 with connections to lights, plugs and data equipment, and a manual transfer switch to connect the data room cooling systems. A feed from Building 3 to Building 2 is limited to a few light fixtures and a couple outlets.

Basically, the generator is to cover the jail with little back-up power to Buildings 1 & 2. This lack of emergency system covering the campus became evident with a utility power outage experienced October 2018, when Buildings 1 & 2 were basically left without power. Building 3 was able to utilize the generator completely but with most of this building being unoccupied, the generator was virtually useless.

The distribution system for the back-up generator does not appear to be wired to meet current code requirements, and any further modifications or additions to this system could lead to a correction notice being issued on the buildings.

The entire emergency generator system at this campus needs to be revised in a manner so the buildings may still remain functional when there is another loss of power.

### **Lighting**

The interior lighting of Buildings 1, 2 & 3 consists of fluorescent fixtures. There appears to have been upgrades from T12 lamps to more efficient T8 lamps in the buildings approximately 8 years ago. The lighting appears adequate for when it was installed. New LED fixtures with higher efficacy and full spectrum output that would not only make the lighting more efficient and reduce maintenance and operating costs but would improve uniformity to eliminate the dark and bright contrasts seen in the buildings.

In 1996 there was an emergency lighting and power upgrade. It is uncertain if this was an attempt to install egress lighting, or stand-by lighting. If this is egress lighting it does not meet current building codes to provide the required lighting along paths of egress. All egress and exit signage needs to be brought up to current code requirements.

Exterior lighting appears to have been selectively upgraded with new LED, but the lighting was not



upgraded to provide full coverage. There are areas that are still dark and/or suffering from poor lighting conditions along pathways and entrances. No exterior emergency egress lighting appears to be present.

Energy efficient controls, required by current code, do not appear to be present throughout the three buildings. Lighting controls should be replaced with new to meet current energy code requirements and increase energy efficiency and use, while allowing occupants better lighting control of their working environment.

### **General Wiring**

Receptacles are standard 20A polarized straight blade, and switches appear to be standard 20A commercial grade.

It appears the electrical branch wiring throughout the building is copper and feeders are aluminum conductors installed in metallic raceway with additions of metallic clad (MC) cabling during upgrades.

Receptacles and switches have reached the end of their life span with loose contacts and broken faces. These should be replaced with new to provide assured contact to plugs and tensions to retain pugs tightly in the receptacles. Switches should be replaced with new in combination with energy efficient controls described above.

### **Fire Alarm System**

The fire alarm system appears to have been upgraded in 2006 with an addressable system currently monitored by a listed monitoring agency. The buildings are sprinklered so detection devices are not required like buildings without fire suppression. Notification devices like horns and strobes should be reviewed to provide proper coverage.

### **Intrusion System**

An intrusion system does not exist on campus.

The Duress system available in all three buildings reports to the sheriff office.

Security Cameras in the assessors and Auditors are independent from the rest of the campus. The remaining portions of the buildings are lacking in coverage.

### **Access Control**

Access control is limited to Exterior doors at the three buildings with some interior doors having card key access control.

### **Telecommunications**

New voice over Internet Protocol (IP) at the 3 buildings appears to be in good operation at this time with regular phone lines being used in various locations.

Building 3 has a Distributed Antenna System (DAS), however this does not appear to be present in Buildings 1 & 2. A DAS system is used for emergency responders to be able to communicate to the exterior of the building. New communications use higher frequencies that buildings are now blocking. It is recommended that a study be completed in all 3 buildings and based on the results, upgrade building 3 as required, and provide new systems in Buildings 1 & 2 as required.

Data cabling was upgraded to Cat 6 within the 3 buildings along with the distribution frames. With modernization this has reduced the heat load and power requirements previously required.

Fiber Optic cabling is run around the campus, however it is lacking redundancy. Redundant cabling should be run for both the private network and cloud based system to prevent loss of connectivity to offsite locations. Private secure fiber optic pairs can be furnished by WSDOT for \$800 a month and shared installation costs where WSDOT does not currently have fiber optic to extend the cabling to the site.

Below are the recommended mechanical and electrical systems to correspond with each of the three options for the Thurston County Courthouse Building.

## **MECHANICAL**

### **Option 1**

#### **HVAC System**

We would recommend the use of a central hydronic plant and central air handlers, with a variable-air-volume (VAV) air distribution system. The central plant would utilize multiple high efficiency gas fired boilers, generating heating hot water. Cooling would be by central water cooled chillers, rotary screw type, with rooftop (or at grade) cooling towers.

The air handlers would consist of multiple units, with variable speed supply and return fans. Ductwork would be constructed of galvanized steel, serving VAV terminal units. The VAV terminal units would be the shut-off type with hot water heating coils. The hydronic system would use steel and copper piping, with variable speed pumps.

The controls would be the Direct Digital Control (DDC) type, with system graphics, and energy metering. Conference rooms, Court Rooms and similar densely occupied areas would have CO2 demand ventilation controls.

#### **Plumbing System**

Waste and vent piping systems would use cast iron piping; domestic water piping would utilize copper with soldered or mechanically coupled joints. Valves would be the ball type, bronze construction.

Domestic hot water would be generated by multiple gas fired water heaters. The system would be circulated with pumps located at the water heaters.

A gray water system consisting of roof drains, collection tanks and pumps could collect and treat rain water for reuse in flushing water closets and urinals. In addition to reducing water usage, this would reduce the volume of storm run-off that would have to be treated. A similar system is operating at the County's Tilley Road campus.

Plumbing fixtures would be the vitreous china type. Water closets and urinals would have automatic sensor operated flush valves; lavatories would have sensor operated faucets with tempered water.

#### **Fire Suppression**

The building would be fully fire sprinklered, per NFPA 13 and local requirements. A fire pump may be needed due depending upon the height of the building. IT areas could utilize pre-action or FM200 chemical suppression systems.



## **Option 2**

Systems would be the same as described in Option 1, but with these changes:

### **Option 2 HVAC System**

For an ultra-high efficiency option, we recommend water-to-water heat recovery chillers connected to a geothermal water loop. The heat recovery chillers simultaneously produce chilled water for cooling and hot water for heating during mild seasons; during summer/winter months it rejects heat to the geothermal loop (when in the cooling mode), or pulls heat out of this loop (when in the heating mode). This system also allows rejected heat from areas requiring cooling to be recovered and used by other areas requiring heat. The geothermal loop consists of a series of geothermal wells. A similar system is operating at the County's Tilley Road campus.

Similar to Option 1, air handlers with variable speed supply and return fans would deliver treated air to VAV terminal units with hot water heating coils. The hydronic system would use steel and copper piping, with variable speed pumps.

The controls would be the Direct Digital Control (DDC) type, with system graphics, and energy metering. Conference rooms, Court Rooms and similar densely occupied areas would have CO2 demand ventilation controls.

## **ELECTRICAL**

### **Electrical System Upgrade Options**

1. Egress & Exit lighting – Utilization of low voltage power supplies to separate egress and exit fixtures. This will eliminate batteries and walking around to each device to test monthly. This may be performed automatically at one location with printer readout to give to the fire marshal when they visit.
2. Luminaire level lighting control to provide addressable devices to report operation and power consumption, data monitoring, and individual lighting control from the user's desk top. This will provide up to an additional 50% savings beyond replacing existing fixtures to LED fixtures
3. Energy management control for power usage monitoring and utilization. This is useful for monitoring for possible failures of equipment due to increased power consumption.
4. Photovoltaic (Solar Panels) systems for reducing utility power consumption, and charging electric vehicles.
5. Wind Turbines for creating additional clean energy in conjunction with photovoltaic systems. Sun shines during the day, winds blow at night.
6. Amorphous Silicon Photovoltaic Glass: Capitalizing on large amounts of glazing that could also be used to create electricity. This is a rather expensive option used primarily for LEED points.
7. EV Chargers for charging electric vehicles.

# CONSULTANT REPORTS: STRUCTURAL

**November 30, 2018**

**STRUCTURAL DESIGN NARRATIVE  
THURSTON COUNTY COURTHOUSE AND CIVIC CENTER  
OLYMPIA, WASHINGTON**

**DESCRIPTION OF PROJECT**

The new proposed Thurston County Courthouse and Civic Center is located in Olympia, Washington. There are nine (9) options for the construction of the new facility. All options include new construction or a mix of new and existing construction. A summary of the various options is as follows:

**Hilltop Campus Option 1:**

Current conceptual plans include replacing two existing buildings for Administrative offices and constructing a new parking structure to serve those buildings. Additionally, a new Courthouse building and associated parking structure would also be built.

**Hilltop Campus Option 2:**

Plans for this option include two separate rectangular shaped buildings for Administration and Courthouses connected with a lobby. Additionally, a connected parking structure would be built to serve both buildings.

**Hilltop Campus Option3:**

Option three includes two connected curved buildings for Administration and Courthouses. The parking structures serving these facilities would not be physically connected.

**Hilltop Campus Option 4:**

Concept plans include replacing two existing buildings for Administrative offices and placing a new Courthouse building to connect the existing buildings. The parking structure will adjoin and serve the Courthouse and Administration offices.

**Hilltop Campus Option 5:**

This option places Administrative offices and Courthouse buildings in a wedge shape connecting at a shared lobby space. Parking structures will be placed nearby and an internal drop off will serve both Courthouse and Administration buildings.

**Plum Street Option 1:**

Plans for this option place Administrative offices and Courthouse buildings end to end and connecting them at a shared lobby space. Parking structures will be placed nearby and will serve both Courthouse and Administration buildings.



## **STRUCTURAL DESIGN NARRATIVE THURSTON COUNTY COURTHOUSE AND CIVIC CENTER**

### **Plum Street Option 2:**

Concept plans for this option place Administrative offices and Courthouse buildings side by side length-wise and connecting them at a shared lobby space. Parking structures will be placed nearby and will serve both Courthouse and Administration buildings.

### **Harrison Avenue Option 1:**

This option places Administrative offices and Courthouse buildings side by side length-wise and connecting them at a shared lobby space. Site parking will be nearby and will serve both Courthouse and Administration buildings.

### **Harrison Avenue Option 2:**

Plans for this option place L-shaped Administrative offices and Courthouse buildings side by side and connecting them at a shared lobby space. Parking structures and surface parking will be placed nearby and will serve both Courthouse and Administration buildings.

## **DESIGN CRITERIA**

Code: 2018 International Building Code

### **Roof Loads:**

- Dead Load (Steel Framed Roof) – 20psf
- Dead Load (Steel Frame with Concrete Topping) – 80psf
- Live Load – 25psf

### **Floor Loads:**

- Dead Load (Steel Frame with Concrete Topping) – 80psf
- Live Load:
  - Office – 40psf + Partitions
  - Fixed Seat Assembly – 60psf
  - Public Spaces/Lobbies and Corridors Serving Them – 100psf
  - Stairs – 100psf
  - Mechanical Floors – 40psf + Mechanical Equip; Weight and Housekeeping Pads

### **Site/Soil Properties (Assumed):**

- Soil Bearing Pressure – 2,000 psf + 1/3 increase for seismic loading
- Site Class “D”
- Retaining Walls:
  - Active Pressure:  $35\text{pcf} + 15.5H^2$  (seismic)
  - Restrained Pressure:  $55\text{pcf} + 15.5H^2$  (seismic)
- Passive Pressure: 250pcf (neglecting top 1')
- Coefficient of Friction: 0.32

## **STRUCTURAL DESIGN NARRATIVE THURSTON COUNTY COURTHOUSE AND CIVIC CENTER**

### Seismic Design:

- $S_S=1.576$ ,  $S_1=0.607$ ,  $S_{DS}=1.051$ ,  $S_{D1}=0.607$
- Importance Factor,  $I=1.25$
- Response Modification Coefficient,  $R=5$  (Special Reinforced Concrete Shear Walls – Bearing Wall System),  $R=8$  (Special Steel Moment Frame)

### Wind Design:

- Basic Wind Speed: 115mph
- Exposure Category: “B”

## **FOUNDATION SYSTEM**

The foundations will be traditional continuous and spread concrete footings for most sites. The plum street site is in an area of weak soil and may require pile foundations. Typically, the tops of the footings/pile caps will be located 1'-6" below the lower level finish floor elevation. Minimum size for strip footings will be 2'-0" at exterior non-bearing walls and wider at basement walls, lateral elements, and bearing walls. We assume that the exterior footings will be supporting non-bearing metal stud framing.

The ground floor will be conventional 5" concrete slabs on grade reinforced with #4 reinforcing at 16" on center each way. A shrinkage reducing admixture and other measures should be utilized to help prevent cracking in areas where architecturally exposed concrete is required.

## **FLOOR FRAMING SYSTEM**

All elevated floors will be constructed with a composite steel floor system, utilizing steel beams (spaced between 7'-0" o.c. and 10'-0" o.c. typically) and steel girders. It is estimated that 2" steel deck and concrete topping totaling 5½" in thickness (including metal deck) will be provided at all levels. Steel wide flange columns will be used to support the framing.

## **ROOF FRAMING SYSTEM**

Typically the roof framing will utilize structural steel joist framing (W14x or W12x spaced approximately 7'-6" o.c. to 10'-0" o.c.) supporting 1½" to 3" deep metal decking. Structural steel girders and columns will be used to support the gravity loads.

## **OPTIONAL FRAMING SCHEME**

It is possible to utilize a mild reinforced concrete or post-tensioned concrete framing scheme for the construction of the new Courthouse and Administration buildings. If this option were selected, we estimate a two-way flat plate method of construction supported by concrete columns would be utilized. The thickness of the slab would vary based on the column spacing.

## **STRUCTURAL DESIGN NARRATIVE THURSTON COUNTY COURTHOUSE AND CIVIC CENTER**

### **PARKING STRUCTURE FRAMING SCHEME**

Typical stand-alone parking structure framing will include one-way post-tensioned concrete floors with one-way 14x16 post-tensioned beams at 20ft to 27ft spacing and 24x36 girders at bearing lines where required for turning bays. Beams are supported by concrete columns that sit atop concrete spread footings. Exterior concrete moment frames or internal concrete shear walls will be utilized for lateral resistance.

### **LATERAL LOAD RESISTING SYSTEM**

Courthouse/Administration: Lateral wind and seismic forces will be transferred through the floor and roof diaphragms and will be resisted by buckling restrained braced frames or special reinforced concrete shear walls. The metal decking at the roof levels will act as a diaphragm to transfer lateral loads to the frame elements. The lateral forces will be resisted by conventional concrete grade beams and mat footings.

Optional Framing Scheme: Two-way flat plates will act as the diaphragm and collect loads to central concrete core walls and perimeter concrete shear walls. Shear walls will be supported by mat foundations.

Parking Structures: Exterior concrete moment frames or internal concrete shear walls will be utilized for lateral resistance.

### **MATERIAL QUANTITY ESTIMATES – INITIAL SCHEMATIC PRICING**

The following quantity estimates can be used for pricing:

Thurston County Courthouse: Material Quantity Estimates for Initial Pricing	
Concrete	
Item	Reinforcing Steel
Mat Foundations under walls and cores	400#/Cu Yd
Spread Footings	80#/Cu Yd
Slab on Grade	90#/CuYd
Basement Wall Foundations	300#/CuYd
Concrete Walls	275#/CuYd
Concrete Columns	500#/Cu Yd
Stair Core Shear Walls	450#/CuYd



## STRUCTURAL DESIGN NARRATIVE THURSTON COUNTY COURTHOUSE AND CIVIC CENTER

Thurston County Courthouse: Material Quantity Estimates for Initial Pricing Structural Steel	
Item	Reinforcing Steel
Roof Framing	20 PSF
Miscellaneous	See Note 1 Below

Note 1: Additional steel quantities should be estimated for all miscellaneous steel including stairs, railings, cladding supports, window washing supports, mechanical supports, operable partition supports, canopies, trellises, other architectural features, elevator supports, etc.

### EXISTING BUILDINGS

Hilltop Campus Options 1 and 4 are currently configured to replace existing building 1 and building 2. Upon the selection of a non-Hilltop Site the buildings 1 to 3 would be removed or potentially maintained and repurposed. Below is a brief summary of structural systems and potential updates or modifications that would be required to bring these buildings up to current code compliance.

#### KEY:



Observations are based on drawings received to date and a cursory review of the existing buildings. If development plans requiring modifications move forward into design, further analysis and field studies will be required.

## **STRUCTURAL DESIGN NARRATIVE THURSTON COUNTY COURTHOUSE AND CIVIC CENTER**

### **Existing Building 1**

Structural System - The existing structural system is comprised of steel columns, glu-lam beams with open web joists at the elevated floors, and open web joists/glu-lam beams at the roof. The building's lateral system is indeterminate with an assumption that plywood diaphragms transfer seismic and wind forces to wood shear walls that provide lateral restraint.

### **Existing Building 2**

Structural System - The existing structural system is comprised of steel columns, pan deck with steel beams at the elevated floor, and open web trusses/glu-lam beams at the roof. The building's lateral system is indeterminate with an assumption that pan deck and plywood diaphragms transfer seismic and wind forces to concrete and/or cmu walls that provide lateral restraint.

### **Existing Building 3**

Structural System - The existing structural system is comprised of concrete columns, concrete walls, cmu walls, "spandeck" proprietary precast concrete plank floors, and steel joists and steel girder beams at the roof. The building's lateral system is comprised of precast and steel roof deck diaphragms that transfer seismic and wind forces to concrete and cmu shear walls which are supported by concrete spread footings.

### **Potential Issues**

Below is a partial list of potential issues related to expansion, renovation, and code compliance:

- If full renovation is pursued it will likely result in inadequacy for foundations, shear walls, diaphragm reinforcing, etc.
- If significant changes are made to the existing floors, including openings or loading modifications, then the existing gravity members will likely not be sufficient.
- Exterior walls may not have proper seismic anchoring.
- Expansion or proximity to new building may affect existing foundations.
- Fire rating of existing roof structure and supporting steel columns may not be sufficient.
- The existing Hilltop campus is located near a steep sloping site.

## **STRUCTURAL DESIGN NARRATIVE THURSTON COUNTY COURTHOUSE AND CIVIC CENTER**

### **Recommended Modifications**

Below are recommendations if significant renovations are pursued:

- Foundations, shear walls, and diaphragm reinforcing will need to be analyzed and strengthened as required to meet current standards.
- Existing floors will need to be checked to conform to current building code.
- Exterior walls may require seismic anchoring to conform with current code.
- Expansion or proximity to new building may require significant revisions to existing foundations.
- A geotechnical analysis of any considered site will be required. Global stability analysis may be required.





XI.

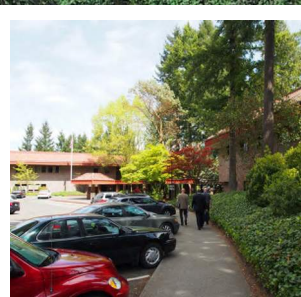
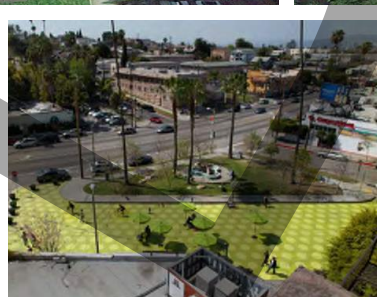
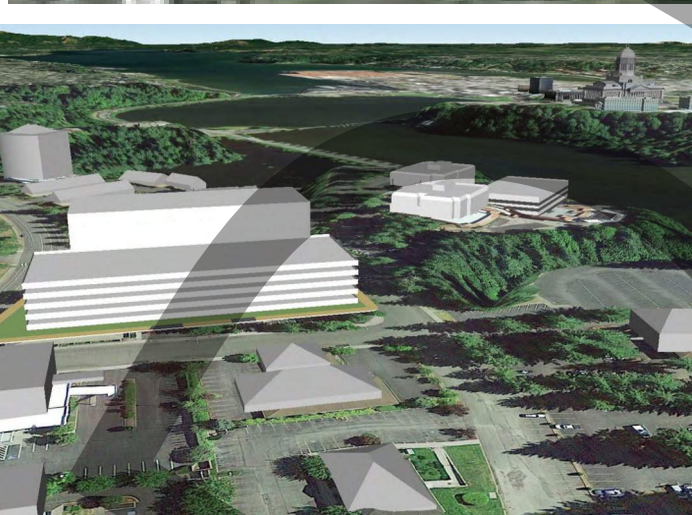
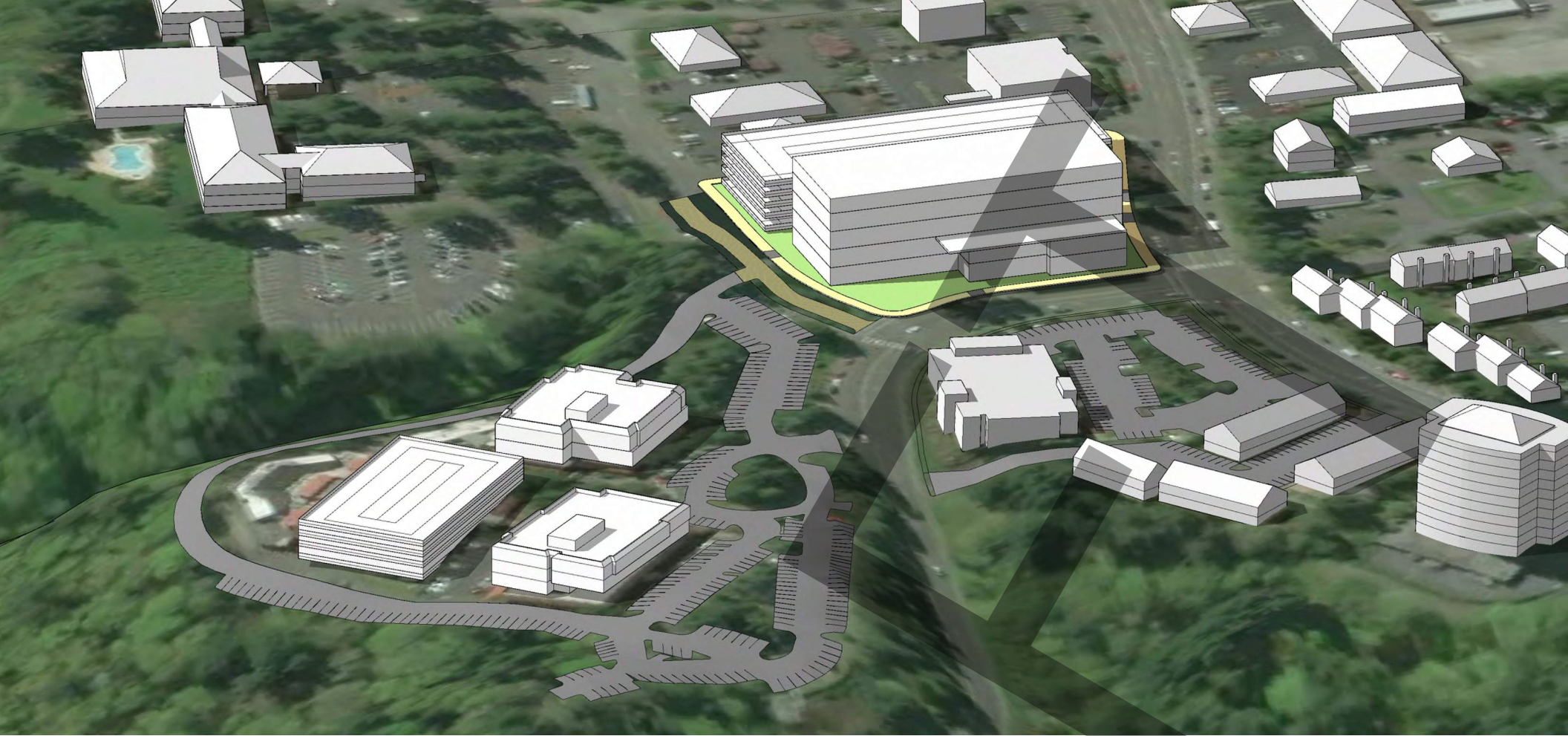
THURSTON COUNTY COURTHOUSE FINAL REPORT

SITE CONCEPTS

DRAFT

# SITE CONCEPTS: HILLTOP CAMPUS







## COURTHOUSE

1 FLOOR @ 14', OFFICE, 28,350 SF x1	=28,350 SF
3 FLOOR @ 17', COURTS, 28,350 SF x3	=85,050 SF
1 FLOOR @ 17', OFFICE, 28,350 SF x1	=28,350 SF
(FUTURE COURT EXPANSION)	
1 FLOOR @ 14', SUBGRADE, 28,350 SF x1	=28,350 SF
=	170,100 SF

NEW PARKING  
STRUCTURE

NEW COURTHOUSE

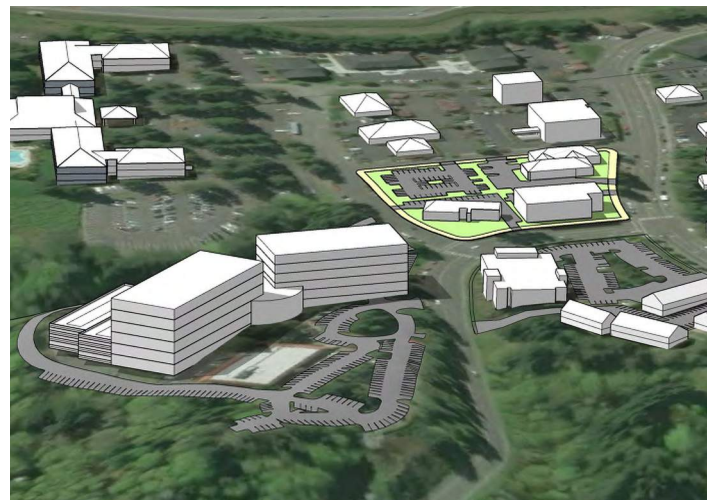
EXISTING  
BUILDING 2

NEW PARKING  
STRUCTURE

EXISTING  
BUILDING 1

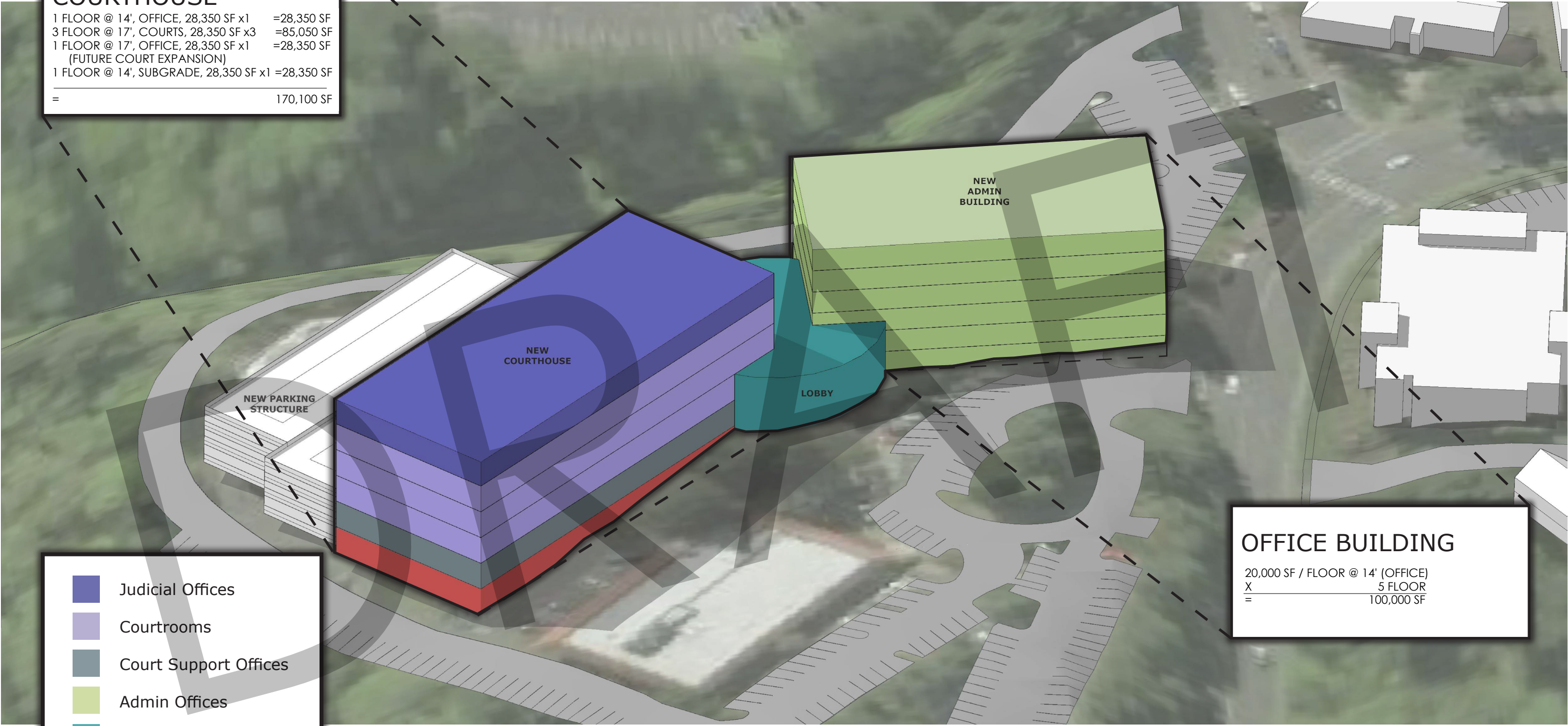
- Judicial Offices
- Courtrooms
- Court Support Offices
- Admin Offices
- Lobby/ Public Functions
- Sheriff Court Support/  
Prisoner Holding







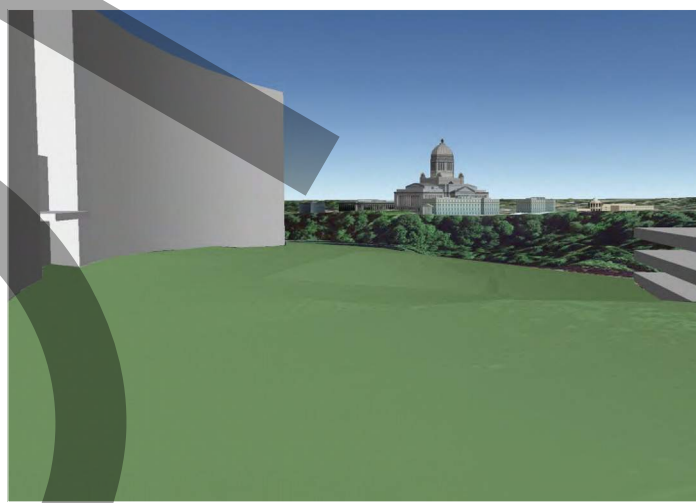
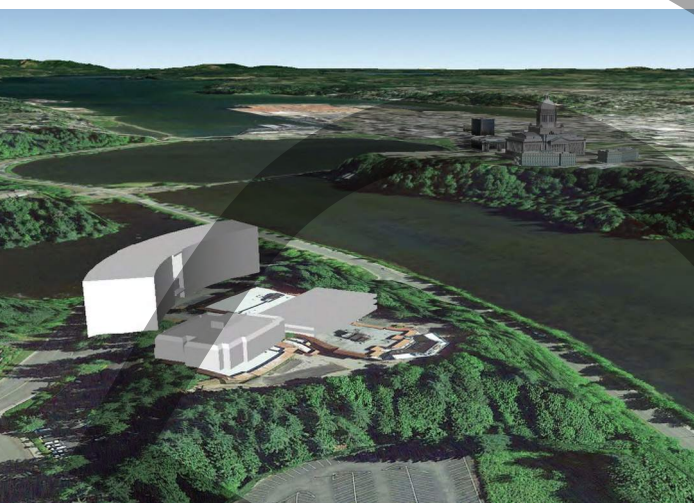
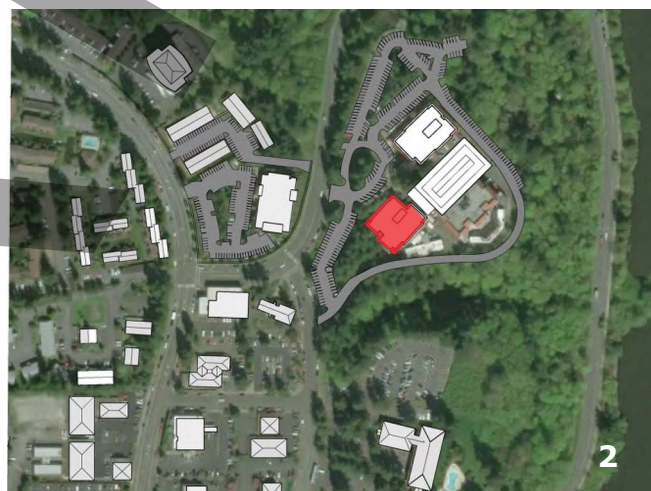
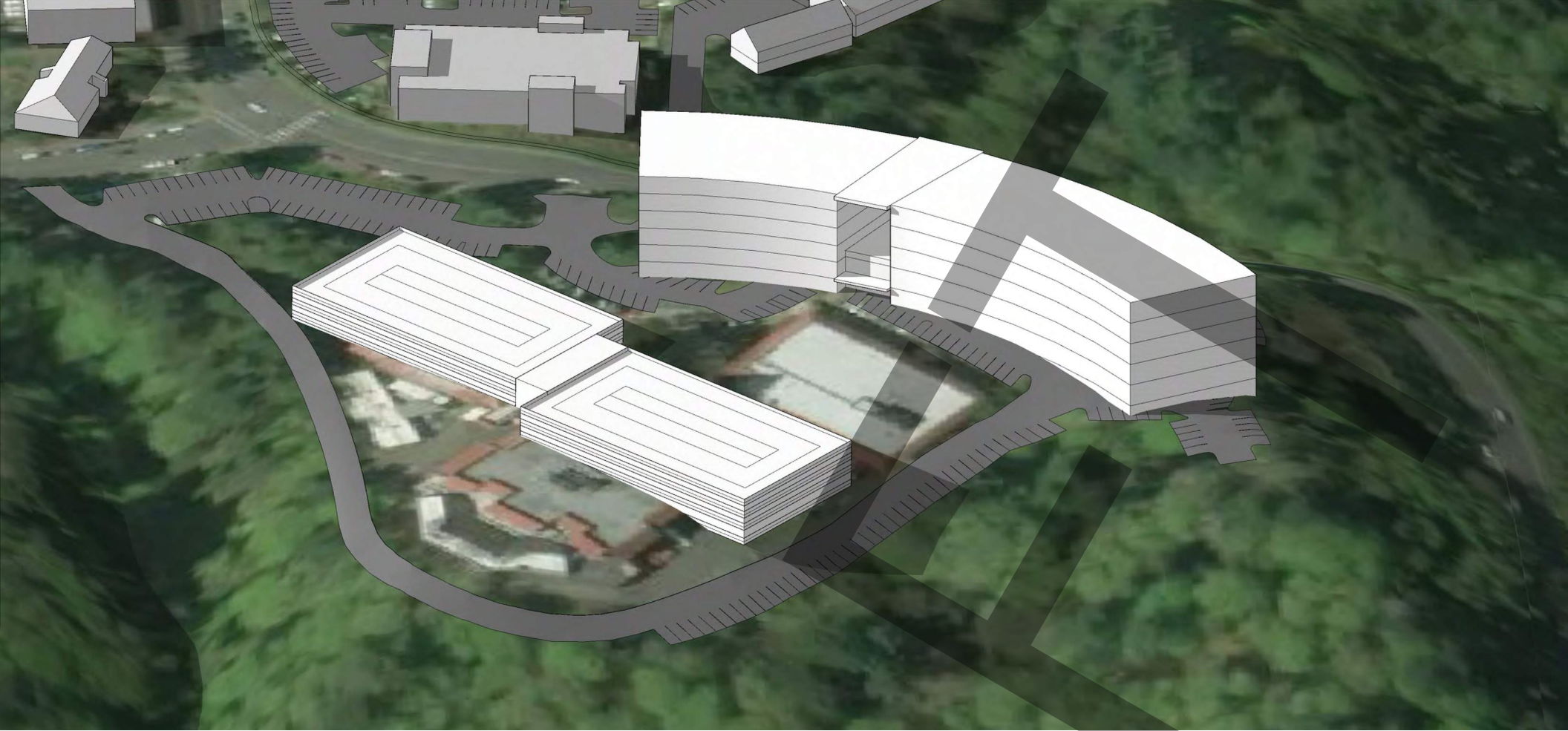
COURTHOUSE		
1 FLOOR @ 14', OFFICE, 28,350 SF x1	=	28,350 SF
3 FLOOR @ 17', COURTS, 28,350 SF x3	=	85,050 SF
1 FLOOR @ 17', OFFICE, 28,350 SF x1	=	28,350 SF
(FUTURE COURT EXPANSION)		
1 FLOOR @ 14', SUBGRADE, 28,350 SF x1	=	28,350 SF
=		170,100 SF



OFFICE BUILDING		
20,000 SF / FLOOR @ 14' (OFFICE)		
X		5 FLOOR
=		100,000 SF

- Judicial Offices
- Courtrooms
- Court Support Offices
- Admin Offices
- Lobby/ Public Functions
- Sheriff Court Support/ Prisoner Holding





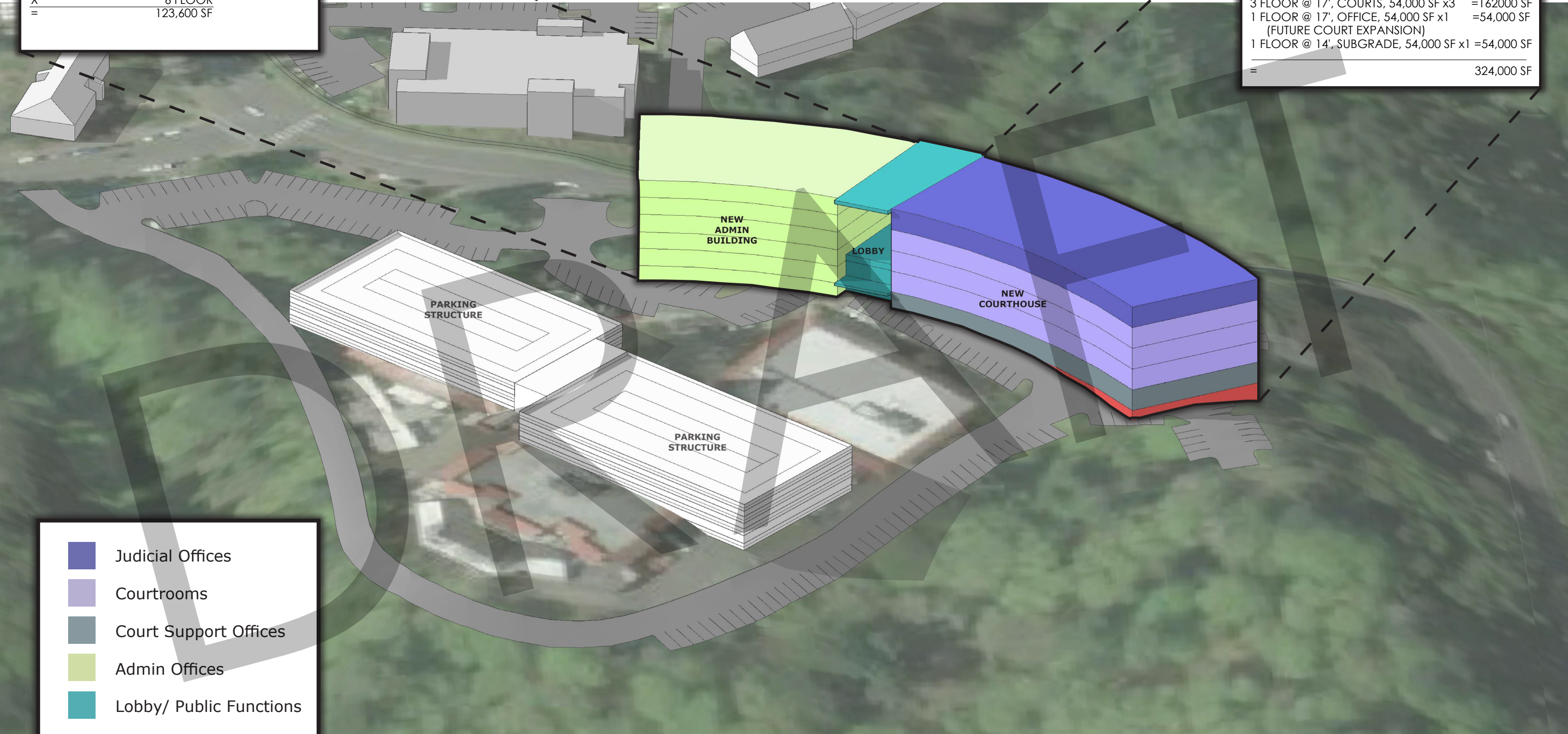


## OFFICE BUILDING

20,600 SF / FLOOR @ 14' (OFFICE)  
 X 6 FLOOR  
 = 123,600 SF

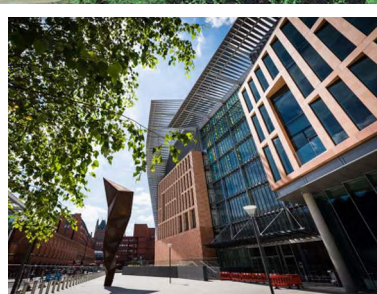
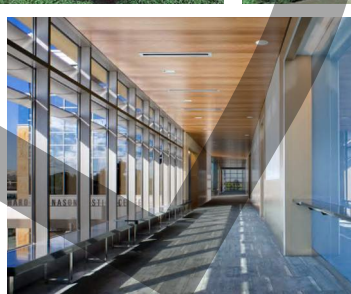
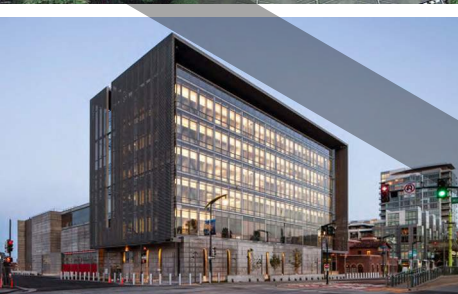
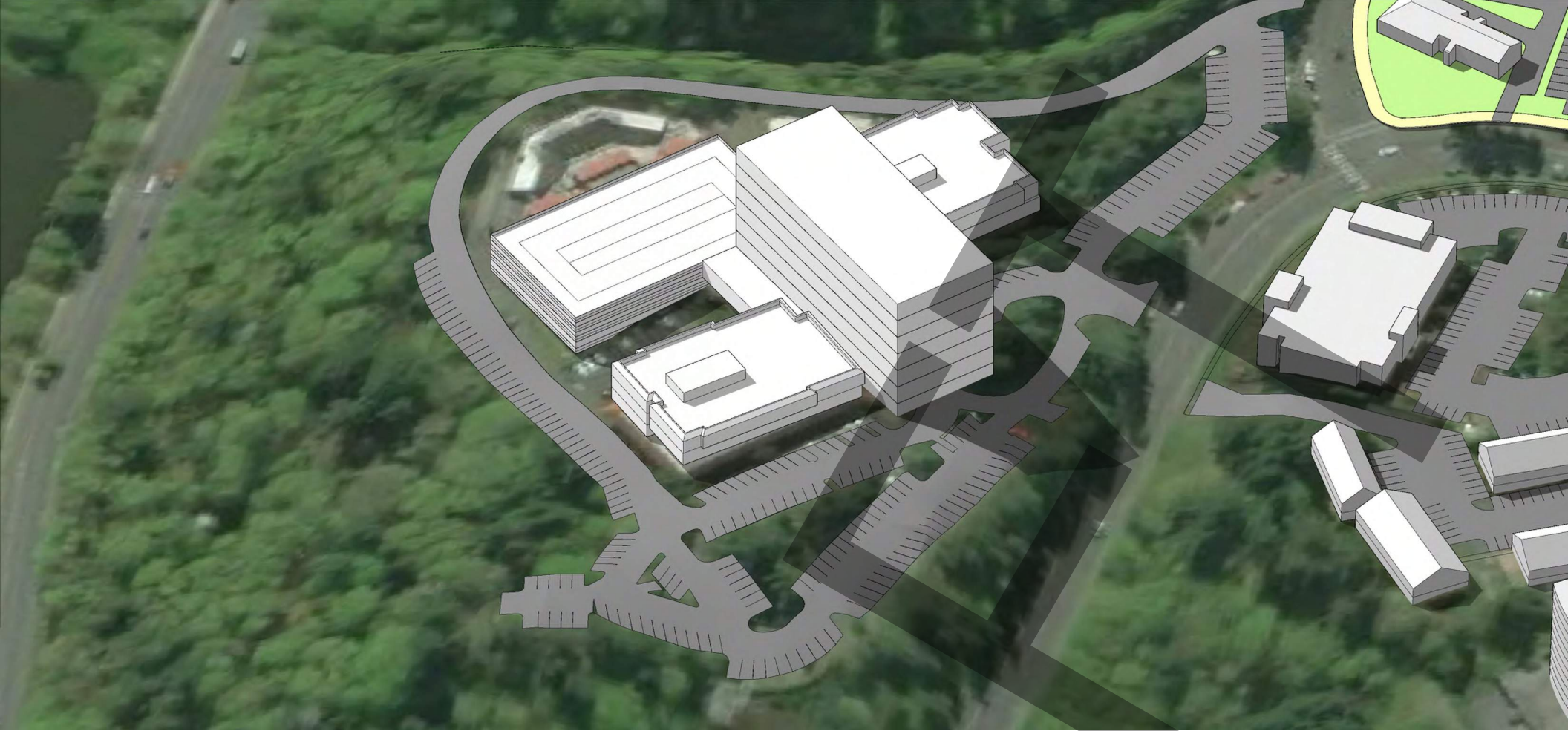
## COURTHOUSE

1 FLOOR @ 14', OFFICE, 54,000 SF x1	=54,000 SF
3 FLOOR @ 17', COURTS, 54,000 SF x3	=162,000 SF
1 FLOOR @ 17', OFFICE, 54,000 SF x1	=54,000 SF
(FUTURE COURT EXPANSION)	
1 FLOOR @ 14', SUBGRADE, 54,000 SF x1	=54,000 SF
<b>=</b>	<b>324,000 SF</b>



- Judicial Offices
- Courtrooms
- Court Support Offices
- Admin Offices
- Lobby/ Public Functions
- Sheriff Court Support/ Prisoner Holding



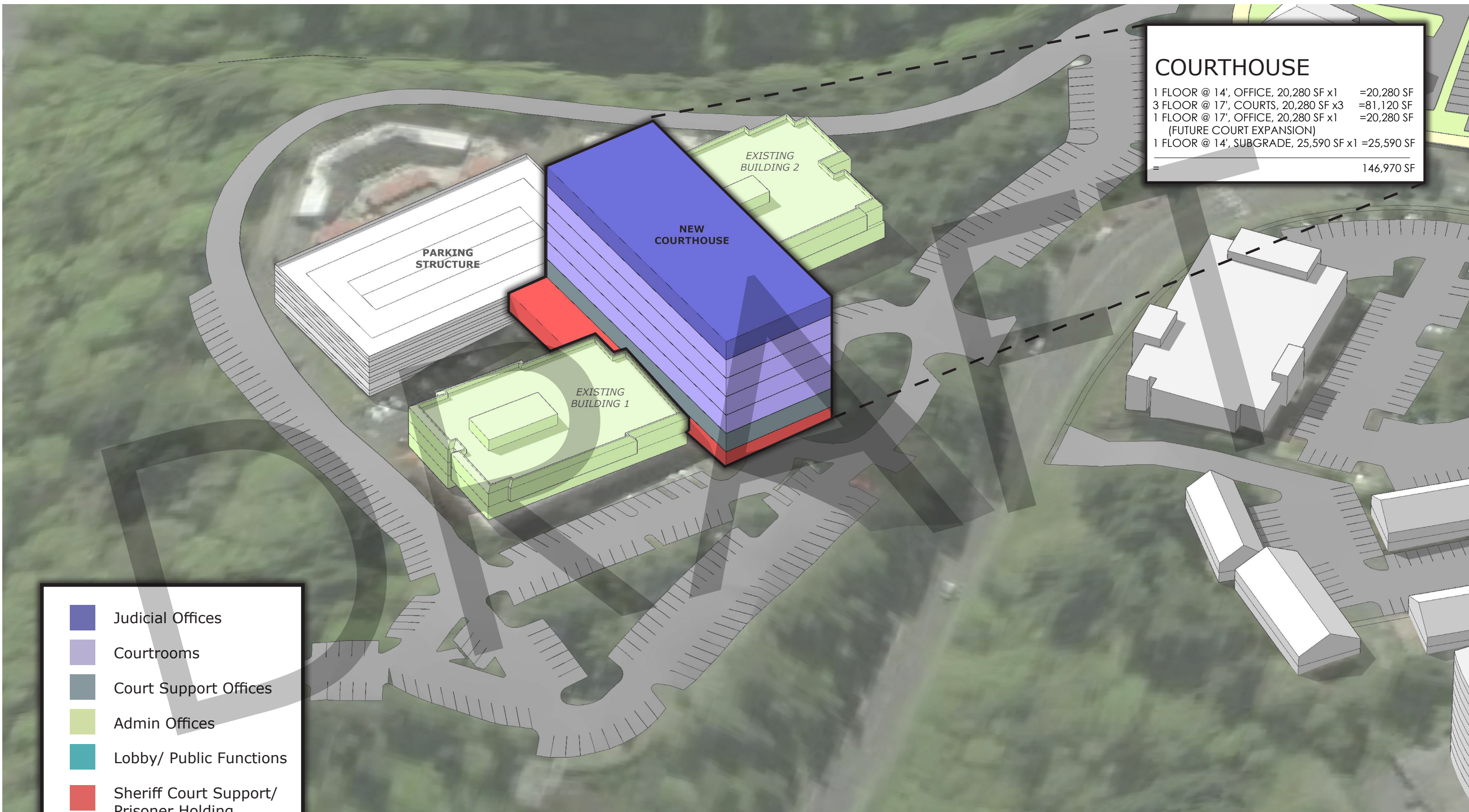




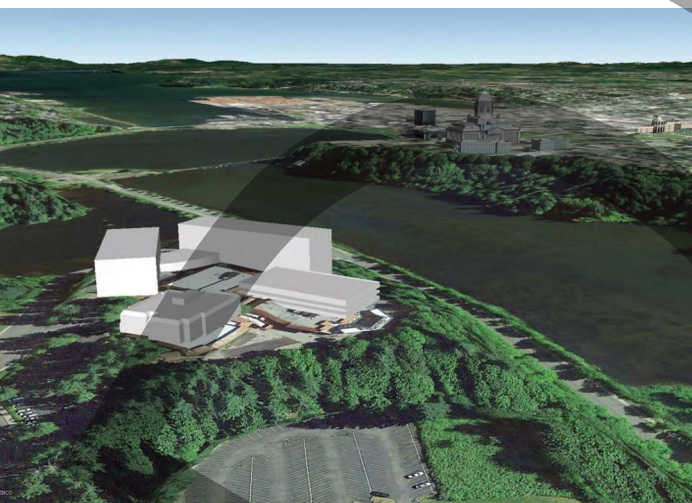
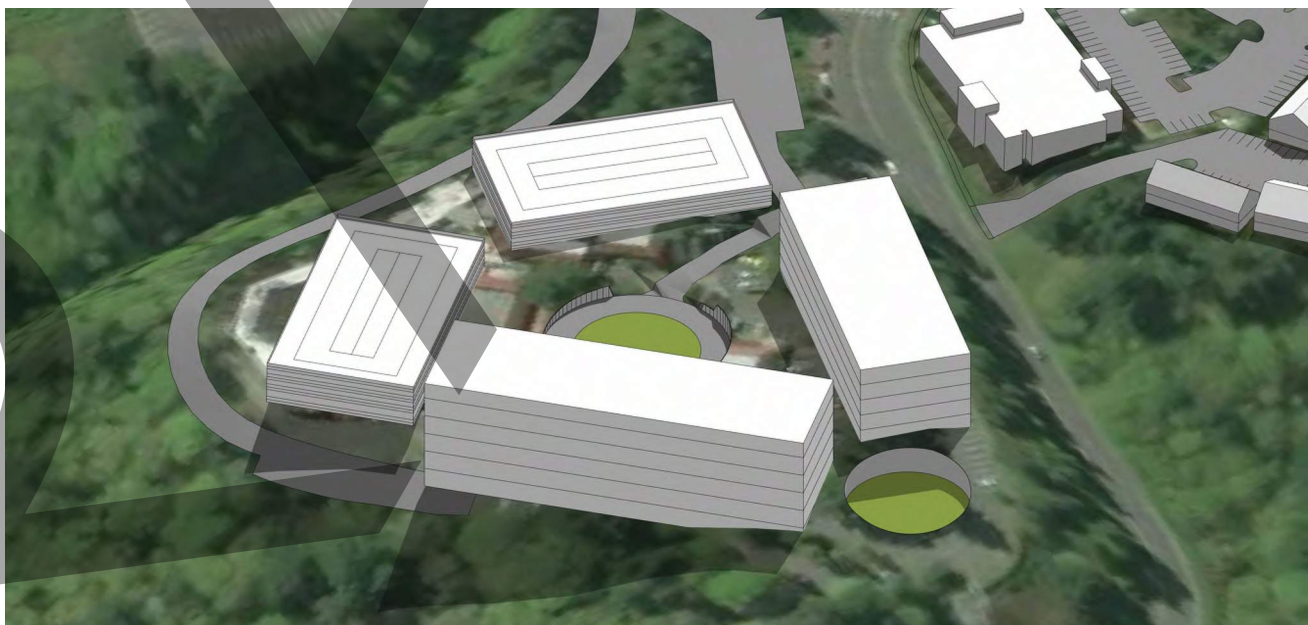
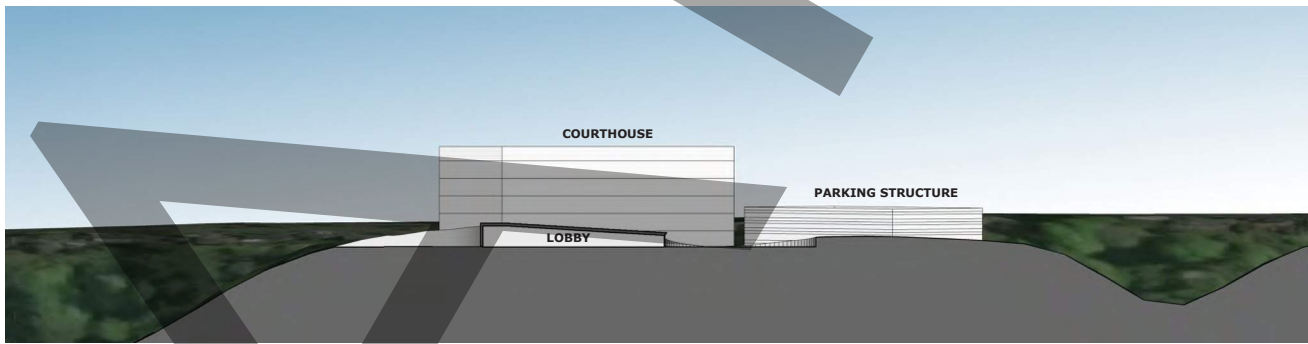
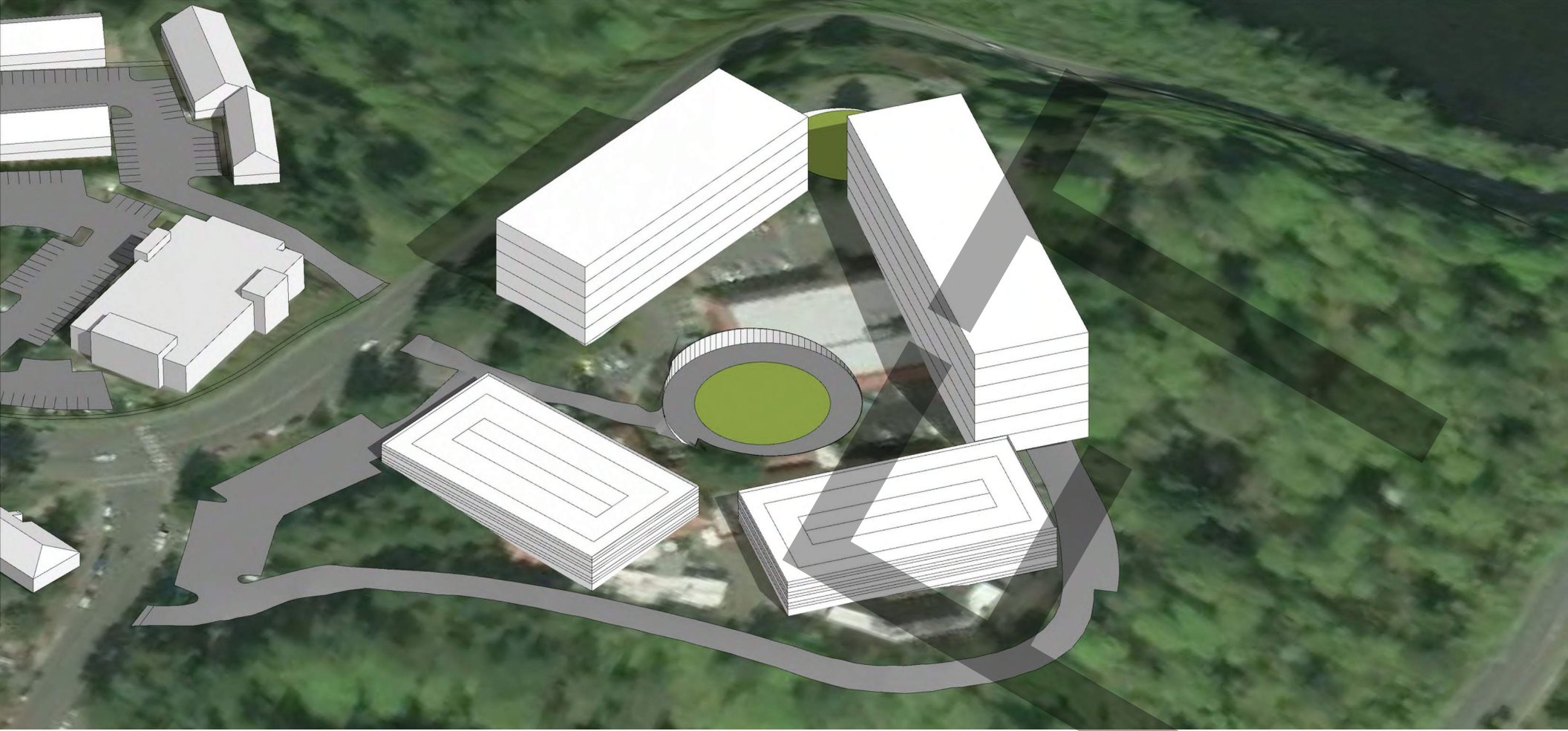
## COURTHOUSE

1 FLOOR @ 14', OFFICE, 20,280 SF x1	=20,280 SF
3 FLOOR @ 17', COURTS, 20,280 SF x3	=81,120 SF
1 FLOOR @ 17', OFFICE, 20,280 SF x1	=20,280 SF
(FUTURE COURT EXPANSION)	
1 FLOOR @ 14', SUBGRADE, 25,590 SF x1	=25,590 SF
<b>=</b>	<b>146,970 SF</b>

- Judicial Offices
- Courtrooms
- Court Support Offices
- Admin Offices
- Lobby/ Public Functions
- Sheriff Court Support/  
Prisoner Holding









## OFFICE BUILDING

22,500 SF / FLOOR @ 14' (OFFICE)  
 X 6 FLOOR  
 = 135,000 SF

## COURTHOUSE

1 FLOOR @ 14', OFFICE, 27,900 SF x1	=27,900 SF
3 FLOOR @ 17', COURTS, 27,900 SF x3	=83,700 SF
1 FLOOR @ 17', OFFICE, 27,900 SF x1	=27,900 SF
(FUTURE COURT EXPANSION)	
1 FLOOR @ 14', SUBGRADE, 27,900 SF x1	=27,900 SF
<b>=</b>	<b>167,400 SF</b>

NEW  
ADMIN  
BUILDING

LOBBY  
BELOW

NEW  
COURTHOUSE

DROP-OFF

PARKING  
STRUCTURE

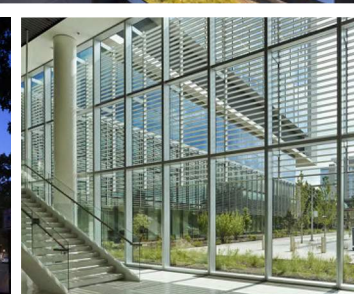
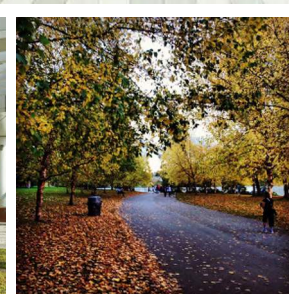
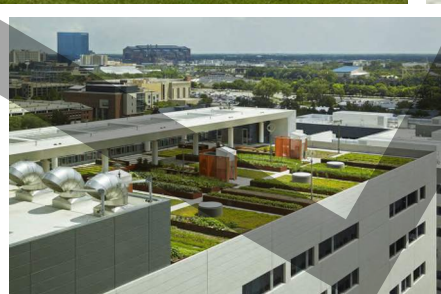
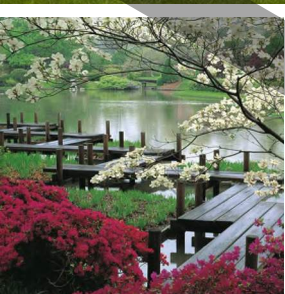
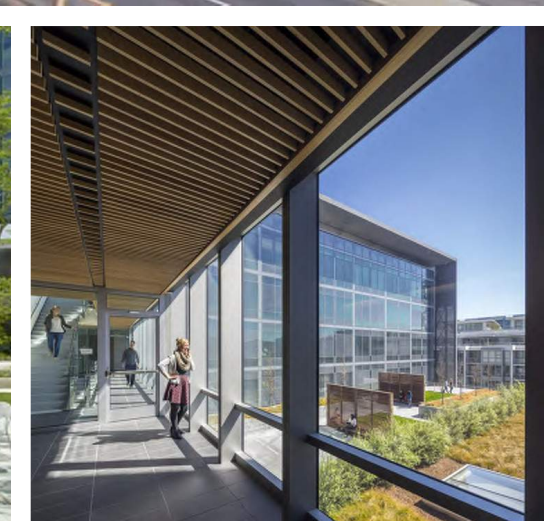
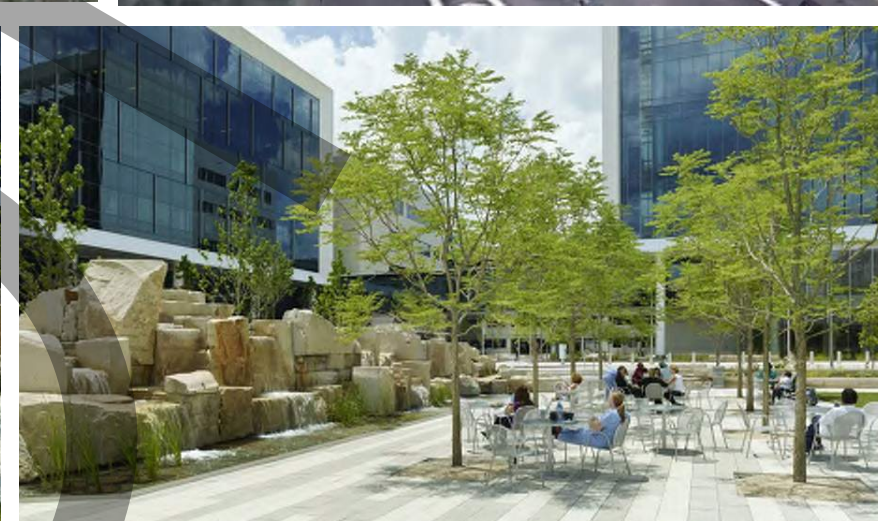
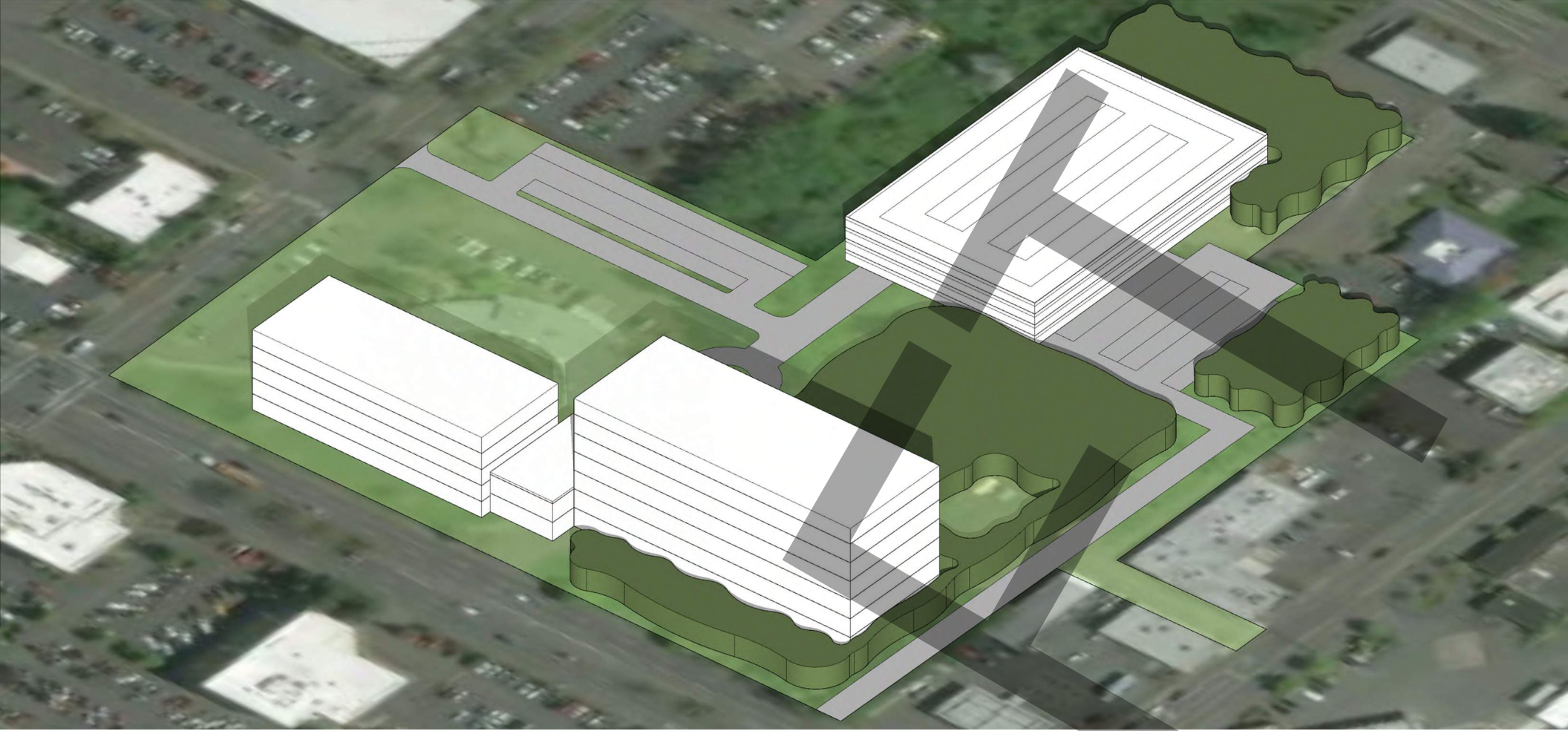
PARKING  
STRUCTURE

- Judicial Offices
- Courtrooms
- Court Support Offices
- Admin Offices
- Lobby/ Public Functions
- Sheriff Court Support/  
Prisoner Holding



## SITE CONCEPTS: PLUM STREET





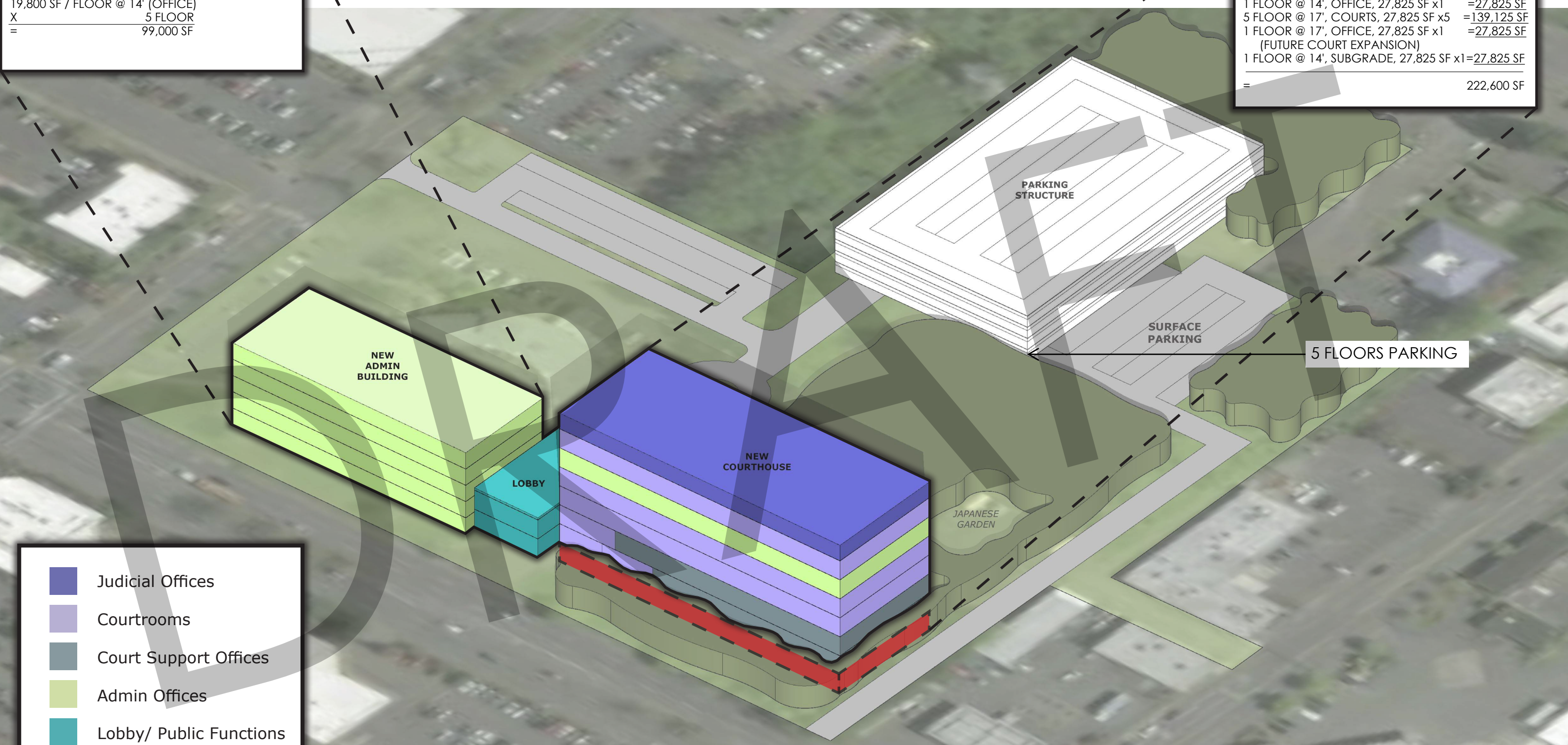


## OFFICE BUILDING

19,800 SF / FLOOR @ 14' (OFFICE)  
 X 5 FLOOR  
 = 99,000 SF

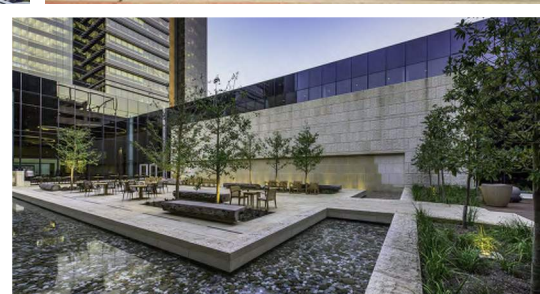
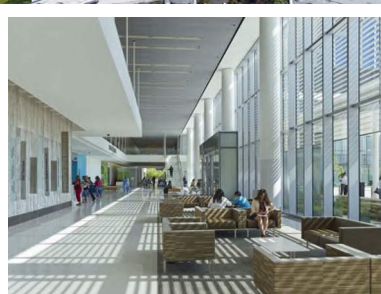
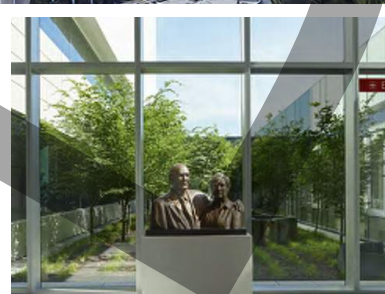
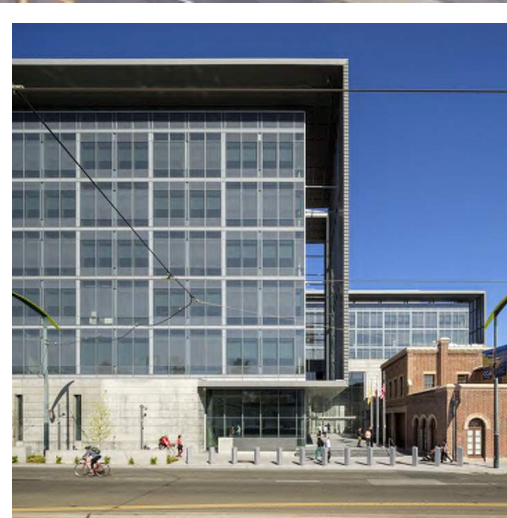
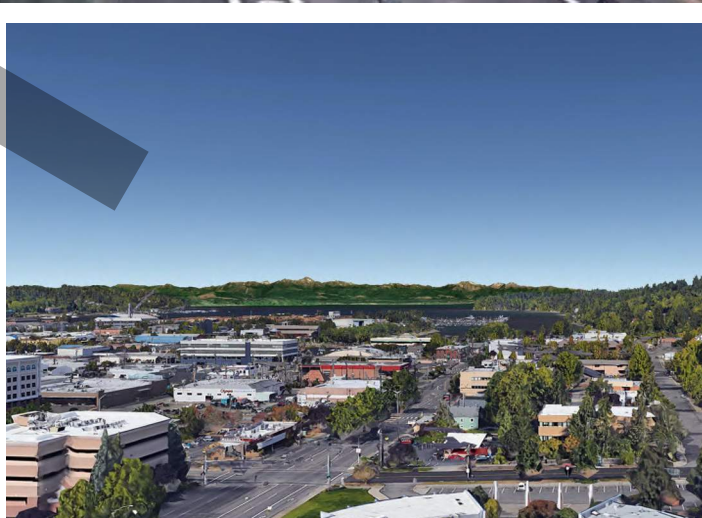
## COURTHOUSE

1 FLOOR @ 14', OFFICE, 27,825 SF x1 =27,825 SF  
 5 FLOOR @ 17', COURTS, 27,825 SF x5 =139,125 SF  
 1 FLOOR @ 17', OFFICE, 27,825 SF x1 =27,825 SF  
 (FUTURE COURT EXPANSION)  
 1 FLOOR @ 14', SUBGRADE, 27,825 SF x1=27,825 SF  
 = 222,600 SF



- Judicial Offices
- Courtrooms
- Court Support Offices
- Admin Offices
- Lobby/ Public Functions
- Sheriff Court Support/ Prisoner Holding







## OFFICE BUILDING

20,00 SF / FLOOR @ 14' (OFFICE)  
 X 5 FLOOR  
 = 103,500 SF

PARKING  
STRUCTURE

5 FLOORS PARKING

NEW SURFACE  
PARKING

JAPANESE  
GARDEN

NEW  
ADMIN  
BUILDING

NEW  
COURTHOUSE

LOBBY

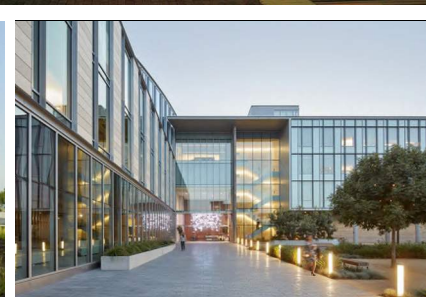
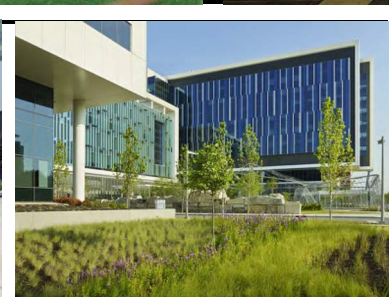
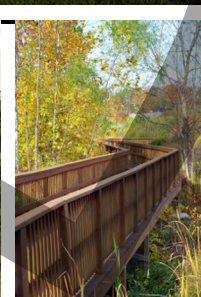
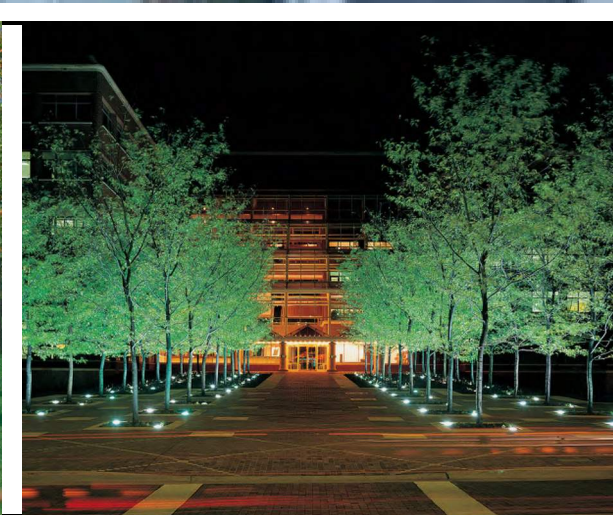
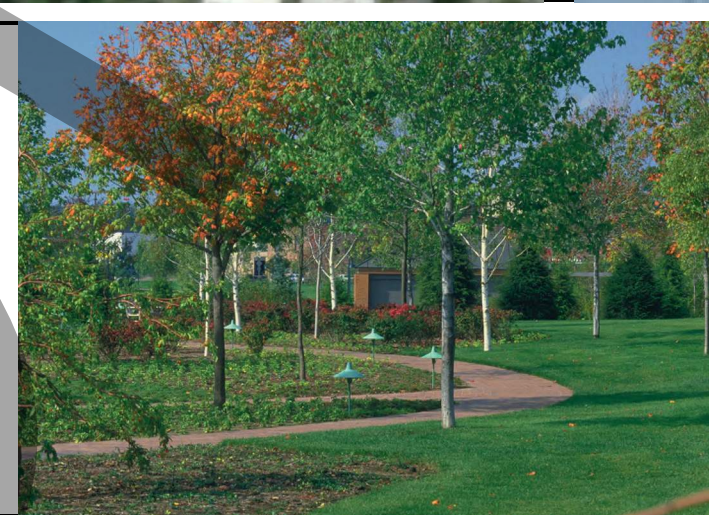
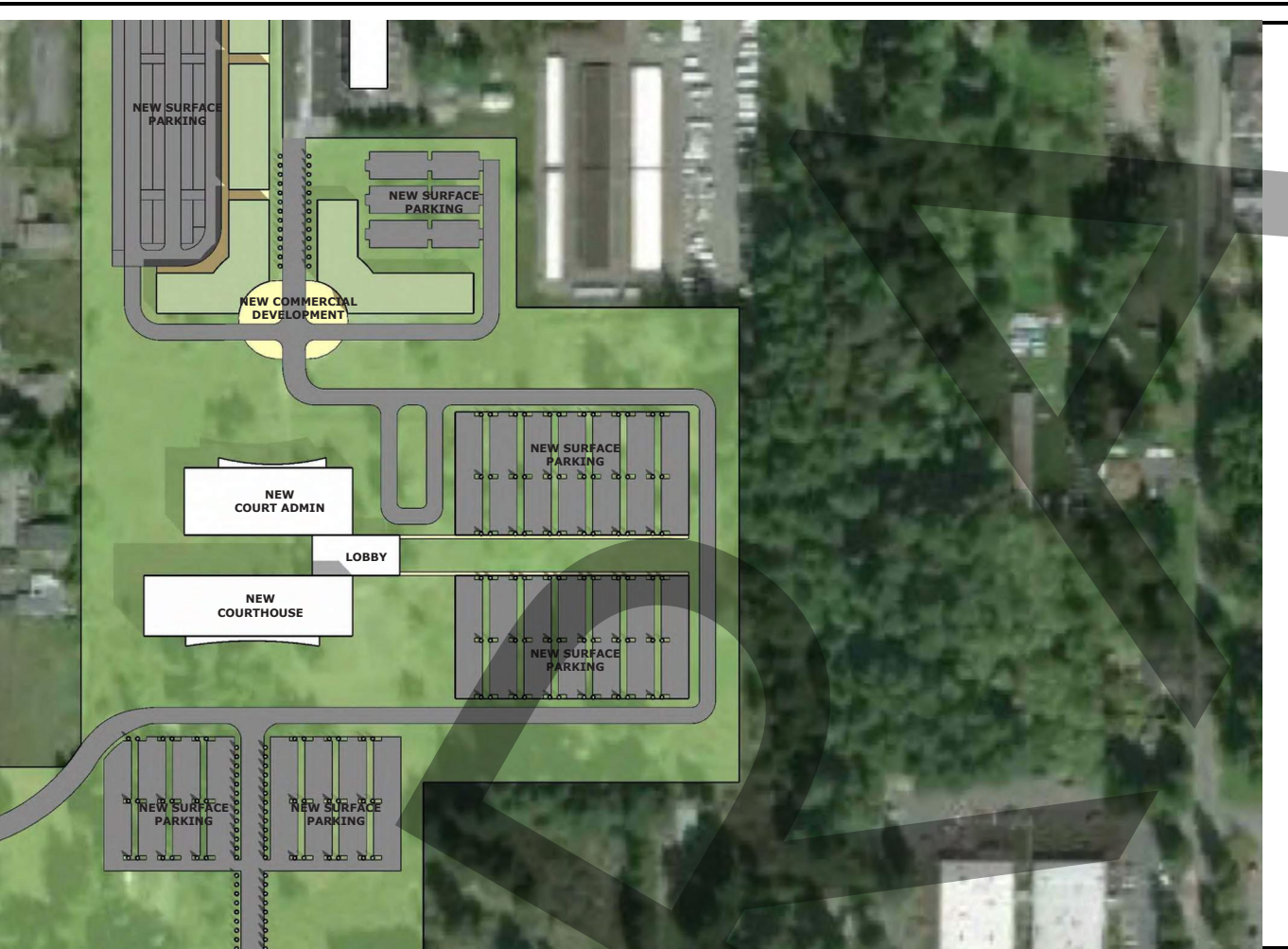
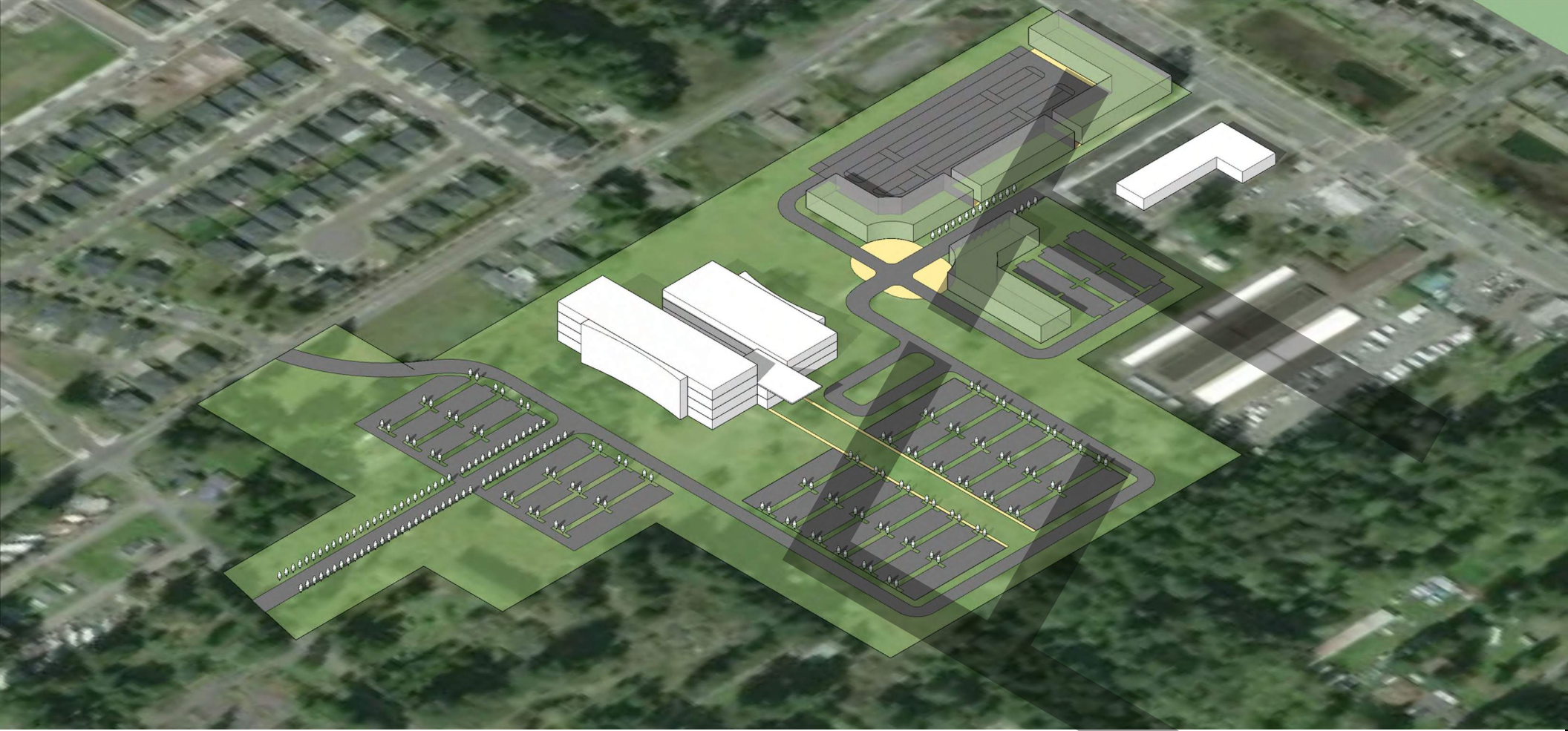
## COURTHOUSE

1 FLOOR @ 14', OFFICE, 27,900 SF x1 =27,900 SF  
 5 FLOOR @ 17', COURTS, 27,900 SF x5 =139,500 SF  
 1 FLOOR @ 17', OFFICE, 27,900 SF x1 =27,900 SF  
 (FUTURE COURT EXPANSION)  
 1 FLOOR @ 14', SUBGRADE, 27,900 SF x1 =27,900 SF  
 = 223,200 SF

- Judicial Offices
- Courtrooms
- Court Support Offices
- Admin Offices
- Lobby/ Public Functions
- Sheriff Court Support/  
Prisoner Holding

# SITE CONCEPTS: HARRISON WEST





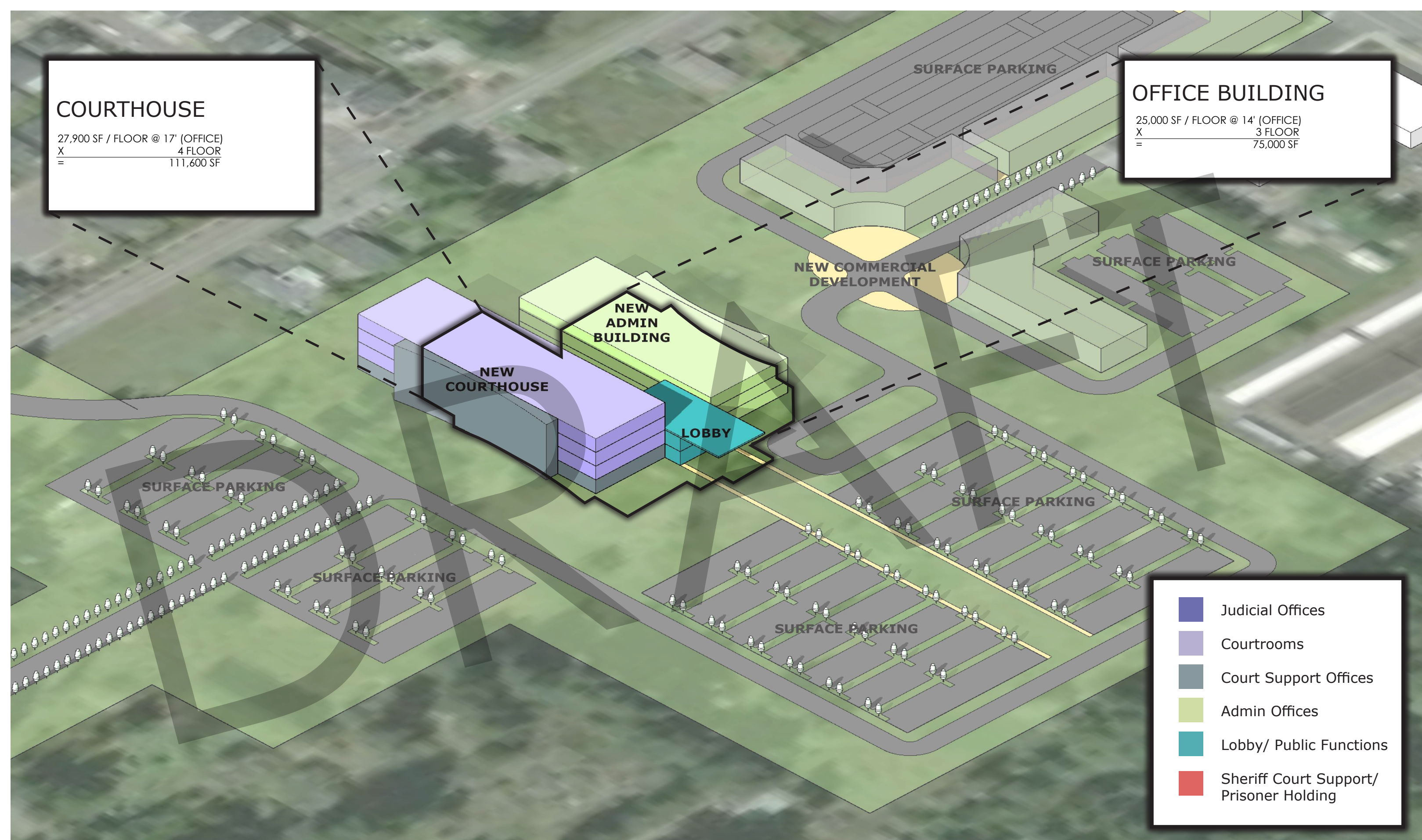


## COURTHOUSE

27,900 SF / FLOOR @ 17' (OFFICE)  
X 4 FLOOR  
= 111,600 SF

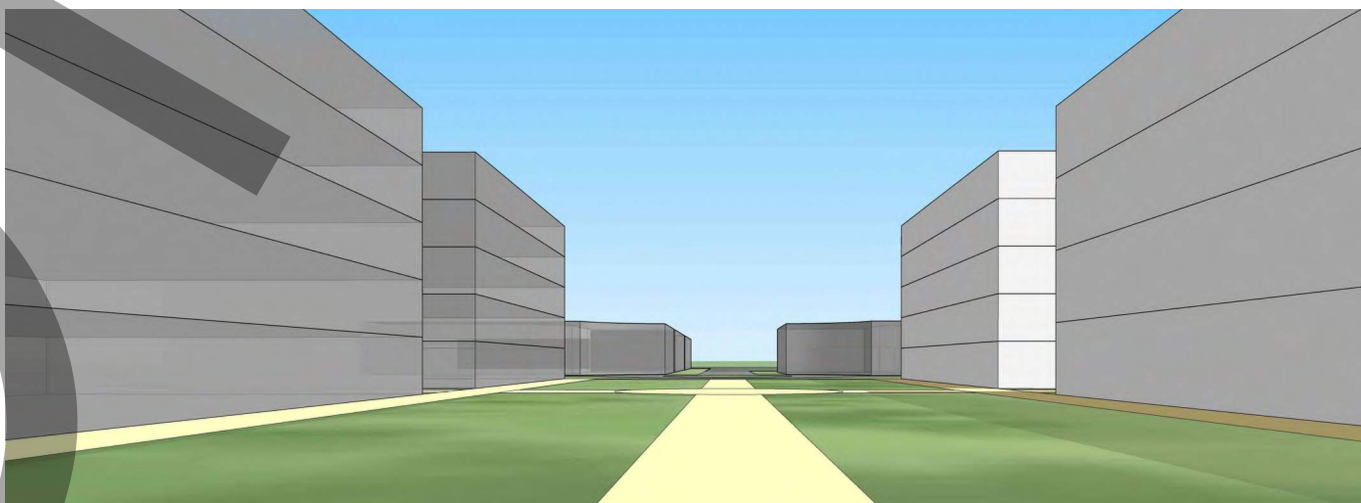
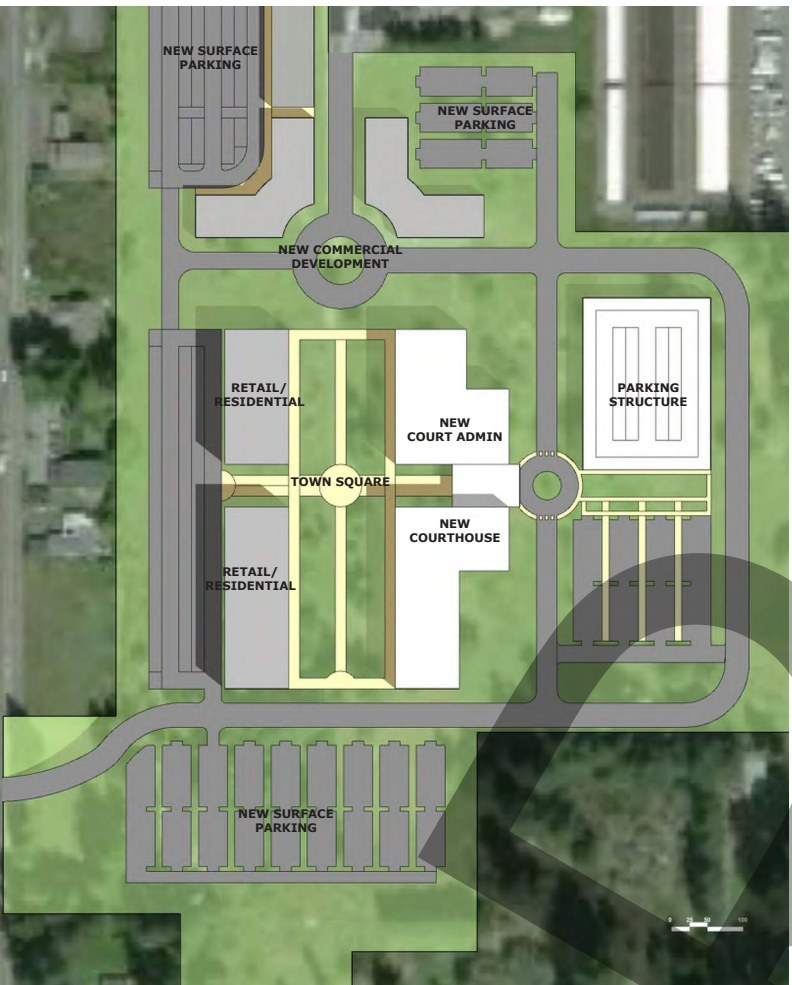
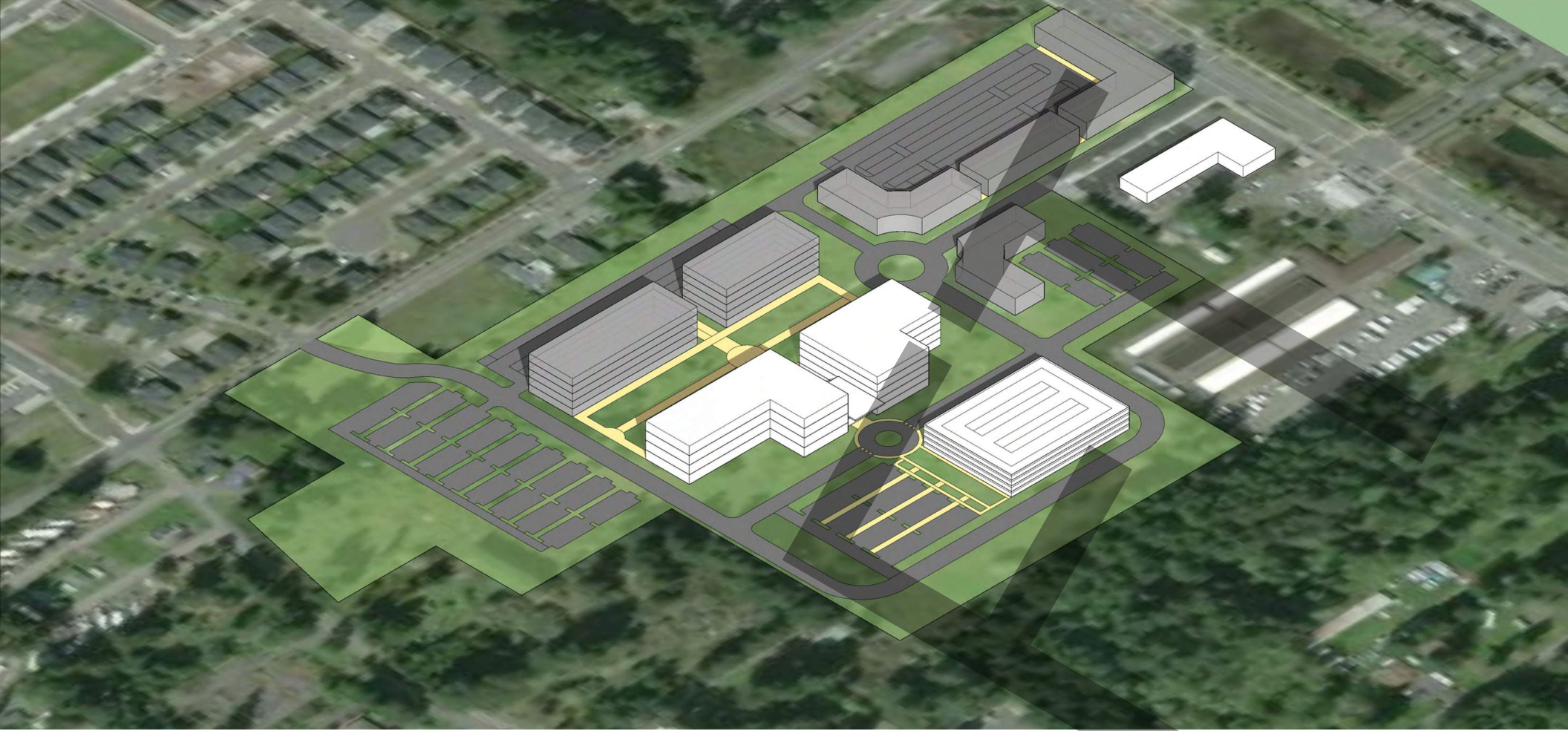
## OFFICE BUILDING

25,000 SF / FLOOR @ 14' (OFFICE)  
X 3 FLOOR  
= 75,000 SF



- Judicial Offices
- Courtrooms
- Court Support Offices
- Admin Offices
- Lobby/ Public Functions
- Sheriff Court Support/ Prisoner Holding







## COURTHOUSE

28,000 SF / FLOOR @ 17' (OFFICE)  
X  
4 FLOOR  
= 112,000 SF

## OFFICE BUILDING

25,000 SF / FLOOR @ 14' (OFFICE)  
X  
5 FLOOR  
= 125,000 SF

RETAIL/  
RESIDENTIAL

RETAIL/  
RESIDENTIAL

NEW COMMERCIAL  
DEVELOPMENT

SURFACE PARKING

NEW  
ADMIN  
BUILDING

NEW  
COURTHOUSE

PARKING  
STRUCTURE

5 FLOORS PARKING

SURFACE PARKING

- Judicial Offices
- Courtrooms
- Court Support Offices
- Admin Offices
- Lobby/ Public Functions
- Sheriff Court Support/  
Prisoner Holding





CONSTRUCTION COST ESTIMATES

CONSTRUCTION COST  
ESTIMATES

XIII.

THURSTON COUNTY COURTHOUSE FINAL REPORT

CONSTRUCTION COST  
ESTIMATES

DRAFT



# THURSTON COUNTY COURTHOUSE and CIVIC CENTER

## A COMPARATIVE OPTION ANALYSIS

**GENERAL SCOPE:**

This is a conceptual cost study to construct new Courthouse and Civic Center facilities for Thurston County in Olympia, WA. Nine different schemes or Options in three different locations have been explored, with field construction targeted to start in spring of 2022, and substantial completion obtained about 24 months later if not phased. The first five 'A' Options are phased and would require a longer time period to complete. All nine Options are figured to have a total of 335,000 SF of new finished floor space, plus another 21,000 SF of basement under each new Courthouse Building. One exception is that Options B1 and B2 each also receive 45,000 SF of supplemental Civic Center floor area, which is broken out in the estimate. New Structured Parking, whether above ground or underground, is not counted in the 335,000 SF plus 21,000 SF of building space. Option C1 is the only Option without Structured Parking. Options C1 and C2 are the only ones with large underground storm detention vaults.

The current scope information provided is limited, and costs listed should be treated as rough-order-of magnitude projections, especially in regards to site related work. Because there are numerous variables to deal with, actual costs could significantly vary as the design scope further develops. As such, it's deemed too early to incorporate large budgetary design contingencies, as the primary intent is to define scopes with a broad brush and make apples-to-apples comparisons.

All estimate pricing includes construction costs, but soft costs such as sales tax, permits, design fees, third party testing, furnishings, and owner's administration & moving costs are specifically excluded. Real estate, procurement and financing costs are excluded as well. Also, for now the offsite work beyond Option C1 & C2 scope, is figured to consist of single lane wide street edge improvements along property lines only, and assumes no significant new street or traffic light replacement or enhancements are required. Also, utility company connection and assessment fees are considered soft costs, and not included.

**INCLUDED:**

Nine independent Options to construct a new Courthouse & Civic Center facilities.  
Building demo in areas of new improvements, and haz-mat abatement allowances.  
Allowances for extensive earthwork cuts & fills and retaining walls in 'A' Options.  
Allowances for onsite storm treatment and underground detention where applicable.  
An emergency back-up generator with full campus power capacity in each Option.  
335,000 SF of new Class 'A' or better building space in each Option.  
Premiums for courtroom spaces, plus 21,000 SF of basement in each Option.  
An additional 45,000 SF of Civic Center floor space in Options B1 & B2 only.  
Pile foundation premiums under new Option B1 and B2 building structures.  
100 year building construction, and Silver LEED premiums.  
Contractor's general requirements, O.H. & profit, bond & insurance, B&O tax.  
Design, sustainable construction, and cost escalation contingencies.

**EXCLUDED:**

New work or expansion in Administration Building at north side of Division St.  
New traffic or street lights or turning lanes, except possibly on Options C1 & C2.  
Offsite utility extensions and extended improvements, except in Options C1 & 2.  
Utility company assessment and connection fees--considered soft costs.  
Wetlands mitigation or contaminated soils abatement--assumed to be N/A.  
Any 'Future Expansion' or 'Private Development' scope noted on plans.  
Temporary courtroom and office facilities for owner, if applicable.  
Jail type construction beyond limited holding areas and in some B1 & 2 spaces.  
Significant subgrade overexcavation work.  
Real estate, procurement and financing costs.  
Sales tax, permits, testing, design fees, and owner's administration costs.  
Furniture, furnishings, or owner's moving costs.

BUDGETARY COST ESTIMATE					
PROJECT COMPONENT	Quantity	Unit	From Page 2	Estimate Cost	REMARKS
<b>THURSTON COUNTY COURTHOUSE &amp; CIVIC CENTER OPTION SUMMARY:</b> (Includes Contractor G.R, OH&P, B&I, B&O, Nominal Contingency & LEED Mark-Ups, Plus Cost Escalation to 2022; No Sales Tax.)					
A1) Hilltop Campus, Option 1.	1.00	LS	\$224,402,303	<b>\$224,402,303</b>	See Page 2 for further Assessment Option breakdowns.
A2) Hilltop Campus, Option 2.	1.00	LS	\$206,781,050	<b>\$206,781,050</b>	" " " " " " " " " " " " " " " "
A3) Hilltop Campus, Option 3.	1.00	LS	\$217,585,488	<b>\$217,585,488</b>	" " " " " " " " " " " " " " " "
A4) Hilltop Campus, Option 4.	1.00	LS	\$225,275,625	<b>\$225,275,625</b>	See Page 3 for further Assessment Option breakdowns.
A5) Hilltop Campus, Option 5.	1.00	LS	\$230,001,381	<b>\$230,001,381</b>	" " " " " " " " " " " " " " " "
B1) Plum Street, Option 1.	1.00	LS	\$238,228,819	<b>\$238,228,819</b>	" " " " " " " " " " " " " " " "
B2) Plum Street, Option 2.	1.00	LS	\$236,444,569	<b>\$236,444,569</b>	See Page 4 for further Assessment Option breakdowns.
C1) Harrison Avenue, Option 1.	1.00	LS	\$196,881,750	<b>\$196,881,750</b>	" " " " " " " " " " " " " " " "
C2) Harrison Avenue, Option 2.	1.00	LS	\$208,922,513	<b>\$208,922,513</b>	" " " " " " " " " " " " " " " "

**NOTES:**

The above Option Summary costs include a 25% contractor's mark-up to cover general requirements, overhead & profit, bond & insurance, and B & O tax, plus a modest design contingency and sustainability premium. Also included are provisions for construction cost escalation premiums, the specifics of which are listed on Page 2 in the Option Summary Breakdown. Soft costs, such as sales tax, permits, third party testing, design fees, furnishings, and owner's administration & moving are not included. Real estate, procurement, financing and utility connection & assessment fees are not included either.

**THURSTON COUNTY COURTHOUSE and CIVIC CENTER**  
**A COMPARATIVE OPTION ANALYSIS**

PROJECT COMPONENT	Escalation Factor	Unit	With G.C. Mark-Ups	With Escalation	REMARKS
<b>THURSTON COUNTY COURTHOUSE &amp; CIVIC CENTER OPTION SUMMARY BREAKDOWN:</b>					
<b>A1) HILLTOP CAMPUS, OPTION 1:</b>					<b>5 Phases</b>
Phase 1, South Campus Building Demolition.	1.16	LS	\$317,750	\$368,590	See Page 5 for estimate details.
Phase 2, South Campus Site Development.	1.17	LS	\$3,100,000	\$3,627,000	" " " " " " " " "
Phase 2, New Courthouse Building.	1.17	LS	\$76,537,500	\$89,548,875	" " " " " " " " "
Phase 2, New Lobby Wing.	1.17	LS	\$4,456,250	\$5,213,813	" " " " " " " " "
Ph. 2, South Structured Parking & Connector.	1.17	LS	\$16,630,000	\$19,457,100	" " " " " " " " "
Phase 3A, Move Into New Courthouse.	1.26	LS	NIC	NIC	Soft costs by owner.
Phase 3B, Building 1, 2 & 3 Demolition.	1.28	LS	\$1,487,500	\$1,904,000	See Page 5 for estimate details.
Phase 4, North Campus Site Development.	1.29	LS	\$5,338,750	\$6,886,988	See Page 6 for estimate details.
Ph. 4, North Structured Parking & Connector.	1.29	LS	\$7,562,500	\$9,755,625	" " " " " " " " "
Phase 5, New Administration Buildings.	1.35	LS	\$64,918,750	\$87,640,313	" " " " " " " " "
\$180,349,000			<b>A1, Hilltop Campus, Option 1:</b>	<b>\$224,402,303</b>	
<b>A2) HILLTOP CAMPUS, OPTION 2:</b>					<b>4 Phases</b>
Phase 1, Building 3 Demolition.	1.16	LS	\$800,000	\$928,000	See Page 7 for estimate details.
Phase 2, Initial Site Development.	1.17	LS	\$3,078,750	\$3,602,138	" " " " " " " " "
Phase 2, New Courthouse Building.	1.17	LS	\$76,537,500	\$89,548,875	" " " " " " " " "
Phase 2, Structured Parking.	1.17	LS	\$16,875,000	\$19,743,750	" " " " " " " " "
Phase 3, Building 2 Demolition.	1.28	LS	\$325,000	\$416,000	" " " " " " " " "
Phase 3, South Site Development.	1.28	LS	\$2,737,500	\$3,504,000	" " " " " " " " "
Phase 3, New Lobby Wing.	1.28	LS	\$4,801,250	\$6,145,600	See Page 8 for estimate details.
Phase 3, New Administration Building.	1.28	LS	\$62,712,500	\$80,272,000	" " " " " " " " "
Phase 4, Building 1 Demolition.	1.35	LS	\$375,000	\$506,250	" " " " " " " " "
Phase 4, North Site Development.	1.35	LS	\$1,566,250	\$2,114,438	" " " " " " " " "
\$169,808,750			<b>A2, Hilltop Campus, Option 2:</b>	<b>\$206,781,050</b>	
<b>A3) HILLTOP CAMPUS, OPTION 3:</b>					<b>3 Phases</b>
Phase 1, Building 3 Demolition.	1.16	LS	\$800,000	\$928,000	See Page 9 for estimate details.
Phase 2, Initial Site Development.	1.17	LS	\$6,656,250	\$7,787,813	" " " " " " " " "
Phase 2, New Courthouse Building.	1.17	LS	\$80,212,500	\$93,848,625	" " " " " " " " "
Phase 2, New Lobby Wing.	1.17	LS	\$4,600,000	\$5,382,000	" " " " " " " " "
Phase 2, New Administration Building.	1.17	LS	\$67,125,000	\$78,536,250	" " " " " " " " "
Phase 2, North Structured Parking.	1.17	LS	\$14,040,000	\$16,426,800	" " " " " " " " "
Phase 3, Building 1 & 2 Demolition.	1.28	LS	\$687,500	\$880,000	See Page 10 for estimate details.
Phase 3, Balance of Site Development.	1.28	LS	\$3,758,125	\$4,810,400	" " " " " " " " "
Ph. 3, South Structured Parking & Connector.	1.28	LS	\$7,020,000	\$8,985,600	" " " " " " " " "
\$184,899,375			<b>A3, Hilltop Campus, Option 3:</b>	<b>\$217,585,488</b>	
<b>THE OPTION SUMMARY BREAKDOWN continues on the next page.</b>					

**NOTES:**

The above Option Summary Breakdown costs include a 25% contractor's mark-up to cover general requirements, overhead & profit, bond & insurance, and B & O tax, plus a modest design contingency and sustainability premium. Also included are premiums for anticipated construction cost escalation as listed in the second column above. Soft costs, such as sales tax, permits, third party testing, design fees, furnishings, and owner's administration and moving costs are not included.



**THURSTON COUNTY COURTHOUSE and CIVIC CENTER**  
**A COMPARATIVE OPTION ANALYSIS**

PROJECT COMPONENT	Escalation Factor	Unit	With G.C. Mark-Ups	With Escalation	REMARKS
<b>THURSTON COUNTY COURTHOUSE &amp; CIVIC CENTER OPTION SUMMARY BREAKDOWN:</b>					
(Continued from the previous page.)					
<b>A4) HILLTOP CAMPUS, OPTION 4:</b>					<b>4 Phases</b>
Phase 1, Building 3 Demolition.	1.16	LS	\$837,500	\$971,500	See Page 11 for estimate details.
Phase 2, Initial North Campus Site Development.	1.17	LS	\$5,443,750	\$6,369,188	" " " " " " " " "
Phase 2, New Courthouse Building.	1.17	LS	\$80,212,500	\$93,848,625	" " " " " " " " "
Phase 2, New Lobby Wing.	1.17	LS	\$4,456,250	\$5,213,813	" " " " " " " " "
Phase 2, North Structured Parking.	1.17	LS	\$23,625,000	\$27,641,250	" " " " " " " " "
Phase 3, Building 1 & 2 Demolition.	1.28	LS	\$650,000	\$832,000	" " " " " " " " "
Phase 3, Balance of North Campus Site Work.	1.28	LS	\$2,350,000	\$3,008,000	See Page 12 for estimate details.
Phase 3, New Administration Buildings.	1.28	LS	\$65,031,250	\$83,240,000	" " " " " " " " "
Phase 4, South Campus Building Demolition.	1.35	LS	\$273,750	\$369,563	" " " " " " " " "
Phase 4, South Campus Site Development.	1.35	LS	\$2,801,250	\$3,781,688	" " " " " " " " "
\$185,681,250					
			<b>A4, Hilltop Campus, Option 4:</b>	<b>\$225,275,625</b>	
<b>A5) HILLTOP CAMPUS, OPTION 5:</b>					<b>3 Phases</b>
Phase 1, Building 3 Demolition.	1.16	LS	\$837,500	\$971,500	See Page 13 for estimate details.
Ph. 1, Initial North Campus Site Development.	1.17	LS	\$1,590,625	\$1,861,031	" " " " " " " " "
Phase 1, North Structured Parking.	1.17	LS	\$23,625,000	\$27,641,250	" " " " " " " " "
Phase 2, Building 1 Demolition.	1.23	LS	\$375,000	\$461,250	" " " " " " " " "
Ph. 2, Bulk of North Campus Site Development.	1.23	LS	\$5,932,500	\$7,296,975	" " " " " " " " "
Phase 2, New Courthouse Building.	1.23	LS	\$76,537,500	\$94,141,125	See Page 14 for estimate details.
Phase 2, New Below Grade Lobby Wing.	1.23	LS	\$4,312,500	\$5,304,375	" " " " " " " " "
Phase 2, New Administration Building.	1.23	LS	\$62,712,500	\$77,136,375	" " " " " " " " "
Phase 3, Building 2 Demolition.	1.35	LS	\$325,000	\$438,750	" " " " " " " " "
Phase 3, Balance of Site Development.	1.35	LS	\$2,175,000	\$2,936,250	" " " " " " " " "
Phase 3, South Structured Parking.	1.35	LS	\$8,750,000	\$11,812,500	" " " " " " " " "
\$187,173,125					
			<b>A5, Hilltop Campus, Option 5:</b>	<b>\$230,001,381</b>	
<b>B1) PLUM STREET, OPTION 1.</b>					<b>All in One Phase</b>
B1 Site Development.	1.17	LS	\$10,000,000	\$11,700,000	See Page 15 for estimate details.
New B1 Courthouse Building.	1.17	LS	\$87,937,500	\$102,886,875	" " " " " " " " "
New B1 Lobby Wing.	1.17	LS	\$4,815,625	\$5,634,281	" " " " " " " " "
New B1 Administration Building.	1.17	LS	\$56,318,750	\$65,892,938	" " " " " " " " "
Supplemental B1 Civic Center Space.	1.17	LS	\$19,462,500	\$22,771,125	" " " " " " " " "
B1 Structured Parking.	1.17	LS	\$25,080,000	\$29,343,600	See Page 16 for estimate details.
\$203,614,375					
			<b>B1, Plum Street, Option 1:</b>	<b>\$238,228,819</b>	
<b>THE OPTION SUMMARY BREAKDOWN continues on the next page.</b>					

**NOTES:**

The above Option Summary Breakdown costs include a 25% contractor's mark-up to cover general requirements, overhead & profit, bond & insurance, and B & O tax, plus a modest design contingency and sustainability premium. Also included are premiums for anticipated construction cost escalation as listed in the second column above. Soft costs, such as sales tax, permits, third party testing, design fees, furnishings, and owner's administration and moving costs are not included.

**THURSTON COUNTY COURTHOUSE and CIVIC CENTER**  
**A COMPARATIVE OPTION ANALYSIS**

PROJECT COMPONENT	Escalation Factor	Unit	With G.C. Mark-Ups	With Escalation	REMARKS
<b>THURSTON COUNTY COURTHOUSE &amp; CIVIC CENTER OPTION SUMMARY BREAKDOWN:</b>					
(Continued from the previous page.)					
<b>B2) PLUM STREET, OPTION 2.</b>					<b>All in One Phase</b>
B2 Site Development.	1.17	LS	\$9,712,500	\$11,363,625	See Page 17 for estimate details.
New B2 Courthouse Building.	1.17	LS	\$86,575,000	\$101,292,750	" " " " " " " " "
New B2 Lobby Wing.	1.17	LS	\$4,815,625	\$5,634,281	" " " " " " " " "
New B2 Administration Building.	1.17	LS	\$56,443,750	\$66,039,188	" " " " " " " " "
Supplemental B2 Civic Center Space.	1.17	LS	\$19,462,500	\$22,771,125	" " " " " " " " "
B2 Structured Parking.	1.17	LS	\$25,080,000	\$29,343,600	See Page 18 for estimate details.
\$202,089,375			<b>B2, Plum Street, Option 2:</b>	<b>\$236,444,569</b>	
<b>C1) HARRISON AVENUE, OPTION 1.</b>					<b>All in One Phase</b>
C1 Site Development.	1.17	LS	\$18,268,750	\$21,374,438	See Page 19 for estimate details.
C1 Offsite Infrastructure.	1.17	LS	\$6,000,000	\$7,020,000	" " " " " " " " "
New C1 Courthouse Building.	1.17	LS	\$76,537,500	\$89,548,875	" " " " " " " " "
New C1 Lobby Wing.	1.17	LS	\$4,756,250	\$5,564,813	" " " " " " " " "
New C1 Administration Building.	1.17	LS	\$62,712,500	\$73,373,625	" " " " " " " " "
\$168,275,000			<b>C1, Harrison Avenue, Option 1:</b>	<b>\$196,881,750</b>	
<b>C2) HARRISON AVENUE, OPTION 2.</b>					<b>All in One Phase</b>
C2 Site Development.	1.17	LS	\$16,325,000	\$19,100,250	See Page 20 for estimate details.
C2 Offsite Infrastructure.	1.17	LS	\$6,000,000	\$7,020,000	" " " " " " " " "
New C2 Courthouse Building.	1.17	LS	\$76,537,500	\$89,548,875	" " " " " " " " "
New C2 Lobby Wing.	1.17	LS	\$4,706,250	\$5,506,313	" " " " " " " " "
New C2 Administration Building.	1.17	LS	\$62,712,500	\$73,373,625	" " " " " " " " "
C2 Structured Parking.	1.17	LS	\$12,285,000	\$14,373,450	" " " " " " " " "
\$178,566,250			<b>C2, Harrison Avenue, Option 2:</b>	<b>\$208,922,513</b>	

**NOTES:**

The above Option Summary Breakdown costs include a 25% contractor's mark-up to cover general requirements, overhead & profit, bond & insurance, and B & O tax, plus a modest design contingency and sustainability premium. Also included are premiums for anticipated construction cost escalation as listed in the second column above. Soft costs, such as sales tax, permits, third party testing, design fees, furnishings, and owner's administration and moving costs are not included.

**A BIG PICTURE ESTIMATE ASSESSMENT:**

New finished building costs make up the bulk of the estimate bottom-line. Since each Option is figured to have the same total of new finished building floor area and same general type of construction, the balance of other variables are going to be relatively small. This is why the range of all the nine Options listed fits within a 20% cost range when not taking into account the additional 45,000 SF of finished floor space in Options B1 and B2. Perhaps the most significant variables beyond B1 and B2 building floor space is the amount of Structure Parking required in each Option, and the site scope in Options C1 and C2.



# THURSTON COUNTY COURTHOUSE and CIVIC CENTER

## A COMPARATIVE OPTION ANALYSIS

	Quantity	Unit	\$\$\$	Est. Cost	
<b>A1 DETAILS, HILLTOP CAMPUS, OPTION 1:</b>					
<b>Phase 1, South Campus Building Demolition:</b>					
Demo existing 2-story building.	18,000.00	SF	\$5.50	\$99,000	Slab-on-grade, framed, no basement.
Demo existing southwest single story building.	10,000.00	SF	\$6.00	\$60,000	" " " " " " " " " "
Demo existing northeast single story building.	4,200.00	SF	\$6.00	\$25,200	" " " " " " " " " "
Add for possible hazardous material abatement.	1.00	LS	\$70,000.00	\$70,000	A relatively minor allowance in both existing buildings.
Add for 25% General Contractor Mark-Ups:	0.25	LS	\$254,200	\$63,550	General requirements, overhead & profit, modest contingency.
<i>Phase 1, South Campus Building Demolition, With 25% G.C. Mark-Ups:</i>					<i>\$317,750.00</i>
<b>Phase 2, South Campus Site Development:</b>					
Site demolition.	60,000.00	SF	\$2.50	\$150,000	Primarily paving, fixtures, and outside utilities.
Erosion control, site clearing & subgrade prep.	105,000.00	SF	\$2.00	\$210,000	Strippings, good soils, nil slopes, a close cut/fill site.
Onsite utilities.	1.00	LS	\$500,000.00	\$500,000	All new onsite services, available from street edges.
Add for offsite utility connections.	1.00	LS	\$50,000.00	\$50,000	Includes tie-ins, minor cuts & patch only, not utility co. work.
New campus emergency back-up generator.	1.00	LS	\$200,000.00	\$200,000	Allowance, able to run full south campus power loads.
Add for storm treatment.	1.00	LS	\$120,000.00	\$120,000	Allowance for some filters and rain gardens, relatively minor.
Add for site lighting.	1.00	LS	\$130,000.00	\$130,000	Mostly pedestrian type lighting around new buildings.
Site fixtures & specialties.	1.00	LS	\$120,000.00	\$120,000	Benches, screen walls, signage, bike racks, receptacles, misc.
Site finishes.	32,000.00	SF	\$10.00	\$320,000	New paving, curbing, accents, and landscaping & irrigation.
Road edge work.	900.00	LF	\$400.00	\$360,000	C&G, sidewalk, 1 new lane, utility revamp, traffic control, misc.
New vehicular & pedestrian street.	400.00	LF	\$800.00	\$320,000	A west side, Lakeridge Way SW converted to a street & path.
Add for 25% General Contractor Mark-Ups:	0.25	LS	\$2,480,000	\$620,000	General requirements, overhead & profit, modest contingency.
<i>Phase 2, South Campus Site Development, With 25% G.C. Mark-Ups:</i>					<i>\$3,100,000.00</i>
<b>Phase 2, New Courthouse Building:</b>					
Courthouse building, above ground, complete.	147,000.00	SF	\$250.00	\$36,750,000	4 levels above ground plus basement, Class "A" office baseline.
Add for MEP penthouse(s).	5,000.00	SF	\$150.00	\$750,000	MEP penthouse with mostly utility room type finishes.
Add for courtroom & holding area premiums.	147,000.00	SF	\$110.00	\$16,170,000	Primarily finish and security upgrades in most of spaces.
Add for underground basement level.	21,000.00	SF	\$150.00	\$3,150,000	Parking, Sally Port, Holding, mechanical & utility spaces.
Add for Silver LEED premiums.	147,000.00	SF	\$30.00	\$4,410,000	Energy, sustainability and certification add-ons.
Add for 25% General Contractor Mark-Ups:	0.25	LS	\$61,230,000	\$15,307,500	General requirements, overhead & profit, modest contingency.
<i>Phase 2, New Courthouse Building, With 25% G.C. Mark-Ups:</i>					<i>\$76,537,500.00</i>
<b>Phase 2, New Lobby Wing:</b>					
Lobby building wing, above ground, complete.	11,500.00	SF	\$280.00	\$3,220,000	2 levels above ground, Class "A", with a high degree of glazing.
Add for Silver LEED premiums.	11,500.00	SF	\$30.00	\$345,000	Energy, sustainability and certification add-ons.
Add for 25% General Contractor Mark-Ups:	0.25	LS	\$3,565,000	\$891,250	General requirements, overhead & profit, modest contingency.
<i>Phase 2 New Lobby Wing, With 25% G.C. Mark-Ups:</i>					<i>\$4,456,250.00</i>
<b>Phase 2, South Structured Parking &amp; Connector:</b>					
Slab-on-grade parking, complete.	38,000.00	SF	\$20.00	\$760,000	Used as a footprint baseline.
Above grade parking levels, complete.	152,000.00	SF	\$80.00	\$12,160,000	Three levels, no roof, semi open concrete or composite steel.
Add for connector to Courthouse, complete.	1,200.00	SF	\$250.00	\$300,000	On-grade, single-story, finished interiors, roughly 40' x 30'.
Add for Platinum LEED premiums.	2,800.00	SF	\$30.00	\$84,000	Allowance at connector portion only.
Add for 25% General Contractor Mark-Ups:	0.25	LS	\$13,304,000	\$3,326,000	General requirements, overhead & profit, modest contingency.
<i>Phase 2, South Structured Parking &amp; Connector, With 25% G.C. Mark-Ups:</i>					<i>\$16,630,000.00</i>
<b>Phase 3B, Building 1, 2 &amp; 3 Demolition:</b>					
Main Courthouse demolition.	80,000.00	SF	\$6.50	\$520,000	Mostly two above ground floors and a partial basement.
North Courthouse demolition.	40,000.00	SF	\$6.00	\$240,000	Two above ground floors, possibly done in Phase 5.
South Courthouse demolition.	35,000.00	SF	\$6.00	\$210,000	Mostly two above ground floors, possible done in Phase 5.
Add for possible hazardous material abatement.	1.00	LS	\$200,000.00	\$200,000	Allowance, assumed to be not too extensive.
Protect existing adjoining buildings.	1.00	LS	\$20,000.00	\$20,000	Allowance between Phase 3B and Phase 4.
Add for 25% General Contractor Mark-Ups:	0.25	LS	\$1,190,000	\$297,500	General requirements, overhead & profit, modest contingency.
<i>Phase 3B, Building 1, 2 &amp; 3 Demolition, With 25% G.C. Mark-Ups:</i>					<i>\$1,487,500.00</i>
<b>A1 DETAILS, HILLTOP CAMPUS, OPTION 1, continues on the next page.</b>					

	Quantity	Unit	\$\$\$	Est. Cost	
<b>A1 DETAILS, HILLTOP CAMPUS, OPTION 1:</b>					
(Continued from the previous page.)					
<b><u>Phase 4, North Campus Site Development:</u></b>					
Site demolition.	60,000.00	SF	\$2.50	\$150,000	Primarily paving, fixtures, and outside utilities.
Erosion control, site clearing & subgrade prep.	175,000.00	SF	\$3.00	\$525,000	Strippings, good soils, steep slopes, possibly a close cut/fill site.
Add for extensive hillside cut & fill premiums.	1.00	LS	\$280,000.00	\$280,000	Allowance at south, east & north sides of site improvements.
Add for onsite retaining walls.	1,000.00	LF	\$500.00	\$500,000	" " " " " " " " " " " " " " " " " "
Onsite utilities.	1.00	LS	\$500,000.00	\$500,000	All new onsite services, available from west side of site.
Add for offsite utility connections.	1.00	LS	\$60,000.00	\$60,000	Includes tie-ins, minor cuts & patch only, not utility co. work.
New campus emergency back-up generator.	1.00	LS	\$150,000.00	\$150,000	Allowance, able to run full north campus power loads.
Add for storm treatment.	1.00	LS	\$160,000.00	\$160,000	Allowance for some filters and rain gardens.
Add for site lighting.	1.00	LS	\$120,000.00	\$120,000	Mostly pedestrian type lighting around new buildings.
Site fixtures & specialties.	1.00	LS	\$130,000.00	\$130,000	Benches, screen walls, signage, bike racks, receptacles, misc.
Site finishes.	100,000.00	SF	\$10.00	\$1,000,000	New paving, curbing, accents, and landscaping & irrigation.
Site improvement periphery restoration.	1,600.00	LF	\$60.00	\$96,000	Allowance, primarily at existing boundary paving that remains.
Phasing scope underlap & overlap premiums.	1.00	LS	\$600,000.00	\$600,000	Allowance to maintain operability between Phases 3B through 5.
Add for 25% General Contractor Mark-Ups:	0.25	LS	\$4,271,000	\$1,067,750	General requirements, overhead & profit, modest contingency.
<i>Phase 4, North Campus Site Development, With 25% G.C. Mark-Ups:</i>					\$5,338,750.00
<b><u>Phase 4, North Structured Parking:</u></b>					
Slab-on-grade parking, complete.	22,000.00	SF	\$20.00	\$440,000	Used as a footprint baseline.
Above grade parking levels, complete.	66,000.00	SF	\$85.00	\$5,610,000	Three levels, no roof, semi open concrete or composite steel.
Add for 25% General Contractor Mark-Ups:	0.25	LS	\$6,050,000	\$1,512,500	General requirements, overhead & profit, modest contingency.
<i>Phase 4, North Structured Parking &amp; Connector, With 25% G.C. Mark-Ups:</i>					\$7,562,500.00
<b><u>Phase 5, New Administration Buildings:</u></b>					
Administration building, above ground, complete.	176,500.00	SF	\$260.00	\$45,890,000	3 levels above ground, Class "A" office as a baseline.
Add for MEP penthouse(s).	5,000.00	SF	\$150.00	\$750,000	MEP penthouse with mostly utility room type finishes.
Add for Silver LEED premiums.	176,500.00	SF	\$30.00	\$5,295,000	Energy, sustainability and certification add-ons.
Add for 25% General Contractor Mark-Ups:	0.25	LS	\$51,935,000	\$12,983,750	General requirements, overhead & profit, modest contingency.
<i>Phase 5, New Administration Buildings, With 25% G.C. Mark-Ups:</i>					\$64,918,750.00
				\$180,349,000	\$180,349,000

A \$600,000 site allowance phasing premium is included in the Phase 4 Site Development costs for scope underlaps and overlaps between Phases 3B to 5.



# **THURSTON COUNTY COURTHOUSE and CIVIC CENTER** A COMPARATIVE OPTION ANALYSIS

	Quantity	Unit	\$\$\$	Est. Cost	
<b>A2 DETAILS, HILLTOP CAMPUS, OPTION 2:</b>					
<b>Phase 1, Building 3 Demolition:</b>					
Main Courthouse demolition.	80,000.00	SF	\$6.50	\$520,000	Mostly two above ground floors and a partial basement.
Add for possible hazardous material abatement.	1.00	LS	\$100,000.00	\$100,000	Allowance, assumed to be not too extensive.
Protect existing adjoining buildings.	1.00	LS	\$20,000.00	\$20,000	Allowance.
Add for 25% General Contractor Mark-Ups:	0.25	LS	\$640,000	\$160,000	General requirements, overhead & profit, modest contingency.
<i>Phase 1, Building 3 Demolition, With 25% G.C. Mark-Ups:</i>					<i>\$800,000.00</i>
<b>Phase 2, Initial Site Development:</b>					
Site demolition.	50,000.00	SF	\$2.50	\$125,000	Primarily paving, fixtures, and outside utilities.
Erosion control, site clearing & subgrade prep.	100,000.00	SF	\$3.00	\$300,000	Strippings, good soils, steep slopes, possibly a close cut/fill site.
Add for extensive hillside cut & fill premiums.	1.00	LS	\$200,000.00	\$200,000	Allowance at south, east & north sides of site improvements.
Add for onsite retaining walls.	600.00	LF	\$500.00	\$300,000	" " " " " " " " " " " " " " " " " "
Onsite utilities.	1.00	LS	\$350,000.00	\$350,000	All new onsite services, available from west side of site.
Add for offsite utility connections.	1.00	LS	\$60,000.00	\$60,000	Includes tie-ins, minor cuts & patch only, not utility co. work.
New campus emergency back-up generator.	1.00	LS	\$150,000.00	\$150,000	Allowance, able to run full north campus power loads.
Add for storm treatment.	1.00	LS	\$160,000.00	\$160,000	Allowance for some filters and rain gardens.
Add for site lighting.	1.00	LS	\$120,000.00	\$120,000	Mostly pedestrian type lighting around new buildings.
Site fixtures & specialties.	1.00	LS	\$130,000.00	\$130,000	Benches, screen walls, signage, bike racks, receptacles, misc.
Site finishes.	42,000.00	SF	\$10.00	\$420,000	New paving, curbing, accents, and landscaping & irrigation.
Site improvement periphery restoration.	800.00	LF	\$60.00	\$48,000	Allowance, primarily at existing boundary paving that remains.
Phasing scope underlap & overlap premiums.	1.00	LS	\$100,000.00	\$100,000	Allowance to maintain operability between Phases 2 and 3.
Add for 25% General Contractor Mark-Ups:	0.25	LS	\$2,463,000	\$615,750	General requirements, overhead & profit, modest contingency.
<i>Phase 2, Initial Site Development, With 25% G.C. Mark-Ups:</i>					<i>\$3,078,750.00</i>
<b>Phase 2, New Courthouse Building:</b>					
Courthouse building, above ground, complete.	147,000.00	SF	\$250.00	\$36,750,000	5 levels above ground plus basement, Class "A" office baseline.
Add for MEP penthouse(s).	5,000.00	SF	\$150.00	\$750,000	MEP penthouse with mostly utility room type finishes.
Add for courtroom & holding area premiums.	147,000.00	SF	\$110.00	\$16,170,000	Primarily finish and security upgrades in most of spaces.
Add for underground basement level.	21,000.00	SF	\$150.00	\$3,150,000	Parking, Sally Port, Holding, mechanical & utility spaces.
Add for Silver LEED premiums.	147,000.00	SF	\$30.00	\$4,410,000	Energy, sustainability and certification add-ons.
Add for 25% General Contractor Mark-Ups:	0.25	LS	\$61,230,000	\$15,307,500	General requirements, overhead & profit, modest contingency.
<i>Phase 2, New Courthouse Building, With 25% G.C. Mark-Ups:</i>					<i>\$76,537,500.00</i>
<b>Phase 2, Structured Parking:</b>					
Slab-on-grade parking, complete.	27,000.00	SF	\$20.00	\$540,000	Used as a footprint baseline.
Above grade parking levels, complete.	162,000.00	SF	\$80.00	\$12,960,000	Six levels, no roof, semi open concrete or composite steel.
Add for 25% General Contractor Mark-Ups:	0.25	LS	\$13,500,000	\$3,375,000	General requirements, overhead & profit, modest contingency.
<i>Phase 2, Structured Parking, With 25% G.C. Mark-Ups:</i>					<i>\$16,875,000.00</i>
<b>Phase 3, Building 2 Demolition:</b>					
South Courthouse demolition.	35,000.00	SF	\$6.00	\$210,000	Mostly two above ground floors.
Add for possible hazardous material abatement.	1.00	LS	\$50,000.00	\$50,000	Allowance, assumed to be not too extensive.
Add for 25% General Contractor Mark-Ups:	0.25	LS	\$260,000	\$65,000	General requirements, overhead & profit, modest contingency.
<i>Phase 3, Building 2 Demolition, With 25% G.C. Mark-Ups:</i>					<i>\$325,000.00</i>
<b>Phase 3, South Site Development:</b>					
Site demolition.	40,000.00	SF	\$2.50	\$100,000	Primarily paving, fixtures, and outside utilities.
Erosion control, site clearing & subgrade prep.	100,000.00	SF	\$3.00	\$300,000	Strippings, good soils, steep slopes, possibly a close cut/fill site.
Add for extensive hillside cut & fill premiums.	1.00	LS	\$200,000.00	\$200,000	Allowance at south, east & north sides of site improvements.
Add for onsite retaining walls.	300.00	LF	\$500.00	\$150,000	" " " " " " " " " " " " " " " " " "
Onsite utilities.	1.00	LS	\$250,000.00	\$250,000	All new onsite services, available from west side of site.
Add for offsite utility connections.	1.00	LS	\$50,000.00	\$50,000	Includes tie-ins, minor cuts & patch only, not utility co. work.
New campus emergency back-up generator.	1.00	LS	\$150,000.00	\$150,000	Allowance, able to run full north campus power loads.
Add for storm treatment.	1.00	LS	\$160,000.00	\$160,000	Allowance for some filters and rain gardens.
Add for site lighting.	1.00	LS	\$120,000.00	\$120,000	Mostly pedestrian type lighting around new buildings.
Site fixtures & specialties.	1.00	LS	\$130,000.00	\$130,000	Benches, screen walls, signage, bike racks, receptacles, misc.
Site finishes.	51,000.00	SF	\$10.00	\$510,000	New paving, curbing, accents, and landscaping & irrigation.
Site improvement periphery restoration.	1,000.00	LF	\$70.00	\$70,000	Allowance, primarily at existing boundary paving that remains.
Add for 25% General Contractor Mark-Ups:	0.25	LS	\$2,190,000	\$547,500	General requirements, overhead & profit, modest contingency.
<i>Phase 3, South Site Development, With 25% G.C. Mark-Ups:</i>					<i>\$2,737,500.00</i>
<b>A2 DETAILS, HILLTOP CAMPUS, OPTION 2, continues on the next page.</b>					

	Quantity	Unit	\$\$\$	Est. Cost	
<b>A2 DETAILS, HILLTOP CAMPUS, OPTION 2:</b>					
(Continued from the previous page.)					
<b><u>Phase 3, New Lobby Wing:</u></b>					
Lobby building wing, above ground, complete.	11,500.00	SF	\$280.00	\$3,220,000	2 levels above ground, Class "A", with a high degree of glazing.
Add for radius design premiums.	11,500.00	SF	\$24.00	\$276,000	A radius footprint per plan.
Add for Silver LEED premiums.	11,500.00	SF	\$30.00	\$345,000	Energy, sustainability and certification add-ons.
Add for 25% General Contractor Mark-Ups:	0.25	LS	\$3,841,000	\$960,250	General requirements, overhead & profit, modest contingency.
<i>Phase 3 New Lobby Wing, With 25% G.C. Mark-Ups:</i>					<i>\$4,801,250.00</i>
<b><u>Phase 3, New Administration Building:</u></b>					
Administration building, above ground, complete.	176,500.00	SF	\$250.00	\$44,125,000	7 levels above ground, Class "A" office as a baseline.
Add for MEP penthouse(s).	5,000.00	SF	\$150.00	\$750,000	MEP penthouse with mostly utility room type finishes.
Add for Silver LEED premiums.	176,500.00	SF	\$30.00	\$5,295,000	Energy, sustainability and certification add-ons.
Add for 25% General Contractor Mark-Ups:	0.25	LS	\$50,170,000	\$12,542,500	General requirements, overhead & profit, modest contingency.
<i>Phase 3, New Administration Building, With 25% G.C. Mark-Ups:</i>					<i>\$62,712,500.00</i>
<b><u>Phase 4, Building 1 Demolition:</u></b>					
North Courthouse demolition.	40,000.00	SF	\$6.00	\$240,000	Two above ground floors.
Add for possible hazardous material abatement.	1.00	LS	\$60,000.00	\$60,000	Allowance, assumed to be not too extensive.
Add for 25% General Contractor Mark-Ups:	0.25	LS	\$300,000	\$75,000	General requirements, overhead & profit, modest contingency.
<i>Phase 4, Building 1 Demolition, With 25% G.C. Mark-Ups:</i>					<i>\$375,000.00</i>
<b><u>Phase 4, North Site Development:</u></b>					
Site demolition.	20,000.00	SF	\$2.50	\$50,000	Primarily paving, fixtures, and outside utilities.
Erosion control, site clearing & subgrade prep.	60,000.00	SF	\$3.00	\$180,000	Strippings, good soils, steep slopes, possibly a close cut/fill site.
Add for extensive hillside cut & fill premiums.	1.00	LS	\$80,000.00	\$80,000	Allowance at south, east & north sides of site improvements.
Add for onsite retaining walls.	200.00	LF	\$500.00	\$100,000	" " " " " " " " " " " " " " " "
Onsite utilities.	1.00	LS	\$100,000.00	\$100,000	Primarily storm, and some minor power.
Add for offsite utility connections.	1.00	LS	\$5,000.00	\$5,000	Includes tie-ins, minor cuts & patch only, not utility co. work.
Add for storm treatment.	1.00	LS	\$50,000.00	\$50,000	Allowance for some filters and rain gardens.
Add for site lighting.	1.00	LS	\$60,000.00	\$60,000	Mostly pedestrian type lighting around new buildings.
Site fixtures & specialties.	1.00	LS	\$30,000.00	\$30,000	Benches, screen walls, signage, bike racks, receptacles, misc.
Site finishes.	60,000.00	SF	\$9.00	\$540,000	New paving, curbing, accents, and landscaping & irrigation.
Site improvement periphery restoration.	300.00	LF	\$60.00	\$18,000	Allowance, primarily at existing boundary paving that remains.
Phasing scope underlap & overlap premiums.	1.00	LS	\$40,000.00	\$40,000	Allowance to maintain operability between Phases 3 and 4.
Add for 25% General Contractor Mark-Ups:	0.25	LS	\$1,253,000	\$313,250	General requirements, overhead & profit, modest contingency.
<i>Phase 4, North Site Development, With 25% G.C. Mark-Ups:</i>					<i>\$1,566,250.00</i>
				<i>\$169,808,750</i>	<i>\$169,808,750</i>

Minimal street edge work helps reduce site costs as well, though extensive earthwork cuts/fills and retaining walls are included in this Option.



# THURSTON COUNTY COURTHOUSE and CIVIC CENTER

## A COMPARATIVE OPTION ANALYSIS

	Quantity	Unit	\$\$\$	Est. Cost	
<b>A3 DETAILS, HILLTOP CAMPUS, OPTION 3:</b>					
<b>Phase 1, Building 3 Demolition:</b>					
Main Courthouse demolition.	80,000.00	SF	\$6.50	\$520,000	Mostly two above ground floors and a possible partial basement.
Add for possible hazardous material abatement.	1.00	LS	\$100,000.00	\$100,000	Allowance, assumed to be not too extensive.
Protect existing adjoining buildings.	1.00	LS	\$20,000.00	\$20,000	Allowance.
Add for 25% General Contractor Mark-Ups:	0.25	LS	\$640,000	\$160,000	General requirements, overhead & profit, modest contingency.
<i>New Courthouse Site Work, With 25% G.C. Mark-Ups:</i>					<i>\$800,000.00</i>
<b>Phase 2, Initial Site Development:</b>					
Site demolition.	120,000.00	SF	\$2.50	\$300,000	Primarily paving, fixtures, and outside utilities.
Erosion control, site clearing & subgrade prep.	250,000.00	SF	\$3.00	\$750,000	Strippings, good soils, steep slopes, possibly a close cut/fill site.
Add for extensive hillside cut & fill premiums.	1.00	LS	\$200,000.00	\$200,000	Allowance at south, east & north sides of site improvements.
Add for onsite retaining walls.	1,000.00	LF	\$500.00	\$500,000	" " " " " " " " " " " " " " " " " "
Onsite utilities.	1.00	LS	\$900,000.00	\$900,000	All new onsite services, available from west side of site.
Add for offsite utility connections.	1.00	LS	\$60,000.00	\$60,000	Includes tie-ins, minor cuts & patch only, not utility co. work.
New campus emergency back-up generator.	1.00	LS	\$300,000.00	\$300,000	Allowance, able to run full campus power loads.
Add for storm treatment.	1.00	LS	\$250,000.00	\$250,000	Allowance for some filters and rain gardens.
Add for site lighting.	1.00	LS	\$180,000.00	\$180,000	Mostly pedestrian type lighting around new buildings.
Site fixtures & specialties.	1.00	LS	\$160,000.00	\$160,000	Benches, screen walls, signage, bike racks, receptacles, misc.
Site finishes.	152,000.00	SF	\$10.00	\$1,520,000	New paving, curbing, accents, and landscaping & irrigation.
Site improvement periphery restoration.	2,500.00	LF	\$50.00	\$125,000	Allowance, at existing boundary edges.
Phasing scope underlap & overlap premiums.	1.00	LS	\$80,000.00	\$80,000	Allowance to maintain operability between Phases 2 and 3.
Add for 25% General Contractor Mark-Ups:	0.25	LS	\$5,325,000	\$1,331,250	General requirements, overhead & profit, modest contingency.
<i>Phase 2, Initial Site Development, With 25% G.C. Mark-Ups:</i>					<i>\$6,656,250.00</i>
<b>Phase 2, New Courthouse Building:</b>					
Courthouse building, above ground, complete.	147,000.00	SF	\$250.00	\$36,750,000	4 levels above ground plus basement, Class "A" office baseline.
Add for MEP penthouse(s).	5,000.00	SF	\$150.00	\$750,000	MEP penthouse with mostly utility room type finishes.
Add for courtroom & holding area premiums.	147,000.00	SF	\$110.00	\$16,170,000	Primarily finish and security upgrades in most of spaces.
Add for underground basement level.	21,000.00	SF	\$150.00	\$3,150,000	Parking, Sally Port, Holding, mechanical & utility spaces.
Add for curved footprint premiums.	147,000.00	SF	\$20.00	\$2,940,000	The long front and back sides are curved.
Add for Silver LEED premiums.	147,000.00	SF	\$30.00	\$4,410,000	Energy, sustainability and certification add-ons.
Add for 25% General Contractor Mark-Ups:	0.25	LS	\$64,170,000	\$16,042,500	General requirements, overhead & profit, modest contingency.
<i>Phase 2, New Courthouse Building, With 25% G.C. Mark-Ups:</i>					<i>\$80,212,500.00</i>
<b>Phase 2, New Lobby Wing:</b>					
Lobby building wing, above ground, complete.	11,500.00	SF	\$280.00	\$3,220,000	2 levels above ground, Class "A", with a high degree of glazing.
Add for angled design premiums.	11,500.00	SF	\$10.00	\$115,000	Based on footprint layout.
Add for Silver LEED premiums.	11,500.00	SF	\$30.00	\$345,000	Energy, sustainability and certification add-ons.
Add for 25% General Contractor Mark-Ups:	0.25	LS	\$3,680,000	\$920,000	General requirements, overhead & profit, modest contingency.
<i>Phase 2 New Lobby Wing, With 25% G.C. Mark-Ups:</i>					<i>\$4,600,000.00</i>
<b>Phase 2, New Administration Building:</b>					
Administration building, above ground, complete.	176,500.00	SF	\$250.00	\$44,125,000	5 levels above ground, Class "A" office as a baseline.
Add for MEP penthouse(s).	5,000.00	SF	\$150.00	\$750,000	MEP penthouse with mostly utility room type finishes.
Add for curved footprint premiums.	176,500.00	SF	\$20.00	\$3,530,000	The long front and back sides are curved.
Add for Silver LEED premiums.	176,500.00	SF	\$30.00	\$5,295,000	Energy, sustainability and certification add-ons.
Add for 25% General Contractor Mark-Ups:	0.25	LS	\$53,700,000	\$13,425,000	General requirements, overhead & profit, modest contingency.
<i>Phase 2, New Administration Building, With 25% G.C. Mark-Ups:</i>					<i>\$67,125,000.00</i>
<b>Phase 2, North Structured Parking:</b>					
Slab-on-grade parking, complete.	43,200.00	SF	\$20.00	\$864,000	Used as a footprint baseline.
Above grade parking levels, complete.	129,600.00	SF	\$80.00	\$10,368,000	Three levels, no roof, semi open concrete or composite steel.
Add for 25% General Contractor Mark-Ups:	0.25	LS	\$11,232,000	\$2,808,000	General requirements, overhead & profit, modest contingency.
<i>Phase 2, North Structured Parking, With 25% G.C. Mark-Ups:</i>					<i>\$14,040,000.00</i>
<b>A3 DETAILS, HILLTOP CAMPUS, OPTION 3, continues on the next page.</b>					

# THURSTON COUNTY COURTHOUSE and CIVIC CENTER

## A COMPARATIVE OPTION ANALYSIS

	Quantity	Unit	\$\$\$	Est. Cost	
<b>A3 DETAILS, HILLTOP CAMPUS, OPTION 3:</b>					
<b>(Continued from the previous page.)</b>					
<b><u>Phase 3, Building 1 &amp; 2 Demolition:</u></b>					
North Courthouse demolition.	40,000.00	SF	\$6.00	\$240,000	Two above ground floors.
South Courthouse demolition.	35,000.00	SF	\$6.00	\$210,000	Mostly two above ground floors.
Add for possible hazardous material abatement.	1.00	LS	\$100,000.00	\$100,000	Allowance, assumed to be not too extensive.
Add for 25% General Contractor Mark-Ups:	0.25	LS	\$550,000	\$137,500	General requirements, overhead & profit, modest contingency.
<i>Phase 3, Building 3 Demolition, With 25% G.C. Mark-Ups:</i>				<i>\$687,500.00</i>	
<b><u>Phase 3, Balance of Site Development:</u></b>					
Site demolition.	50,000.00	SF	\$2.50	\$125,000	Primarily paving, fixtures, and outside utilities.
Erosion control, site clearing & subgrade prep.	150,000.00	SF	\$3.00	\$450,000	Strippings, good soils, steep slopes, possibly a close cut/fill site.
Add for extensive hillside cut & fill premiums.	1.00	LS	\$200,000.00	\$200,000	Allowance at south, east & north sides of site improvements.
Add for onsite retaining walls.	500.00	LF	\$500.00	\$250,000	" " " " " " " " " " " " " " " "
Onsite utilities.	1.00	LS	\$200,000.00	\$200,000	Primarily storm, fire sprinkler and power.
Add for offsite utility connections.	1.00	LS	\$20,000.00	\$20,000	Includes tie-ins, minor cuts & patch only, not utility co. work.
Add for storm treatment.	1.00	LS	\$100,000.00	\$100,000	Allowance for some filters and rain gardens.
Add for site lighting.	1.00	LS	\$80,000.00	\$80,000	Mostly pedestrian type lighting around new buildings.
Site fixtures & specialties.	1.00	LS	\$60,000.00	\$60,000	Benches, screen walls, signage, bike racks, receptacles, misc.
Site finishes.	137,000.00	SF	\$9.50	\$1,301,500	New paving, curbing, accents, and landscaping & irrigation.
Site improvement periphery restoration.	2,000.00	LF	\$50.00	\$100,000	Allowance, at existing boundary edges.
Phasing scope underlap & overlap premiums.	1.00	LS	\$120,000.00	\$120,000	Allowance to maintain operability between Phases 2 and 3.
Add for 25% General Contractor Mark-Ups:	0.25	LS	\$3,006,500	\$751,625	General requirements, overhead & profit, modest contingency.
<i>Phase 3, Balance of Site Development, With 25% G.C. Mark-Ups:</i>				<i>\$3,758,125.00</i>	
<b><u>Phase 3, South Structured Parking &amp; Connector:</u></b>					
Slab-on-grade parking, complete.	27,000.00	SF	\$20.00	\$540,000	Used as a footprint baseline.
Above grade parking levels, complete.	54,000.00	SF	\$80.00	\$4,320,000	Two levels, no roof, semi open concrete or composite steel.
Add for connector to Courthouse, complete.	2,800.00	SF	\$240.00	\$672,000	On-grade, single-story, finished interiors, roughly 40' x 30'.
Add for Platinum LEED premiums.	2,800.00	SF	\$30.00	\$84,000	Allowance at connector portion only.
Add for 25% General Contractor Mark-Ups:	0.25	LS	\$5,616,000	\$1,404,000	General requirements, overhead & profit, modest contingency.
<i>Phase 3, South Structured Parking &amp; Connector, With 25% G.C. Mark-Ups:</i>				<i>\$7,020,000.00</i>	
				<i>\$184,899,375</i>	<i>\$184,899,375</i>

**A3) HILLTOP CAMPUS OPTION 3 NOTES:**

The same general scope assumptions listed in the A1 Hilltop Option 1 Notes apply here.

As with Option A2, no South Campus work is figured in this Hilltop Option 3.

Site development for the North Structured Parking is figured to take place with the Courthouse, Lobby & Administration site work in Phase 2.

Curved footprint and angled premiums are included in new building costs.

Though this Option's 3-D drawing implies the Lobby space may be six levels high, it is figured to be two levels high.

40,000 SF of site development and accompanying building demolition noted at the southeast portion of a Phase 3 plan is figured to take place in Phase 2 for the North Structured Parking.

A connector between the two new Structured Parking footprints is figured to be on-grade and single-story only.



	Quantity	Unit	\$\$\$	Est. Cost	
<b>A4 DETAILS, HILLTOP CAMPUS, OPTION 4:</b>					
<b><u>Phase 1, Building 3 Demolition:</u></b>					
Main Courthouse demolition.	80,000.00	SF	\$6.50	\$520,000	Mostly two above ground floors and a possible partial basement.
Demo south portion of Building	5,000.00	SF	\$6.00	\$30,000	An above ground single-story structure.
Add for possible hazardous material abatement.	1.00	LS	\$110,000.00	\$110,000	Allowance, assumed to be not too extensive.
Protect existing adjoining buildings.	1.00	LS	\$10,000.00	\$10,000	Allowance.
Add for 25% General Contractor Mark-Ups:	0.25	LS	\$670,000	\$167,500	General requirements, overhead & profit, modest contingency.
<i>Phase 1, Building 3 Demolition, With 25% G.C. Mark-Ups:</i>				<i>\$837,500.00</i>	
<b><u>Phase 2, Initial Site Development:</u></b>					
Site demolition.	80,000.00	SF	\$2.50	\$200,000	Primarily paving, fixtures, and outside utilities.
Erosion control, site clearing & subgrade prep.	160,000.00	SF	\$3.00	\$480,000	Strippings, good soils, steep slopes, possibly a close cut/fill site.
Add for extensive hillside cut & fill premiums.	1.00	LS	\$300,000.00	\$300,000	Allowance at south, east & north sides of site improvements.
Add for onsite retaining walls.	1,000.00	LF	\$500.00	\$500,000	" " " " " " " " " " " " " " " "
Onsite utilities.	1.00	LS	\$800,000.00	\$800,000	All new onsite services, available from west side of site.
Add for offsite utility connections.	1.00	LS	\$60,000.00	\$60,000	Includes tie-ins, minor cuts & patch only, not utility co. work.
New campus emergency back-up generator.	1.00	LS	\$300,000.00	\$300,000	Allowance, able to run full campus power loads.
Add for storm treatment.	1.00	LS	\$250,000.00	\$250,000	Allowance for some filters and rain gardens.
Add for site lighting.	1.00	LS	\$150,000.00	\$150,000	Mostly pedestrian type lighting around new buildings.
Site fixtures & specialties.	1.00	LS	\$120,000.00	\$120,000	Benches, screen walls, signage, bike racks, receptacles, misc.
Site finishes.	95,000.00	SF	\$10.00	\$950,000	New paving, curbing, accents, and landscaping & irrigation.
Site improvement periphery restoration.	2,500.00	LF	\$50.00	\$125,000	Allowance, at existing boundary edges.
Phasing scope underlap & overlap premiums.	1.00	LS	\$120,000.00	\$120,000	Allowance to maintain operability between Phases 2 and 3.
Add for 25% General Contractor Mark-Ups:	0.25	LS	\$4,355,000	\$1,088,750	General requirements, overhead & profit, modest contingency.
<i>Phase 2, Initial Site Development, With 25% G.C. Mark-Ups:</i>				<i>\$5,443,750.00</i>	
<b><u>Phase 2, New Courthouse Building:</u></b>					
Courthouse building, above ground, complete.	147,000.00	SF	\$250.00	\$36,750,000	7 levels above ground plus basement, Class "A" office baseline.
Add for MEP penthouse(s).	5,000.00	SF	\$150.00	\$750,000	MEP penthouse with mostly utility room type finishes.
Add for courtroom & holding area premiums.	147,000.00	SF	\$110.00	\$16,170,000	Primarily finish and security upgrades in most of spaces.
Add for underground basement level.	21,000.00	SF	\$150.00	\$3,150,000	Parking, Sally Port, Holding, mechanical & utility spaces.
Add for curved footprint premiums.	147,000.00	SF	\$20.00	\$2,940,000	The long front and back sides are curved.
Add for Silver LEED premiums.	147,000.00	SF	\$30.00	\$4,410,000	Energy, sustainability and certification add-ons.
Add for 25% General Contractor Mark-Ups:	0.25	LS	\$64,170,000	\$16,042,500	General requirements, overhead & profit, modest contingency.
<i>Phase 2, New Courthouse Building, With 25% G.C. Mark-Ups:</i>				<i>\$80,212,500.00</i>	
<b><u>Phase 2, New Lobby Wing:</u></b>					
Lobby building wing, above ground, complete.	11,500.00	SF	\$280.00	\$3,220,000	2 levels above ground, Class "A", with a high degree of glazing.
Add for Silver LEED premiums.	11,500.00	SF	\$30.00	\$345,000	Energy, sustainability and certification add-ons.
Add for 25% General Contractor Mark-Ups:	0.25	LS	\$3,565,000	\$891,250	General requirements, overhead & profit, modest contingency.
<i>Phase 2, New Lobby Wing, With 25% G.C. Mark-Ups:</i>				<i>\$4,456,250.00</i>	
<b><u>Phase 3, North Structured Parking:</u></b>					
Slab-on-grade parking, complete.	37,800.00	SF	\$20.00	\$756,000	Used as a footprint baseline.
Above grade parking levels, complete.	226,800.00	SF	\$80.00	\$18,144,000	Six levels, no roof, semi open concrete or composite steel.
Add for 25% General Contractor Mark-Ups:	0.25	LS	\$18,900,000	\$4,725,000	General requirements, overhead & profit, modest contingency.
<i>Phase 3, North Structured Parking, With 25% G.C. Mark-Ups:</i>				<i>\$23,625,000.00</i>	
<b><u>Phase 3, Building 1 &amp; 2 Demolition:</u></b>					
North Courthouse demolition.	40,000.00	SF	\$6.00	\$240,000	Two above ground floors.
South Courthouse demolition.	30,000.00	SF	\$6.00	\$180,000	Two above ground floors, less demo's Phase 2 portion.
Add for possible hazardous material abatement.	1.00	LS	\$100,000.00	\$100,000	Allowance, assumed to be not too extensive.
Add for 25% General Contractor Mark-Ups:	0.25	LS	\$520,000	\$130,000	General requirements, overhead & profit, modest contingency.
<i>Phase 3, Building 3 Demolition, With 25% G.C. Mark-Ups:</i>				<i>\$650,000.00</i>	
<b>A4 DETAILS, HILLTOP CAMPUS, OPTION 4, continues on the next page.</b>					

# THURSTON COUNTY COURTHOUSE and CIVIC CENTER

## A COMPARATIVE OPTION ANALYSIS

	Quantity	Unit	\$\$\$	Est. Cost	
<b>A4 DETAILS, HILLTOP CAMPUS, OPTION 4:</b>					
<b>(Continued from the previous page.)</b>					
<b><u>Phase 3, Balance of North Campus Site Work:</u></b>					
Site demolition.	30,000.00	SF	\$2.50	\$75,000	Primarily paving, fixtures, and outside utilities.
Erosion control, site clearing & subgrade prep.	85,000.00	SF	\$3.00	\$255,000	Strippings, good soils, steep slopes, possibly a close cut/fill site.
Add for extensive hillside cut & fill premiums.	1.00	LS	\$120,000.00	\$120,000	Allowance at south, east & north sides of site improvements.
Add for onsite retaining walls.	400.00	LF	\$500.00	\$200,000	" " " " " " " " " " " " " " " "
Onsite utilities.	1.00	LS	\$200,000.00	\$200,000	All new onsite services, available from west side of site.
Add for offsite utility connections.	1.00	LS	\$20,000.00	\$20,000	Includes tie-ins, minor cuts & patch only, not utility co. work.
Add for storm treatment.	1.00	LS	\$150,000.00	\$150,000	Allowance for some filters and rain gardens.
Add for site lighting.	1.00	LS	\$100,000.00	\$100,000	Mostly pedestrian type lighting around new buildings.
Site fixtures & specialties.	1.00	LS	\$80,000.00	\$80,000	Benches, screen walls, signage, bike racks, receptacles, misc.
Site finishes.	50,000.00	SF	\$10.00	\$500,000	New paving, curbing, accents, and landscaping & irrigation.
Site improvement periphery restoration.	1,600.00	LF	\$50.00	\$80,000	Allowance, at existing boundary edges.
Phasing scope underlap & overlap premiums.	2.00	LS	\$50,000.00	\$100,000	Allowance to maintain operability between Phases 2 and 3.
Add for 25% General Contractor Mark-Ups:	0.25	LS	\$1,880,000	\$470,000	General requirements, overhead & profit, modest contingency.
<i>Phase 3, Balance of North Campus Site Work, With 25% G.C. Mark-Ups:</i>				\$2,350,000.00	
<b><u>Phase 3, New Administration Buildings:</u></b>					
Administration building, above ground, complete.	176,500.00	SF	\$260.00	\$45,890,000	5 levels above ground, Class "A" office as a baseline.
Add for MEP penthouse(s).	5,000.00	SF	\$150.00	\$750,000	MEP penthouse with mostly utility room type finishes.
Add for building transition tie-ins.	180.00	LF	\$500.00	\$90,000	Where Phase 3 Administrations connect to Phase 2 Lobbies.
Add for Silver LEED premiums.	176,500.00	SF	\$30.00	\$5,295,000	Energy, sustainability and certification add-ons.
Add for 25% General Contractor Mark-Ups:	0.25	LS	\$52,025,000	\$13,006,250	General requirements, overhead & profit, modest contingency.
<i>Phase 3, New Administration Buildings, With 25% G.C. Mark-Ups:</i>				\$65,031,250.00	
<b><u>Phase 4, South Campus Building Demolition:</u></b>					
Demo existing 2-story building.	18,000.00	SF	\$5.50	\$99,000	Slab-on-grade, framed, no basement.
Demo existing single-story building.	10,000.00	SF	\$6.00	\$60,000	" " " " " " " " " " " "
Add for possible hazardous material abatement.	1.00	LS	\$60,000.00	\$60,000	A relatively minor allowance in both existing buildings.
Add for 25% General Contractor Mark-Ups:	0.25	LS	\$219,000	\$54,750	General requirements, overhead & profit, modest contingency.
<i>Phase 4, South Campus Building Demolition, With 25% G.C. Mark-Ups:</i>				\$273,750.00	
<b><u>Phase 4, South Campus Site Development:</u></b>					
Site demolition.	50,000.00	SF	\$2.50	\$125,000	Primarily paving, fixtures, and outside utilities.
Erosion control, site clearing & subgrade prep.	80,000.00	SF	\$2.00	\$160,000	Strippings, good soils, nil slopes, a close cut/fill site.
Onsite utilities.	1.00	LS	\$400,000.00	\$400,000	Mostly storm and some power, available from street edges.
Add for offsite utility connections.	1.00	LS	\$40,000.00	\$40,000	Includes tie-ins, minor cuts & patch only, not utility co. work.
Add for storm treatment.	1.00	LS	\$100,000.00	\$100,000	Allowance for some filters and rain gardens, relatively minor.
Add for site lighting.	1.00	LS	\$95,000.00	\$95,000	Mostly pedestrian type lighting around new buildings.
Site fixtures & specialties.	1.00	LS	\$100,000.00	\$100,000	Benches, screen walls, signage, bike racks, receptacles, misc.
Site finishes.	80,000.00	SF	\$10.00	\$800,000	New paving, curbing, accents, and landscaping & irrigation.
Road edge work.	1,000.00	LF	\$400.00	\$400,000	C&G, sidewalk, 1 new lane, utility revamp, traffic control, misc.
Site improvement periphery restoration.	350.00	LF	\$60.00	\$21,000	Allowance, at northeast area that butts into existing..
Add for 25% General Contractor Mark-Ups:	0.25	LS	\$2,241,000	\$560,250	General requirements, overhead & profit, modest contingency.
<i>Phase 4, South Campus Site Development, With 25% G.C. Mark-Ups:</i>				\$2,801,250.00	
				\$185,681,250	\$185,681,250

**A4) HILLTOP CAMPUS OPTION 4 NOTES:**

The same general scope assumptions listed in the A1 Hilltop Option 1 Notes apply here.

Phased transition building tie-in premiums are included between the new Courthouse and Lobby buildings.

The South Campus Site Development includes roughly 3/4's of an existing block rather than the entire block, as is the case in Option A1.



# THURSTON COUNTY COURTHOUSE and CIVIC CENTER

## A COMPARATIVE OPTION ANALYSIS

	Quantity	Unit	\$\$\$	Est. Cost	
<b>A5 DETAILS, HILLTOP CAMPUS, OPTION 5:</b>					
<b>Phase 1, Building 3 Demolition:</b>					
Main Courthouse demolition.	80,000.00	SF	\$6.50	\$520,000	Mostly two above ground floors and a possible partial basement.
Demo south portion of Building	5,000.00	SF	\$6.00	\$30,000	An above ground single-story structure.
Add for possible hazardous material abatement.	1.00	LS	\$110,000.00	\$110,000	Allowance, assumed to be not too extensive.
Protect existing adjoining buildings.	1.00	LS	\$10,000.00	\$10,000	Allowance.
Add for 25% General Contractor Mark-Ups:	0.25	LS	\$670,000	\$167,500	Assumed to be limited to existing site driveway entrances.
<i>Phase 1, Building 3 Demolition, With 25% G.C. Mark-Ups:</i>				\$837,500.00	
<b>Phase 1, Initial North Site Development:</b>					
Site demolition.	35,000.00	SF	\$2.50	\$87,500	Primarily paving, fixtures, and outside utilities.
Erosion control, site clearing & subgrade prep.	70,000.00	SF	\$3.00	\$210,000	Strippings, good soils, steep slopes, possibly a close cut/fill site.
Add for extensive hillside cut & fill premiums.	1.00	LS	\$140,000.00	\$140,000	Allowance at south, east & north sides of site improvements.
Add for onsite retaining walls.	400.00	LF	\$500.00	\$200,000	" " " " " " " " " " " " " " " " " "
Onsite utilities.	1.00	LS	\$150,000.00	\$150,000	Primarily storm and some power.
Add for offsite utility connections.	1.00	LS	\$10,000.00	\$10,000	Includes tie-ins, minor cuts & patch only, not utility co. work.
Add for storm treatment.	1.00	LS	\$50,000.00	\$50,000	Allowance for some filters and rain gardens.
Add for site lighting.	1.00	LS	\$40,000.00	\$40,000	Mostly pedestrian type lighting around new buildings.
Site fixtures & specialties.	1.00	LS	\$20,000.00	\$20,000	Benches, screen walls, signage, bike racks, receptacles, misc.
Site finishes.	32,000.00	SF	\$10.00	\$320,000	New paving, curbing, accents, and landscaping & irrigation.
Site improvement periphery restoration.	900.00	LF	\$50.00	\$45,000	Allowance, at existing boundary edges.
Add for 25% General Contractor Mark-Ups:	0.25	LS	\$1,272,500	\$318,125	Assumed to be limited to existing site driveway entrances.
<i>Phase 1, Initial North Site Development, With 25% G.C. Mark-Ups:</i>				\$1,590,625.00	
<b>Phase 1, North Structured Parking:</b>					
Slab-on-grade parking, complete.	37,800.00	SF	\$20.00	\$756,000	Used as a footprint baseline.
Above grade parking levels, complete.	226,800.00	SF	\$80.00	\$18,144,000	Six levels, no roof, semi open concrete or composite steel.
Add for 25% General Contractor Mark-Ups:	0.25	LS	\$18,900,000	\$4,725,000	Assumed to be limited to existing site driveway entrances.
<i>Phase 1, North Structured Parking, With 25% G.C. Mark-Ups:</i>				\$23,625,000.00	
<b>Phase 2, Building 1 Demolition:</b>					
North Courthouse demolition.	40,000.00	SF	\$6.00	\$240,000	Two above ground floors.
Add for possible hazardous material abatement.	1.00	LS	\$60,000.00	\$60,000	Allowance, assumed to be not too extensive.
Add for 25% General Contractor Mark-Ups:	0.25	LS	\$300,000	\$75,000	Assumed to be limited to existing site driveway entrances.
<i>Phase 2, Building 1 Demolition, With 25% G.C. Mark-Ups:</i>				\$375,000.00	
<b>Phase 2, Balance of North Campus Site Work:</b>					
Site demolition.	80,000.00	SF	\$2.50	\$200,000	Primarily paving, fixtures, and outside utilities.
Erosion control, site clearing & subgrade prep.	200,000.00	SF	\$3.00	\$600,000	Strippings, good soils, steep slopes, possibly a close cut/fill site.
Add for extensive hillside cut & fill premiums.	1.00	LS	\$400,000.00	\$400,000	Allowance at south, east & north sides of site improvements.
Add for onsite retaining walls.	1,000.00	LF	\$500.00	\$500,000	" " " " " " " " " " " " " " " " " "
Onsite utilities.	1.00	LS	\$800,000.00	\$800,000	All new onsite services, available from west side of site.
New campus emergency back-up generator.	1.00	LS	\$300,000.00	\$300,000	Allowance, able to run full campus power loads.
Add for offsite utility connections.	1.00	LS	\$20,000.00	\$20,000	Includes tie-ins, minor cuts & patch only, not utility co. work.
Add for storm treatment.	1.00	LS	\$200,000.00	\$200,000	Allowance for some filters and rain gardens.
Add for site lighting.	1.00	LS	\$150,000.00	\$150,000	Mostly pedestrian type lighting around new buildings.
Site fixtures & specialties.	1.00	LS	\$120,000.00	\$120,000	Benches, screen walls, signage, bike racks, receptacles, misc.
Site finishes.	128,000.00	SF	\$9.50	\$1,216,000	New paving, curbing, accents, and landscaping & irrigation.
Site improvement periphery restoration.	1,600.00	LF	\$50.00	\$80,000	Allowance, at existing boundary edges.
Phasing scope underlap & overlap premiums.	1.00	LS	\$160,000.00	\$160,000	Allowance to maintain operability between Phases.
Add for 25% General Contractor Mark-Ups:	0.25	LS	\$4,746,000	\$1,186,500	Assumed to be limited to existing site driveway entrances.
<i>Phase 2, Balance of North Campus Work, With 25% G.C. Mark-Ups:</i>				\$5,932,500.00	
<b>A5 DETAILS, HILLTOP CAMPUS OPTION 5, continues on the next page:</b>					

# THURSTON COUNTY COURTHOUSE and CIVIC CENTER

## A COMPARATIVE OPTION ANALYSIS

	Quantity	Unit	\$\$	Est. Cost	
<b>A5 DETAILS, HILLTOP CAMPUS OPTION 5:</b>					
<b>(Continued from the previous page.)</b>					
<b><u>Phase 2, New Courthouse Building:</u></b>					
Courthouse building, above ground, complete.	147,000.00	SF	\$250.00	\$36,750,000	5 levels above ground plus basement, Class "A" office baseline.
Add for MEP penthouse(s).	5,000.00	SF	\$150.00	\$750,000	MEP penthouse with mostly utility room type finishes.
Add for courtroom & holding area premiums.	147,000.00	SF	\$110.00	\$16,170,000	Primarily finish and security upgrades in most of spaces.
Add for underground basement level.	21,000.00	SF	\$150.00	\$3,150,000	Parking, Sally Port, Holding, mechanical & utility spaces.
Add for Silver LEED premiums.	147,000.00	SF	\$30.00	\$4,410,000	Energy, sustainability and certification add-ons.
Add for 25% General Contractor Mark-Ups:	0.25	LS	\$61,230,000	\$15,307,500	Assumed to be limited to existing site driveway entrances.
<i>Phase 2, New Courthouse Building, With 25% G.C. Mark-Ups:</i>				\$76,537,500.00	
<b><u>Phase 2, New Below Grade Lobby Wing:</u></b>					
Lobby building wing, above ground, complete.	11,500.00	SF	\$270.00	\$3,105,000	1 level, mostly underground with less glazing, Class "A".
Add for Silver LEED premiums.	11,500.00	SF	\$30.00	\$345,000	Energy, sustainability and certification add-ons.
Add for 25% General Contractor Mark-Ups:	0.25	LS	\$3,450,000	\$862,500	Assumed to be limited to existing site driveway entrances.
<i>Phase 2, New Below Grade Lobby Wing, With 25% G.C. Mark-Ups:</i>				\$4,312,500.00	
<b><u>Phase 2, New Administration Building:</u></b>					
Administration building, above ground, complete.	176,500.00	SF	\$250.00	\$44,125,000	7 levels above ground, Class "A" office as a baseline.
Add for MEP penthouse(s).	5,000.00	SF	\$150.00	\$750,000	MEP penthouse with mostly utility room type finishes.
Add for Silver LEED premiums.	176,500.00	SF	\$30.00	\$5,295,000	Energy, sustainability and certification add-ons.
Add for 25% General Contractor Mark-Ups:	0.25	LS	\$50,170,000	\$12,542,500	Assumed to be limited to existing site driveway entrances.
<i>Phase 2, New Administration Building, With 25% G.C. Mark-Ups:</i>				\$62,712,500.00	
<b><u>Phase 3, Building 2 Demolition:</u></b>					
South Courthouse demolition.	35,000.00	SF	\$6.00	\$210,000	Mostly two above ground floors.
Add for possible hazardous material abatement.	1.00	LS	\$50,000.00	\$50,000	Allowance, assumed to be not too extensive.
Add for 25% General Contractor Mark-Ups:	0.25	LS	\$260,000	\$65,000	General requirements, overhead & profit, modest contingency.
<i>Phase 3, Building 3 Demolition, With 25% G.C. Mark-Ups:</i>				\$325,000.00	
<b><u>Phase 3, Balance of Site Development:</u></b>					
Site demolition.	20,000.00	SF	\$2.50	\$50,000	Primarily paving, fixtures, and outside utilities.
Erosion control, site clearing & subgrade prep.	80,000.00	SF	\$3.00	\$240,000	Strippings, good soils, steep slopes, possibly a close cut/fill site.
Add for extensive hillside cut & fill premiums.	1.00	LS	\$120,000.00	\$120,000	Allowance at south, east & north sides of site improvements.
Add for onsite retaining walls.	400.00	LF	\$500.00	\$200,000	" " " " " " " " " " " " " " " " " "
Onsite utilities.	1.00	LS	\$180,000.00	\$180,000	All new onsite services, available from west side of site.
Add for offsite utility connections.	1.00	LS	\$10,000.00	\$10,000	Includes tie-ins, minor cuts & patch only, not utility co. work.
Add for storm treatment.	1.00	LS	\$120,000.00	\$120,000	Allowance for some filters and rain gardens.
Add for site lighting.	1.00	LS	\$90,000.00	\$90,000	Mostly pedestrian type lighting around new buildings.
Site fixtures & specialties.	1.00	LS	\$75,000.00	\$75,000	Benches, screen walls, signage, bike racks, receptacles, misc.
Site finishes.	55,000.00	SF	\$10.00	\$550,000	New paving, curbing, accents, and landscaping & irrigation.
Site improvement periphery restoration.	900.00	LF	\$50.00	\$45,000	Allowance, at existing boundary edges.
Phasing scope underlap & overlap premiums.	1.00	LS	\$60,000.00	\$60,000	Allowance to maintain operability between Phases 2 and 3.
Add for 25% General Contractor Mark-Ups:	0.25	LS	\$1,740,000	\$435,000	Assumed to be limited to existing site driveway entrances.
<i>Phase 3, Balance of Site Development, With 25% G.C. Mark-Ups:</i>				\$2,175,000.00	
<b><u>Phase 3, South Structured Parking:</u></b>					
Slab-on-grade parking, complete.	25,000.00	SF	\$40.00	\$1,000,000	Used as a footprint baseline, with some retaining walls.
Above grade parking levels, complete.	75,000.00	SF	\$80.00	\$6,000,000	Three levels, no roof, semi open concrete or composite steel.
Add for 25% General Contractor Mark-Ups:	0.25	LS	\$7,000,000	\$1,750,000	General requirements, overhead & profit, modest contingency.
<i>Phase 3, South Structured Parking, With 25% G.C. Mark-Ups:</i>				\$8,750,000.00	
				\$187,173,125	\$187,173,125

**A5) HILLTOP CAMPUS OPTION 5 NOTES:**

The same general scope assumptions listed in the A1 Hilltop Option 1 Notes apply here.

Hilltop Option A5 is the only one that features a one-level Lobby area that is mostly below ground.



# THURSTON COUNTY COURTHOUSE and CIVIC CENTER

## A COMPARATIVE OPTION ANALYSIS

	Quantity	Unit	\$\$\$	Est. Cost	
<b>B1 DETAILS, PLUM STREET OPTION 1:</b>					
<b><u>B1 Site Development:</u></b>					
Courthouse/jail building demolition.	24,000.00	SF	\$8.00	\$192,000	Mostly single story structures with varying heights.
Other onsite building demolition.	8,000.00	SF	\$6.00	\$48,000	Simple single-story structures.
Add for possible hazardous material abatement.	1.00	LS	\$60,000.00	\$60,000	Allowance assumed to be relatively minor.
Site demolition.	240,000.00	SF	\$2.50	\$600,000	Primarily paving, fixtures, and outside utilities.
Erosion control, site clearing & subgrade prep.	456,000.00	SF	\$3.00	\$1,368,000	Strippings, questionable soils, nil slopes, a close cut/fill site.
Onsite utilities.	1.00	LS	\$900,000.00	\$900,000	All new onsite services, available from street edges.
Add for offsite utility connections.	1.00	LS	\$60,000.00	\$60,000	Includes tie-ins, minor cuts & patch only, not utility co. work.
New campus emergency back-up generator.	1.00	LS	\$300,000.00	\$300,000	Allowance, to be sized, able to run full campus power loads.
Add for storm treatment.	1.00	LS	\$200,000.00	\$200,000	Allowance for some filters and rain gardens.
Add for site lighting.	1.00	LS	\$200,000.00	\$200,000	Mostly pedestrian type lighting around new buildings.
Site fixtures & specialties.	1.00	LS	\$200,000.00	\$200,000	Benches, screen walls, signage, bike racks, receptacles, misc.
Site finishes.	340,000.00	SF	\$10.00	\$3,400,000	New paving, curbing, accents, and landscaping & irrigation.
Road edge work.	1,180.00	LF	\$400.00	\$472,000	C&G, sidewalk, 1 new lane, utility revamp, traffic control, misc.
Add for 25% General Contractor Mark-Ups:	0.25	LS	\$8,000,000	\$2,000,000	General requirements, overhead & profit, modest contingency.
<i>B1 Site Development, With 25% G.C. Mark-Ups:</i>				<i>\$10,000,000.00</i>	
<b><u>New B1 Courthouse Building:</u></b>					
Courthouse building, above ground, complete.	147,000.00	SF	\$250.00	\$36,750,000	7 levels above ground plus basement, Class "A" office baseline.
Additional Administrative floor space.	27,000.00	SF	\$250.00	\$6,750,000	An additional floor level, all Class "A" office space.
Add for pile foundation premium.	27,000.00	SF	\$50.00	\$1,350,000	Allowance under building footprint.
Add for MEP penthouse(s).	5,000.00	SF	\$150.00	\$750,000	MEP penthouse with mostly utility room type finishes.
Add for courtroom & holding area premiums.	147,000.00	SF	\$110.00	\$16,170,000	Primarily finish and security upgrades in most of spaces.
Add for underground basement level.	21,000.00	SF	\$160.00	\$3,360,000	Parking, Sally Port, Holding, mechanical & utility spaces.
Add for Silver LEED premiums.	174,000.00	SF	\$30.00	\$5,220,000	Energy, sustainability and certification add-ons.
Add for 25% General Contractor Mark-Ups:	0.25	LS	\$70,350,000	\$17,587,500	General requirements, overhead & profit, modest contingency.
<i>B1 Courthouse Building, With 25% G.C. Mark-Ups:</i>				<i>\$87,937,500.00</i>	
<b><u>New B1 Lobby Wing:</u></b>					
Lobby building wing, above ground, complete.	11,500.00	SF	\$280.00	\$3,220,000	2 levels above ground, Class "A", with a high degree of glazing.
Add for pile foundation premium.	5,750.00	SF	\$50.00	\$287,500	Allowance under building footprint.
Add for Silver LEED premiums.	11,500.00	SF	\$30.00	\$345,000	Energy, sustainability and certification add-ons.
Add for 25% General Contractor Mark-Ups:	0.25	LS	\$3,852,500	\$963,125	General requirements, overhead & profit, modest contingency.
<i>B1 Lobby Wing, With 25% G.C. Mark-Ups:</i>				<i>\$4,815,625.00</i>	
<b><u>New B1 Administration Building:</u></b>					
Administration building, above ground, complete.	149,500.00	SF	\$260.00	\$38,870,000	7 levels above ground, Class "A" office as a baseline.
Add for pile foundation premium.	19,000.00	SF	\$50.00	\$950,000	Allowance under half of new building foundation.
Add for MEP penthouse(s).	5,000.00	SF	\$150.00	\$750,000	MEP penthouse with mostly utility room type finishes.
Add for Silver LEED premiums.	149,500.00	SF	\$30.00	\$4,485,000	Energy, sustainability and certification add-ons.
Add for 25% General Contractor Mark-Ups:	0.25	LS	\$45,055,000	\$11,263,750	General requirements, overhead & profit, modest contingency.
<i>B1 Administration Building, With 25% G.C. Mark-Ups:</i>				<i>\$56,318,750.00</i>	
<b><u>Supplemental B1 Civic Center Space:</u></b>					
Administration building, above ground.	45,000.00	SF	\$260.00	\$11,700,000	2 additional levels above ground, Class "A" office as a baseline.
Add for courtroom & holding area premiums.	5,000.00	SF	\$120.00	\$600,000	Primarily finish and security upgrades in limited areas.
Add for jail function premiums.	12,000.00	SF	\$160.00	\$1,920,000	Isolation, hardened construction and security premiums.
Add for Silver LEED premiums.	45,000.00	SF	\$30.00	\$1,350,000	Energy, sustainability and certification add-ons.
Add for 25% General Contractor Mark-Ups:	0.25	LS	\$15,570,000	\$3,892,500	General requirements, overhead & profit, modest contingency.
<i>Supplemental B1 Civic Center Space, With 25% G.C. Mark-Ups:</i>				<i>\$19,462,500.00</i>	
<b>B1 DETAILS, PLUM STREET OPTION 2, continues on the next page:</b>					

**THURSTON COUNTY COURTHOUSE and CIVIC CENTER**  
**A COMPARATIVE OPTION ANALYSIS**

	Quantity	Unit	\$\$\$	Est. Cost	
<b>B1 DETAILS, CAMPUS STREET OPTION 1:</b>					
(Continued from the previous page.)					
<b>B1 Structured Parking:</b>					
Slab-on-grade parking, complete.	52,800.00	SF	\$20.00	\$1,056,000	Used as a footprint baseline.
Add for pile foundation premium.	52,800.00	SF	\$40.00	\$2,112,000	Allowance under building footprint.
Above grade parking levels, complete.	211,200.00	SF	\$80.00	\$16,896,000	Four levels, no roof, semi open concrete or composite steel.
Add for 25% General Contractor Mark-Ups:	0.25	LS	\$20,064,000	\$5,016,000	General requirements, overhead & profit, modest contingency.
	<i>B1 Structured Parking, With 25% G.C. Mark-Ups:</i>				<i>\$25,080,000.00</i>
				\$203,614,375	\$203,614,375

**B1) PLUM STREET OPTION 1 NOTES:**

No project phasing premiums are figured in Option B1. Construction work is to start in spring of 2022.

Per a preliminary civil report, a high water table is figured, possibly requiring dewatering premiums in basement construction.

No new traffic or street lights are figured--existing to be protected and replace in place or be reused.

New B1 Courthouse, Lobby and Administration buildings are a lone facility, broken out by cost, but to be constructed in conjunction with each other.

Pile foundation premiums, including under structured parking, and high water premiums at basement levels are figured in Plum Street Options B1 and B2 only.

B1 Structured Parking figured to have one ground level and now four elevated levels with ramps, stairs & elevators but no roof over the top general parking level.

Beyond 335,000 SF of finished building & 21,000 SF of basement floor area, Options B1 and B2 also include a supplemental 45,000 SF of Civic Center floor space.

27,000 SF of administrative Class "A" office space has been transferred from the New B1 Administration Building to the New B1 Courthouse Building.

The transferring of 27,000 SF of administrative Class "A" office space from the New Administrative to New Courthouse Building occurs in Option B2 as well.

The 45,000 SF of supplemental Civic Center floor space is figured to require a slightly overall heavier structure and foundation due to the floor levels added to it.

5,000 SF of supplemental Civic Center is figured to be Courtroom space, 12,000 SF secured jail type functions, and the balance Class 'A' office space.

It is assumed that two additional floor levels to accommodate 45,000 SF of supplemental Civic Center floor space will not cause permit or zoning issues.



# THURSTON COUNTY COURTHOUSE and CIVIC CENTER

## A COMPARATIVE OPTION ANALYSIS

	Quantity	Unit	\$\$\$	Est. Cost	
<b>B2 DETAILS, PLUM STREET OPTION 2:</b>					
<b>B2 Site Development:</b>					
Courthouse/jail building demolition.	24,000.00	SF	\$8.00	\$192,000	Mostly single story structures with varying heights.
Other onsite building demolition.	8,000.00	SF	\$6.00	\$48,000	Simple single-story structures.
Add for possible hazardous material abatement.	1.00	LS	\$60,000.00	\$60,000	Allowance assumed to be relatively minor.
Site demolition.	240,000.00	SF	\$2.50	\$600,000	Primarily paving, fixtures, and outside utilities.
Erosion control, site clearing & subgrade prep.	436,000.00	SF	\$3.00	\$1,308,000	Strippings, questionable soils, nil slopes, a close cut/fill site.
Onsite utilities.	1.00	LS	\$900,000.00	\$900,000	All new onsite services, available from street edges.
Add for offsite utility connections.	1.00	LS	\$60,000.00	\$60,000	Includes tie-ins, minor cuts & patch only, not utility co. work.
New campus emergency back-up generator.	1.00	LS	\$300,000.00	\$300,000	Allowance, to be sized, able to run full campus power loads.
Add for storm treatment.	1.00	LS	\$200,000.00	\$200,000	Allowance for some filters and rain gardens.
Add for site lighting.	1.00	LS	\$200,000.00	\$200,000	Mostly pedestrian type lighting around new buildings.
Site fixtures & specialties.	1.00	LS	\$200,000.00	\$200,000	Benches, screen walls, signage, bike racks, receptacles, misc.
Site finishes.	340,000.00	SF	\$9.50	\$3,230,000	New paving, curbing, accents, and landscaping & irrigation.
Road edge work.	1,180.00	LF	\$400.00	\$472,000	C&G, sidewalk, 1 new lane, utility revamp, traffic control, misc.
Add for 25% General Contractor Mark-Ups:	0.25	LS	\$7,770,000	\$1,942,500	General requirements, overhead & profit, modest contingency.
<b>B2 Site Development, With 25% G.C. Mark-Ups:</b>				<b>\$9,712,500.00</b>	
<b>New B2 Courthouse Building:</b>					
Courthouse building, above ground, complete.	147,000.00	SF	\$250.00	\$36,750,000	7 levels above ground plus basement, Class "A" office baseline.
Additional Administrative floor space.	27,000.00	SF	\$250.00	\$6,750,000	An additional floor level, all Class "A" office space.
Add for pile foundation premium.	21,400.00	SF	\$50.00	\$1,070,000	Allowance under building footprint.
Add for MEP penthouse(s).	5,000.00	SF	\$150.00	\$750,000	MEP penthouse with mostly utility room type finishes.
Add for courtroom & holding area premiums.	147,000.00	SF	\$110.00	\$16,170,000	Primarily finish and security upgrades in most of spaces.
Add for underground basement level.	21,000.00	SF	\$160.00	\$3,360,000	Parking, Sally Port, Holding, mechanical & utility spaces.
Add for Silver LEED premiums.	147,000.00	SF	\$30.00	\$4,410,000	Energy, sustainability and certification add-ons.
Add for 25% General Contractor Mark-Ups:	0.25	LS	\$69,260,000	\$17,315,000	General requirements, overhead & profit, modest contingency.
<b>B2 Courthouse Building, With 25% G.C. Mark-Ups:</b>				<b>\$86,575,000.00</b>	
<b>New B2 Lobby Wing:</b>					
Lobby building wing, above ground, complete.	11,500.00	SF	\$280.00	\$3,220,000	2 levels above ground, Class "A", with a high degree of glazing.
Add for pile foundation premium.	5,750.00	SF	\$50.00	\$287,500	Allowance under building footprint.
Add for Silver LEED premiums.	11,500.00	SF	\$30.00	\$345,000	Energy, sustainability and certification add-ons.
Add for 25% General Contractor Mark-Ups:	0.25	LS	\$3,852,500	\$963,125	General requirements, overhead & profit, modest contingency.
<b>B2 Lobby Wing, With 25% G.C. Mark-Ups:</b>				<b>\$4,815,625.00</b>	
<b>New B2 Administration Building:</b>					
Administration building, above ground, complete.	149,500.00	SF	\$260.00	\$38,870,000	7 levels above ground, Class "A" office as a baseline.
Add for pile foundation premium.	21,000.00	SF	\$50.00	\$1,050,000	Allowance under half of new building foundation.
Add for MEP penthouse(s).	5,000.00	SF	\$150.00	\$750,000	MEP penthouse with mostly utility room type finishes.
Add for Silver LEED premiums.	149,500.00	SF	\$30.00	\$4,485,000	Energy, sustainability and certification add-ons.
Add for 25% General Contractor Mark-Ups:	0.25	LS	\$45,155,000	\$11,288,750	General requirements, overhead & profit, modest contingency.
<b>B2 Administration Building, With 25% G.C. Mark-Ups:</b>				<b>\$56,443,750.00</b>	
<b>Supplemental B2 Civic Center Space:</b>					
Administration building, above ground.	45,000.00	SF	\$260.00	\$11,700,000	2 additional levels above ground, Class "A" office as a baseline.
Add for courtroom & holding area premiums.	5,000.00	SF	\$120.00	\$600,000	Primarily finish and security upgrades in limited areas.
Add for jail function premiums.	12,000.00	SF	\$160.00	\$1,920,000	Isolation, hardened construction and security premiums.
Add for Silver LEED premiums.	45,000.00	SF	\$30.00	\$1,350,000	Energy, sustainability and certification add-ons.
Add for 25% General Contractor Mark-Ups:	0.25	LS	\$15,570,000	\$3,892,500	General requirements, overhead & profit, modest contingency.
<b>Supplemental B1 Civic Center Space, With 25% G.C. Mark-Ups:</b>				<b>\$19,462,500.00</b>	
<b>B2 DETAILS, PLUM STREET OPTION 2, continues on the next page:</b>					

**THURSTON COUNTY COURTHOUSE and CIVIC CENTER**  
**A COMPARATIVE OPTION ANALYSIS**

	Quantity	Unit	\$\$\$	Est. Cost	
<b>B2 DETAILS, CAMPUS STREET OPTION 2:</b>					
(Continued from the previous page.)					
<b>B2 Structured Parking:</b>					
Slab-on-grade parking, complete.	52,800.00	SF	\$20.00	\$1,056,000	Used as a footprint baseline.
Add for pile foundation premium.	52,800.00	SF	\$40.00	\$2,112,000	Allowance under building footprint.
Above grade parking levels, complete.	211,200.00	SF	\$80.00	\$16,896,000	Four levels, no roof, semi open concrete or composite steel.
Add for 25% General Contractor Mark-Ups:	0.25	LS	\$20,064,000	\$5,016,000	General requirements, overhead & profit, modest contingency.
	<i>B2 Structured Parking, With 25% G.C. Mark-Ups:</i>				<i>\$25,080,000.00</i>
				\$202,089,375	\$202,089,375

**B2) PLUM STREET OPTION 2 NOTES:**

The same general scope assumptions listed in the B1 Plum Street Option 1 Notes apply here.

A 'Future Extension' noted on the B2 site plan is figured as landscaping here and considered future work by others.

There are some minor differences in B1 and B2 site and building layouts and individual building footprint sizes, but their scopes are essentially the same.

27,000 SF of administrative Class "A" office space has been transferred from the New B2 Administration Building to the New B2 Courthouse Building.

The same total floor area and general scope assumptions of the B1 Supplemental Civic Center Space applies to B2 as well.

13,200 SF of basement level parking has been added under the new Administration Building, plus 5,400 SF under the Lobby in this Option.



# THURSTON COUNTY COURTHOUSE and CIVIC CENTER

## A COMPARATIVE OPTION ANALYSIS

	Quantity	Unit	\$\$\$	Est. Cost	
<b>C1 DETAILS, HARRISON AVENUE OPTION 1:</b>					
<b>C1 Site Development:</b>					
Site demolition.	20,000.00	SF	\$3.00	\$60,000	Primarily paving, fixtures, and outside utilities.
Erosion control, site clearing & subgrade prep.	950,000.00	SF	\$2.50	\$2,375,000	Strippings, questionable soils, nil slopes, a close cut/fill site.
Onsite utilities.	1.00	LS	\$900,000.00	\$900,000	All new onsite services, available from street edges.
Add for offsite utility connections.	1.00	LS	\$60,000.00	\$60,000	Includes tie-ins, minor cuts & patch only, not utility co. work.
New campus emergency back-up generator.	1.00	LS	\$300,000.00	\$300,000	Allowance, to be sized, able to run full campus power loads.
Add for storm treatment.	1.00	LS	\$200,000.00	\$200,000	Allowance for some filters, swales, ponds and rain gardens.
Add for large storm detention vaults.	1.00	LS	\$2,000,000	\$2,000,000	Allowance, a self-contained system, sizes to be determined.
Add for site lighting.	1.00	LS	\$350,000.00	\$350,000	Mostly pedestrian and parking lot type lighting.
Site fixtures & specialties.	1.00	LS	\$250,000.00	\$250,000	Benches, screen walls, signage, bike racks, receptacles, misc.
Site finishes, less Private Development areas.	840,000.00	SF	\$9.00	\$7,560,000	New paving, curbing, accents, and landscaping & irrigation.
Road edge work.	900.00	LF	\$400.00	\$360,000	C&G, sidewalk, 1 new lane, utility revamp, traffic control, misc.
Site improvement periphery restoration.	5,000.00	LF	\$40.00	\$200,000	Allowance, at existing boundary edges, less street edges.
Add for 25% General Contractor Mark-Ups:	0.25	LS	\$14,615,000	\$3,653,750	General requirements, overhead & profit, modest contingency.
<i>C1 Site Development, With 25% G.C. Mark-Ups:</i>					<i>\$18,268,750.00</i>
<b>C1 Offsite Infrastructure:</b>					
Possible additional offsite street improvements.	1.00	LS	\$3,000,000	\$3,000,000	Allowance, scope and extent to be addressed.
Extended water & sanitary services to site edge.	1.00	LS	\$600,000	\$600,000	" " " " " " " " " " " "
Extended gas & electrical services to site edge.	1.00	LS	\$1,200,000	\$1,200,000	" " " " " " " " " " " "
Add for 25% General Contractor Mark-Ups:	0.25	LS	\$4,800,000	\$1,200,000	General requirements, overhead & profit, modest contingency.
<i>C1 Offsite Infrastructure, With 25% G.C. Mark-Ups:</i>					<i>\$6,000,000.00</i>
<b>New C1 Courthouse Building:</b>					
Courthouse building, above ground, complete.	147,000.00	SF	\$250.00	\$36,750,000	5 levels above ground plus basement, Class "A" office baseline.
Add for MEP penthouse(s).	5,000.00	SF	\$150.00	\$750,000	MEP penthouse with mostly utility room type finishes.
Add for courtroom & holding area premiums.	147,000.00	SF	\$110.00	\$16,170,000	Primarily finish and security upgrades in most of spaces.
Add for underground basement level.	21,000.00	SF	\$150.00	\$3,150,000	Parking, Sally Port, Holding, mechanical & utility spaces.
Add for Silver LEED premiums.	147,000.00	SF	\$30.00	\$4,410,000	Energy, sustainability and certification add-ons.
Add for 25% General Contractor Mark-Ups:	0.25	LS	\$61,230,000	\$15,307,500	General requirements, overhead & profit, modest contingency.
<i>C1 Courthouse Building, With 25% G.C. Mark-Ups:</i>					<i>\$76,537,500.00</i>
<b>New C1 Lobby Wing:</b>					
Lobby building wing, above ground, complete.	11,500.00	SF	\$280.00	\$3,220,000	2 levels above ground, Class "A", with a high degree of glazing.
Add for extended canopy.	3,000.00	SF	\$80.00	\$240,000	Allowance at front side entrance of Lobby.
Add for Silver LEED premiums.	11,500.00	SF	\$30.00	\$345,000	Energy, sustainability and certification add-ons.
Add for 25% General Contractor Mark-Ups:	0.25	LS	\$3,805,000	\$951,250	General requirements, overhead & profit, modest contingency.
<i>C1 Lobby Wing, With 25% G.C. Mark-Ups:</i>					<i>\$4,756,250.00</i>
<b>New C1 Administration Building:</b>					
Administration building, above ground, complete.	176,500.00	SF	\$250.00	\$44,125,000	6 levels above ground, Class "A" office as a baseline.
Add for MEP penthouse(s).	5,000.00	SF	\$150.00	\$750,000	MEP penthouse with mostly utility room type finishes.
Add for Silver LEED premiums.	176,500.00	SF	\$30.00	\$5,295,000	Energy, sustainability and certification add-ons.
Add for 25% General Contractor Mark-Ups:	0.25	LS	\$50,170,000	\$12,542,500	General requirements, overhead & profit, modest contingency.
<i>C1 Administration Building, With 25% G.C. Mark-Ups:</i>					<i>\$62,712,500.00</i>
				<i>\$168,275,000</i>	<i>\$168,275,000</i>

**C1) HARRISON AVENUE OPTION 1 NOTES:**

No project phasing premiums are figured in Option C1. Construction work is to start in spring of 2022.

Storm treatment at C1 & C2 sites are figured to be self-contained, mostly in large underground vaults due to poorly draining native soils.

Private development areas are figured to be cleared and transitioned at their peripheries, but otherwise left as future work to be done by others.

A total of \$6,000,000 with mark-ups is included for likely offsite street improvements and extended utility services yet to be addressed or quantified.

In order to maintain limited floor level heights and provide a total of 335,000 SF floor space, new building footprints are figured to be larger than what is noted.

No Structured Parking is shown or figured in this Option.

# THURSTON COUNTY COURTHOUSE and CIVIC CENTER

## A COMPARATIVE OPTION ANALYSIS

	Quantity	Unit	\$\$\$	Est. Cost	
<b>C2 DETAILS, HARRISON AVENUE OPTION 2:</b>					
<b>C2 Site Development:</b>					
Site demolition.	20,000.00	SF	\$3.00	\$60,000	Primarily paving, fixtures, and outside utilities.
Erosion control, site clearing & subgrade prep.	900,000.00	SF	\$2.50	\$2,250,000	Strippings, questionable soils, nil slopes, a close cut/fill site.
Onsite utilities.	1.00	LS	\$900,000.00	\$900,000	All new onsite services, available from street edges.
Add for offsite utility connections.	1.00	LS	\$60,000.00	\$60,000	Includes tie-ins, minor cuts & patch only, not utility co. work.
New campus emergency back-up generator.	1.00	LS	\$300,000.00	\$300,000	Allowance, to be sized, able to run full campus power loads.
Add for storm treatment.	1.00	LS	\$250,000.00	\$250,000	Allowance for some filters, swales, ponds and rain gardens.
Add for large storm detention vaults.	1.00	LS	\$2,000,000	\$2,000,000	Allowance, a self-contained system, sizes to be determined.
Add for site lighting.	1.00	LS	\$320,000.00	\$320,000	Mostly pedestrian and parking lot type lighting.
Site fixtures & specialties.	1.00	LS	\$240,000.00	\$240,000	Benches, screen walls, signage, bike racks, receptacles, misc.
Site finishes.	680,000.00	SF	\$9.00	\$6,120,000	New paving, curbing, accents, and landscaping & irrigation.
Road edge work.	900.00	LF	\$400.00	\$360,000	C&G, sidewalk, 1 new lane, utility revamp, traffic control, misc.
Site improvement periphery restoration.	5,000.00	LF	\$40.00	\$200,000	Allowance, at existing boundary edges, less street edges.
Add for 25% General Contractor Mark-Ups:	0.25	LS	\$13,060,000	\$3,265,000	General requirements, overhead & profit, modest contingency.
<i>C2 Site Development, With 25% G.C. Mark-Ups:</i>				\$16,325,000.00	
<b>C2 Offsite Infrastructure:</b>					
Possible additional offsite street improvements.	1.00	LS	\$3,000,000	\$3,000,000	Allowance, scope and extent to be addressed.
Extended water & sanitary services to site edge.	1.00	LS	\$600,000	\$600,000	" " " " " " " " " " " "
Extended gas & electrical services to site edge.	1.00	LS	\$1,200,000	\$1,200,000	" " " " " " " " " " " "
Add for 25% General Contractor Mark-Ups:	0.25	LS	\$4,800,000	\$1,200,000	General requirements, overhead & profit, modest contingency.
<i>C2 Offsite Infrastructure, With 25% G.C. Mark-Ups:</i>				\$6,000,000.00	
<b>New C2 Courthouse Building:</b>					
Courthouse building, above ground, complete.	147,000.00	SF	\$250.00	\$36,750,000	5 levels above ground plus basement, Class "A" office baseline.
Add for MEP penthouse(s).	5,000.00	SF	\$150.00	\$750,000	MEP penthouse with mostly utility room type finishes.
Add for courtroom & holding area premiums.	147,000.00	SF	\$110.00	\$16,170,000	Primarily finish and security upgrades in most of spaces.
Add for underground basement level.	21,000.00	SF	\$150.00	\$3,150,000	Parking, Sally Port, Holding, mechanical & utility spaces.
Add for Silver LEED premiums.	147,000.00	SF	\$30.00	\$4,410,000	Energy, sustainability and certification add-ons.
Add for 25% General Contractor Mark-Ups:	0.25	LS	\$61,230,000	\$15,307,500	General requirements, overhead & profit, modest contingency.
<i>C2 Courthouse Building, With 25% G.C. Mark-Ups:</i>				\$76,537,500.00	
<b>New C2 Lobby Wing:</b>					
Lobby building wing, above ground, complete.	11,500.00	SF	\$280.00	\$3,220,000	2 levels above ground, Class "A", with a high degree of glazing.
Add for extended canopy.	2,500.00	SF	\$80.00	\$200,000	Allowance at front side entrance of Lobby.
Add for Silver LEED premiums.	11,500.00	SF	\$30.00	\$345,000	Energy, sustainability and certification add-ons.
Add for 25% General Contractor Mark-Ups:	0.25	LS	\$3,765,000	\$941,250	General requirements, overhead & profit, modest contingency.
<i>C2 Lobby Wing, With 25% G.C. Mark-Ups:</i>				\$4,706,250.00	
<b>New C2 Administration Building:</b>					
Administration building, above ground, complete.	176,500.00	SF	\$250.00	\$44,125,000	6 levels above ground, Class "A" office as a baseline.
Add for MEP penthouse(s).	5,000.00	SF	\$150.00	\$750,000	MEP penthouse with mostly utility room type finishes.
Add for Silver LEED premiums.	176,500.00	SF	\$30.00	\$5,295,000	Energy, sustainability and certification add-ons.
Add for 25% General Contractor Mark-Ups:	0.25	LS	\$50,170,000	\$12,542,500	General requirements, overhead & profit, modest contingency.
<i>C2 Administration Building, With 25% G.C. Mark-Ups:</i>				\$62,712,500.00	
<b>C2 Structured Parking:</b>					
Slab-on-grade parking, complete.	37,800.00	SF	\$20.00	\$756,000	Used as a footprint baseline.
Above grade parking levels, complete.	113,400.00	SF	\$80.00	\$9,072,000	Three levels, no roof, semi open concrete or composite steel.
Add for 25% General Contractor Mark-Ups:	0.25	LS	\$9,828,000	\$2,457,000	General requirements, overhead & profit, modest contingency.
<i>B2 Structured Parking, With 25% G.C. Mark-Ups:</i>				\$12,285,000.00	
				\$178,566,250	\$178,566,250

**C2) HARRISON AVENUE OPTION 2 NOTES:**

The same general scope assumptions listed in the C1 Harrison Avenue Option 2 Notes apply here.

The most significant cost difference between Option C1 and C2 is the latter has an independent Structured Parking facility and the other does not.

Another notable difference is C2 has a greater site area of Private Development by others.



# XIV.

THURSTON COUNTY COURTHOUSE FINAL REPORT

## TOTAL PROJECT COST SUMMARY

DRAFT

# TOTAL PROJECT COSTS SUMMARY (PROGRAM DRIVEN): HILLTOP CAMPUS



Thurston County Courthouse and Civic Center  
Comprehensive Comparative Feasibility Study

Total Project Cost Summary  
Option A1 - Program Driven

ITEM DESCRIPTION	UNITS	UNIT PRICE	COST	NOTES
<b>CONSTRUCTION COST</b>				
SOUTH CAMPUS BUILDING DEMOLITION			\$254,200	FROM 12-5-18 ACKER ESTIMATE (REPORT SECTION 13)
SOUTH CAMPUS SITE DEVELOPMENT			\$2,480,000	
4 LEVEL COURTHOUSE BUILDING			\$61,230,000	WITH BASEMENT LEVEL
LOBBY WING			\$3,565,000	
SOUTH PARKING STRUCTURE			\$13,304,000	INCLUDES CONNECTOR TO COURTHOUSE
BUILDING 1, 2, & 3 DEMOLITION			\$1,190,000	
NORTH CAMPUS SITE DEVELOPMENT			\$4,271,000	
NORTH PARKING STRUCTURE			\$6,050,000	
3 LEVEL ADMINISTRATION BUILDING			\$51,935,000	
SUB TOTAL			<b>\$144,279,200</b>	
25% CONTRACTORS MARKUP	\$144,279,200	25%	\$36,069,800	GEN REQ, OH&P, BOND & INS, B&O TAX, DESIGN CONT.
SUB TOTAL			\$180,349,000	HARD CONSTRUCTION COSTS IN 2018 DOLLARS
ESCALATION PREMIUM (17% & 28%)			\$44,053,303	FROM 12-5-18 ACKER ESTIMATE (REPORT SECTION 13)
<b>TOTAL CONSTRUCTION COST</b>			<b>\$224,402,303</b>	
<b>OTHER PROJECT EXPENSES</b>				
FURNISHINGS (FF&E)	1	ALLOWANCE	\$6,100,000	
WA STATE SALES TAX	\$224,402,303	8.90%	\$19,971,805	
OWNER'S CONTINGENCY	\$224,402,303	5.00%	\$11,220,115	OWNER ONLY CONTINGENCY (NEW CONST)
CITY OF OLYMPIA PERMITTING & IMPACT FEES	1	ALLOWANCE	\$4,180,850	(EST.) SEE DETAILED LIST OF FEES (REPORT SECTION 11)
CONST. MANAGEMENT SERVICES	\$224,402,303	1.50%	\$3,366,035	SURVEY, GEOTECH, TESTING, COMMISSIONING, CRITERIA DOCS
MOVING EXPENSES	1	ALLOWANCE	\$300,000	400 STAFF X \$750/STAFF
A&E STIPENDS		ALLOWANCE	\$500,000	FOR DESIGN-BUILD FINALISTS (2 UNSUCCESSFUL FIRMS)
ARCHITECT & ENGINEERING	\$224,402,303	7.0%	\$15,708,161	BASIC SERVICES
ADDITIONAL CONSULTANT SERVICES	\$224,402,303	2.5%	\$5,610,058	CIVIL, COST EST., I.D., L.A., ACCOUST., ENVELOPE, LEED
DESIGN-BUILD MARKUP ON A&E FEES	\$21,318,219	15.0%	\$3,197,733	TAXES & OVERHEAD COSTS ON A&E FEES FOR DESIGN-BUILDER
PROPERTY ACQUISITION	1	APPRAISAL	\$800,000	FOR WESTERN BLDG. MAT. (FROM 11-27-18 K.M. APPRAISAL)
OWNER FINANCING COSTS (EST. @ 1% OF TOTAL)	1	ALLOWANCE	\$3,000,000	BOND UNDERWRITING, LEGAL, BOND PLACEMENT FEES
OWNER INTERNAL MANAGEMENT EXPENSES	1	ALLOWANCE	\$500,000	INTERNAL COSTS DIRECTLY ASSOCIATED WITH THIS PROJECT
LEASE EXPENSES FOR COUNTY (45,000 SF X \$20/SF)		ALLOWANCE	\$1,800,000	RELOCATE BUILDING 1 STAFF FOR 2 YEARS
COSTS ASSOCIATED WITH LEASING OFF SITE		ALLOWANCE	\$3,640,500	DESIGN, T.I. CONSTRUCTION, IT CABLE, FURNITURE, MOVING
<b>SUB TOTAL</b>			<b>\$304,297,559</b>	
PROCEEDS FROM SALE OF COUNTY PROPERTY		APPRAISAL	(\$2,000,000)	SELECT COUNTY HILLTOP PARCELS COULD BE SURPLUSED
<b>TOTAL PROJECT COST</b>			<b>\$302,297,559</b>	

Thurston County Courthouse and Civic Center  
Comprehensive Comparative Feasibility Study

Total Project Cost Summary  
Option A2 - Program Driven

ITEM DESCRIPTION	UNITS	UNIT PRICE	COST NOTES
<b>CONSTRUCTION COST</b>			
BUILDING 3 DEMOLITION			FROM 12-5-18 ACKER ESTIMATE (REPORT SECTION 13)
INITIAL SITE DEVELOPMENT			\$640,000
5 LEVEL COURTHOUSE BUILDING			\$2,463,000
PARKING STRUCTURE			\$61,230,000 WITH BASEMENT LEVEL
BUILDING 2 DEMOLITION			\$13,500,000 INCLUDES CONNECTOR TO COURTHOUSE
SOUTH SITE DEVELOPMENT			\$260,000
LOBBY WING			\$2,190,000
7 LEVEL ADMINISTRATION BUILDING			\$3,841,000
BUILDING 1 DEMOLITION			\$50,170,000
NORTH SITE DEVELOPMENT			\$300,000
SUB TOTAL			\$1,253,000
			<b>\$135,847,000</b>
25% CONTRACTORS MARKUP	\$135,847,000	25%	\$33,961,750 GEN REQ, OH&P, BOND & INS, B&O TAX, DESIGN CONT.
SUB TOTAL			\$169,808,750 HARD CONSTRUCTION COSTS IN 2018 DOLLARS
ESCALATION PREMIUM (17%, 28%, & 35%)	\$169,808,750		\$36,972,300 FROM 12-5-18 ACKER ESTIMATE (REPORT SECTION 13)
<b>TOTAL CONSTRUCTION COST</b>			<b>\$206,781,050</b>
<b>OTHER PROJECT EXPENSES</b>			
FURNISHINGS (FF&E)	1	ALLOWANCE	\$6,100,000
WA STATE SALES TAX	\$206,781,050	8.90%	\$18,403,513
OWNER'S CONTINGENCY	\$206,781,050	5.00%	\$10,339,053 OWNER ONLY CONTINGENCY (NEW CONST)
CITY OF OLYMPIA PERMITTING & IMPACT FEES	1	ALLOWANCE	\$4,180,850 (EST.) SEE DETAILED LIST OF FEES (REPORT SECTION 11)
CONST. MANAGEMENT SERVICES	\$206,781,050	1.50%	\$3,101,716 SURVEY, GEOTECH, TESTING, COMMISSIONING, CRITERIA DOCS
MOVING EXPENSES	1	ALLOWANCE	\$300,000 400 STAFF X \$750/STAFF
A&E STIPENDS		ALLOWANCE	\$500,000 FOR DESIGN-BUILD FINALISTS (2 UNSUCCESSFUL FIRMS)
ARCHITECT & ENGINEERING	\$206,781,050	7.0%	\$14,474,674 BASIC SERVICES
ADDITIONAL CONSULTANT SERVICES	\$206,781,050	2.5%	\$5,169,526 CIVIL, COST EST., I.D., L.A., ACCOUST., ENVELOPE, LEED
DESIGN-BUILD MARKUP ON A&E FEES	\$19,644,200	15.0%	\$2,946,630 TAXES & OVERHEAD COSTS ON A&E FEES FOR DESIGN-BUILDER
PROPERTY ACQUISITION	1	APPRAISAL	\$800,000 FOR WESTERN BLDG. MAT. (FROM 11-27-18 K.M. APPRAISAL)
OWNER FINANCING COSTS (EST. @ 1% OF TOTAL)	1	ALLOWANCE	\$2,700,000 BOND UNDERWRITING, LEGAL, BOND PLACEMENT FEES
OWNER INTERNAL MANAGEMENT EXPENSES	1	ALLOWANCE	\$500,000 INTERNAL COSTS DIRECTLY ASSOCIATED WITH THIS PROJECT
LEASE EXPENSES FOR COUNTY (22,000 SF X \$20/SF)	1	ALLOWANCE	\$880,000 RELOCATE BUILDING 3 OCCUPANTS FOR 2 YEARS
COSTS ASSOCIATED WITH LEASING OFF SITE	1	ALLOWANCE	\$1,768,250 DESIGN, T.I. CONSTRUCTION, IT CABLE, FURNITURE, MOVING
<b>SUB TOTAL</b>			<b>\$278,945,261</b>
PROCEEDS FROM SALE OF COUNTY PROPERTY		APPRAISAL	(\$2,000,000) SELECT COUNTY HILLTOP PARCELS COULD BE SURPLUSED
<b>TOTAL PROJECT COST</b>			<b>\$276,945,261</b>



Thurston County Courthouse and Civic Center  
Comprehensive Comparative Feasibility Study

Total Project Cost Summary  
Option A3 - Program Driven

ITEM DESCRIPTION	UNITS	UNIT PRICE	COST	NOTES
<b>CONSTRUCTION COST</b>				
BUILDING 3 DEMOLITION			\$640,000	FROM 12-5-18 ACKER ESTIMATE (REPORT SECTION 13)
INITIAL SITE DEVELOPMENT			\$5,325,000	
4 LEVEL COURTHOUSE BUILDING			\$64,170,000	WITH BASEMENT LEVEL
LOBBY WING			\$3,680,000	
5 LEVEL ADMINISTRATION BUILDING			\$53,700,000	
NORTH PARKING STRUCTURE			\$11,232,000	
BUILDING 1 & 2 DEMOLITION			\$550,000	
BALANCE OF SITE DEVELOPMENT			\$3,006,500	
SOUTH PARKING STRUCTURE			\$5,616,000	INCLUDES CONNECTOR TO COURTHOUSE
<b>SUB TOTAL</b>			<b>\$147,919,500</b>	
25% CONTRACTORS MARKUP	\$147,919,500	25%	\$36,979,875	GEN REQ, OH&P, BOND & INS, B&O TAX, DESIGN CONT.
<b>SUB TOTAL</b>			<b>\$184,899,375</b>	HARD CONSTRUCTION COSTS IN 2018 DOLLARS
ESCALATION PREMIUM (17% & 28%)			\$32,686,113	FROM 12-5-18 ACKER ESTIMATE (REPORT SECTION 13)
<b>TOTAL CONSTRUCTION COST</b>			<b>\$217,585,488</b>	
<b>OTHER PROJECT EXPENSES</b>				
FURNISHINGS (FF&E)	1	ALLOWANCE	\$6,100,000	
WA STATE SALES TAX	\$217,585,488	8.90%	\$19,365,108	
OWNER'S CONTINGENCY	\$217,585,488	5.00%	\$10,879,274	OWNER ONLY CONTINGENCY (NEW CONST)
CITY OF OLYMPIA PERMITTING & IMPACT FEES	1	ALLOWANCE	\$4,180,850	(EST.) SEE DETAILED LIST OF FEES (REPORT SECTION 11)
CONST. MANAGEMENT SERVICES	\$217,585,488	1.50%	\$3,263,782	SURVEY, GEOTECH, TESTING, COMMISSIONING, CRITERIA DOCS
MOVING EXPENSES	1	ALLOWANCE	\$300,000	400 STAFF X \$750/STAFF
A&E STIPENDS		ALLOWANCE	\$500,000	FOR DESIGN-BUILD FINALISTS (2 UNSUCCESSFUL FIRMS)
ARCHITECT & ENGINEERING	\$217,585,488	7.0%	\$15,230,984	BASIC SERVICES
ADDITIONAL CONSULTANT SERVICES	\$217,585,488	2.5%	\$5,439,637	CIVIL, COST EST., I.D., L.A., ACCOUST., ENVELOPE, LEED
DESIGN-BUILD MARKUP ON A&E FEES	\$20,670,621	15.0%	\$3,100,593	TAXES & OVERHEAD COSTS ON A&E FEES FOR DESIGN-BUILDER
OWNER FINANCING COSTS (EST. @ 1% OF TOTAL)	1	ALLOWANCE	\$2,900,000	BOND UNDERWRITING, LEGAL, BOND PLACEMENT FEES
OWNER INTERNAL MANAGEMENT EXPENSES	1	ALLOWANCE	\$500,000	INTERNAL COSTS DIRECTLY ASSOCIATED WITH THIS PROJECT
LEASE EXPENSES FOR COUNTY (22,000 SF X \$20/SF)	1	ALLOWANCE	\$880,000	RELOCATE BUILDING 3 OCCUPANTS FOR 2 YEARS
COSTS ASSOCIATED WITH LEASING OFF SITE	1	ALLOWANCE	\$1,768,250	DESIGN, T.I. CONSTRUCTION, IT CABLE, FURNITURE, MOVING
<b>SUB TOTAL</b>			<b>\$291,993,968</b>	
PROCEEDS FROM SALE OF COUNTY PROPERTY		APPRAISAL	(\$3,000,000)	SELECT COUNTY HILLTOP PARCELS COULD BE SURPLUSED
<b>TOTAL PROJECT COST</b>			<b>\$288,993,968</b>	

Thurston County Courthouse and Civic Center  
Comprehensive Comparative Feasibility Study

Total Project Cost Summary  
Option A4 - Program Driven

ITEM DESCRIPTION	UNITS	UNIT PRICE	COST	NOTES
<b>CONSTRUCTION COST</b>				
BUILDING 3 DEMOLITION			\$670,000	FROM 12-5-18 ACKER ESTIMATE (REPORT SECTION 13)
INITIAL SITE DEVELOPMENT			\$4,355,000	
7 LEVEL COURTHOUSE BUILDING			\$64,170,000	WITH BASEMENT LEVEL
LOBBY WING			\$3,565,000	
NORTH PARKING STRUCTURE			\$18,900,000	INCLUDES CONNECTOR TO COURTHOUSE
BUILDING 1 & 2 DEMOLITION			\$520,000	
BALANCE OF NORTH CAMPUS SITE DEVELOPMENT			\$1,880,000	
5 LEVEL ADMINISTRATION BUILDING			\$52,025,000	
SOUTH CAMPUS BUILDING DEMOLITION			\$219,000	
SOUTH CAMPUS SITE DEVELOPMENT			\$2,241,000	
SUB TOTAL			<b>\$148,545,000</b>	
25% CONTRACTORS MARKUP	\$148,545,000	25%	\$37,136,250	GEN REQ, OH&P, BOND & INS, B&O TAX, DESIGN CONT.
SUB TOTAL			<b>\$185,681,250</b>	HARD CONSTRUCTION COSTS IN 2018 DOLLARS
ESCALATION PREMIUM (17%, 28%, & 35%)			\$39,594,375	FROM 12-5-18 ACKER ESTIMATE (REPORT SECTION 13)
<b>TOTAL CONSTRUCTION COST</b>			<b>\$225,275,625</b>	
<b>OTHER PROJECT EXPENSES</b>				
FURNISHINGS (FF&E)	1	ALLOWANCE	\$6,100,000	
WA STATE SALES TAX	\$225,275,625	8.90%	\$20,049,531	
OWNER'S CONTINGENCY	\$225,275,625	5.00%	\$11,263,781	OWNER ONLY CONTINGENCY (NEW CONST)
CITY OF OLYMPIA PERMITTING & IMPACT FEES	1	ALLOWANCE	\$4,180,850	(EST.) SEE DETAILED LIST OF FEES (REPORT SECTION 11)
CONST. MANAGEMENT SERVICES	\$225,275,625	1.50%	\$3,379,134	SURVEY, GEOTECH, TESTING, COMMISSIONING, CRITERIA DOCS
MOVING EXPENSES	1	ALLOWANCE	\$300,000	400 STAFF X \$750/STAFF
A&E STIPENDS		ALLOWANCE	\$500,000	FOR DESIGN-BUILD FINALISTS (2 UNSUCCESSFUL FIRMS)
ARCHITECT & ENGINEERING	\$225,275,625	7.0%	\$15,769,294	BASIC SERVICES
ADDITIONAL CONSULTANT SERVICES	\$225,275,625	2.5%	\$5,631,891	CIVIL, COST EST., I.D., L.A., ACCOUST., ENVELOPE, LEED
DESIGN-BUILD MARKUP ON A&E FEES	\$21,401,184	15.0%	\$3,210,178	TAXES & OVERHEAD COSTS ON A&E FEES FOR DESIGN-BUILDER
OWNER FINANCING COSTS (EST. @ 1% OF TOTAL)	1	ALLOWANCE	\$3,000,000	BOND UNDERWRITING, LEGAL, BOND PLACEMENT FEES
OWNER INTERNAL MANAGEMENT EXPENSES	1	ALLOWANCE	\$500,000	INTERNAL COSTS DIRECTLY ASSOCIATED WITH THIS PROJECT
LEASE EXPENSES FOR COUNTY (45,000 SF X \$20/SF)		ALLOWANCE	\$1,800,000	RELOCATE BUILDING 1 STAFF FOR 2 YEARS
COSTS ASSOCIATED WITH LEASING OFF SITE		ALLOWANCE	\$3,640,500	DESIGN, T.I. CONSTRUCTION, IT CABLE, FURNITURE, MOVING
<b>SUB TOTAL</b>			<b>\$304,600,783</b>	
PROCEEDS FROM SALE OF COUNTY PROPERTY		APPRAISAL	(\$1,000,000)	SELECT COUNTY HILLTOP PARCELS COULD BE SURPLUSED
<b>TOTAL PROJECT COST</b>			<b>\$303,600,783</b>	

Thurston County Courthouse and Civic Center  
Comprehensive Comparative Feasibility Study

Total Project Cost Summary  
Option A5 - Program Driven

ITEM DESCRIPTION	UNITS	UNIT PRICE	COST NOTES
<b>CONSTRUCTION COST</b>			
BUILDING 3 DEMOLITION			FROM 12-5-18 ACKER ESTIMATE (REPORT SECTION 13)
INITIAL SITE DEVELOPMENT			\$670,000
NORTH PARKING STRUCTURE			\$1,272,500
BUILDING 1 DEMOLITION			\$18,900,000
BALANCE OF NORTH CAMPUS SITE WORK			\$300,000
5 LEVEL COURTHOUSE BUILDING			\$4,746,000
LOBBY WING			\$61,230,000 WITH BASEMENT LEVEL
7 LEVEL ADMINISTRATION BUILDING			\$3,450,000
BUILDING 2 DEMOLITION			\$50,170,000
BALANCE OF SITE DEVELOPMENT			\$260,000
SOUTH PARKING STRUCTURE			\$1,740,000
SUB TOTAL			\$7,000,000
25% CONTRACTORS MARKUP	\$149,738,500	25%	<b>\$149,738,500</b>
SUB TOTAL			\$37,434,625 GEN REQ, OH&P, BOND & INS, B&O TAX, DESIGN CONT.
ESCALATION PREMIUM (17%, 23%, & 35%)			\$187,173,125 HARD CONSTRUCTION COSTS IN 2018 DOLLARS
<b>TOTAL CONSTRUCTION COST</b>			<b>\$42,828,256</b> FROM 12-5-18 ACKER ESTIMATE (REPORT SECTION 13)
<b>OTHER PROJECT EXPENSES</b>			<b>\$230,001,381</b>
FURNISHINGS (FF&E)	1	ALLOWANCE	\$6,100,000
WA STATE SALES TAX	\$230,001,381	8.90%	\$20,470,123
OWNER'S CONTINGENCY	\$230,001,381	5.00%	\$11,500,069 OWNER ONLY CONTINGENCY (NEW CONST)
CITY OF OLYMPIA PERMITTING & IMPACT FEES	1	ALLOWANCE	\$4,180,850 (EST.) SEE DETAILED LIST OF FEES (REPORT SECTION 11)
CONST. MANAGEMENT SERVICES	\$230,001,381	1.50%	\$3,450,021 SURVEY, GEOTECH, TESTING, COMMISSIONING, CRITERIA DOCS
MOVING EXPENSES	1	ALLOWANCE	\$300,000 400 STAFF X \$750/STAFF
A&E STIPENDS		ALLOWANCE	\$500,000 FOR DESIGN-BUILD FINALISTS (2 UNSUCCESSFUL FIRMS)
ARCHITECT & ENGINEERING	\$230,001,381	7.0%	\$16,100,097 BASIC SERVICES
ADDITIONAL CONSULTANT SERVICES	\$230,001,381	2.5%	\$5,750,035 CIVIL, COST EST., I.D., L.A., ACCOUST., ENVELOPE, LEED
DESIGN-BUILD MARKUP ON A&E FEES	\$21,850,131	15.0%	\$3,277,520 TAXES & OVERHEAD COSTS ON A&E FEES FOR DESIGN-BUILDER
OWNER FINANCING COSTS (EST. @ 1% OF TOTAL)	1	ALLOWANCE	\$3,000,000 BOND UNDERWRITING, LEGAL, BOND PLACEMENT FEES
OWNER INTERNAL MANAGEMENT EXPENSES	1	ALLOWANCE	\$500,000 INTERNAL COSTS DIRECTLY ASSOCIATED WITH THIS PROJECT
LEASE EXPENSES FOR COUNTY (45,000 SF X \$20/SF)	1	ALLOWANCE	\$1,800,000 RELOCATE BUILDING 3 OCCUPANTS FOR 2 YEARS
COSTS ASSOCIATED WITH LEASING OFF SITE	1	ALLOWANCE	\$3,640,500 DESIGN, T.I. CONSTRUCTION, IT CABLE, FURNITURE, MOVING
<b>SUB TOTAL</b>			<b>\$310,570,595</b>
PROCEEDS FROM SALE OF COUNTY PROPERTY		APPRAISAL	(\$3,000,000) SELECT COUNTY HILLTOP PARCELS COULD BE SURPLUSED
<b>TOTAL PROJECT COST</b>			<b>\$307,570,595</b>



TOTAL PROJECT COSTS (PROGRAM DRIVEN): PLUM STREET

Thurston County Courthouse and Civic Center  
Comprehensive Comparative Feasibility Study

Total Project Cost Summary  
Option B1 - Program Driven

ITEM DESCRIPTION	UNITS	UNIT PRICE	COST NOTES
<b>CONSTRUCTION COST</b>			
SITE PREP & IMPROVEMENTS			FROM 12-5-18 ACKER ESTIMATE (REPORT SECTION 13)
7 LEVEL COURTHOUSE BUILDING			\$8,000,000
LOBBY WING			\$70,350,000 WITH BASEMENT LEVEL
7 LEVEL ADMINISTRATION BUILDING			\$3,852,500
STRUCTURED PARKING			\$45,055,000
CITY OF OLYMPIA MUNICIPAL SPACE			\$20,064,000 5 LEVELS
SUB TOTAL			\$15,570,000 (\$22,771,125 WITH MARKUP & ESCALATION)
25% CONTRACTORS MARKUP	\$162,891,500	25%	<b>\$162,891,500</b>
SUB TOTAL			\$40,722,875 GEN REQ, OH&P, BOND & INS, B&O TAX, DESIGN CONT.
17% ESCALATION PREMIUM	\$203,614,375	17.0%	\$203,614,375 HARD CONSTRUCTION COSTS IN 2018 DOLLARS
<b>TOTAL CONSTRUCTION COST</b>			<b>\$34,614,444 CONSTRUCTION WORK START IN SPRING OF 2022</b>
<b>OTHER PROJECT EXPENSES</b>			<b>\$238,228,819</b>
FURNISHINGS (FF&E)	1	ALLOWANCE	\$6,700,000 \$6,100,000 (COUNTY) + \$600,000 (CITY)
WA STATE SALES TAX	\$238,228,819	8.90%	\$21,202,365
OWNER'S CONTINGENCY	\$238,228,819	5.00%	\$11,911,441 OWNER ONLY CONTINGENCY (NEW CONST)
CITY OF OLYMPIA PERMITTING & IMPACT FEES	1	ALLOWANCE	\$4,094,900 (EST.) SEE DETAILED LIST OF FEES (REPORT SECTION 11)
CONST. MANAGEMENT SERVICES	\$238,228,819	1.50%	\$3,573,432 SURVEY, GEOTECH, TESTING, COMMISSIONING, CRITERIA DOCS
MOVING EXPENSES	1	ALLOWANCE	\$337,500 450 STAFF X \$750/STAFF
A&E STIPENDS		ALLOWANCE	\$500,000 FOR DESIGN-BUILD FINALISTS (2 UNSUCCESSFUL FIRMS)
ARCHITECT & ENGINEERING	\$238,228,819	7.0%	\$16,676,017 BASIC SERVICES
ADDITIONAL CONSULTANT SERVICES	\$238,228,819	2.5%	\$5,955,720 CIVIL, COST EST., I.D., L.A., ACCOUST., ENVELOPE, LEED
DESIGN-BUILD MARKUP ON A&E FEES	\$22,631,738	15.0%	\$3,394,761 TAXES & OVERHEAD COSTS ON A&E FEES FOR DESIGN-BUILDER
PROPERTY ACQUISITION	1	APPRAISAL	\$4,422,200 FOR 10.47 ACRES FROM 11-27-18 K.M. APPRAISAL
OWNER FINANCING COSTS (EST. @ 1% OF TOTAL)	1	ALLOWANCE	\$3,100,000 BOND UNDERWRITING, LEGAL, BOND PLACEMENT FEES
OWNER INTERNAL MANAGEMENT EXPENSES	1	ALLOWANCE	\$500,000 INTERNAL COSTS DIRECTLY ASSOCIATED WITH THIS PROJECT
LEASE EXPENSES FOR CITY (24,000 SF X \$20/SF)	1	ALLOWANCE	\$960,000 RELOCATE CREIGHTON JUSTICE CENTER STAFF FOR 2 YEARS
COSTS ASSOCIATED WITH LEASING OFF SITE	1	ALLOWANCE	\$1,952,500 DESIGN, T.I. CONSTRUCTION, IT CABLE, FURNITURE, MOVING
<b>SUB TOTAL</b>			<b>\$323,509,655</b>
PROCEEDS FROM SALE OF COUNTY PROPERTY		APPRAISAL	(\$7,800,000) POTENTIAL FOR ALL COUNTY HILLTOP PARCELS TO BE SURPLUSED
<b>TOTAL PROJECT COST</b>			<b>\$315,709,655</b> County portion = \$315,709,655 - \$32,514,963 = \$283,194,692

Note: 1. The Total Project Cost for the City of Olympia Municipal Space = \$32,514,963

Thurston County Courthouse and Civic Center  
Comprehensive Comparative Feasibility Study

Total Project Cost Summary  
Option B2 - Program Driven

ITEM DESCRIPTION	UNITS	UNIT PRICE	COST NOTES
<b>CONSTRUCTION COST</b>			
SITE PREP & IMPROVEMENTS			FROM 12-5-18 ACKER ESTIMATE (REPORT SECTION 13)
7 LEVEL COURTHOUSE BUILDING			\$7,770,000
LOBBY WING			\$69,260,000 WITH BASEMENT LEVEL
7 LEVEL ADMINISTRATION BUILDING			\$3,852,500
STRUCTURED PARKING			\$45,155,000
CITY OF OLYMPIA MUNICIPAL SPACE			\$20,064,000 5 LEVELS
SUB TOTAL			\$15,570,000 (\$22,771,125 WITH MARKUP & ESCALATION)
			<b>\$161,671,500</b>
25% CONTRACTORS MARKUP	\$161,671,500	25%	\$40,417,875 GEN REQ, OH&P, BOND & INS, B&O TAX, DESIGN CONT.
SUB TOTAL			\$202,089,375 HARD CONSTRUCTION COSTS IN 2018 DOLLARS
17% ESCALATION PREMIUM	\$202,089,375	17.0%	\$34,355,194 CONSTRUCTION WORK START IN SPRING OF 2022
<b>TOTAL CONSTRUCTION COST</b>			<b>\$236,444,569</b>
<b>OTHER PROJECT EXPENSES</b>			
FURNISHINGS (FF&E)	1	ALLOWANCE	\$6,700,000 \$6,100,000 (COUNTY) + \$600,000 (CITY)
WA STATE SALES TAX	\$236,444,569	8.90%	\$21,043,567
OWNER'S CONTINGENCY	\$236,444,569	5.00%	\$11,822,228 OWNER ONLY CONTINGENCY (NEW CONST)
CITY OF OLYMPIA PERMITTING & IMPACT FEES	1	ALLOWANCE	\$4,094,900 (EST.) SEE DETAILED LIST OF FEES (REPORT SECTION 11)
CONST. MANAGEMENT SERVICES	\$236,444,569	1.50%	\$3,546,669 SURVEY, GEOTECH, TESTING, COMMISSIONING, CRITERIA DOCS
MOVING EXPENSES	1	ALLOWANCE	\$337,500 450 STAFF X \$750/STAFF
A&E STIPENDS		ALLOWANCE	\$500,000 FOR DESIGN-BUILD FINALISTS (2 UNSUCCESSFUL FIRMS)
ARCHITECT & ENGINEERING	\$236,444,569	7.0%	\$16,551,120 BASIC SERVICES
ADDITIONAL CONSULTANT SERVICES	\$236,444,569	2.5%	\$5,911,114 CIVIL, COST EST., I.D., L.A., ACCOUST., ENVELOPE, LEED
DESIGN-BUILD MARKUP ON A&E FEES	\$22,462,234	15.0%	\$3,369,335 TAXES & OVERHEAD COSTS ON A&E FEES FOR DESIGN-BUILDER
PROPERTY ACQUISITION	1	APPRAISAL	\$4,422,200 FOR 10.47 ACRES FROM 11-27-18 K.M. APPRAISAL
OWNER FINANCING COSTS (EST. @ 1% OF TOTAL)	1	ALLOWANCE	\$3,100,000 BOND UNDERWRITING, LEGAL, BOND PLACEMENT FEES
OWNER INTERNAL MANAGEMENT EXPENSES	1	ALLOWANCE	\$500,000 INTERNAL COSTS DIRECTLY ASSOCIATED WITH THIS PROJECT
LEASE EXPENSES FOR CITY (24,000 SF X \$20/SF)	1	ALLOWANCE	\$960,000 RELOCATE CREIGHTON JUSTICE CENTER STAFF FOR 2 YEARS
COSTS ASSOCIATED WITH LEASING OFF SITE	1	ALLOWANCE	\$1,952,500 DESIGN, T.I. CONSTRUCTION, IT CABLE, FURNITURE, MOVING
<b>SUB TOTAL</b>			<b>\$321,255,701</b>
PROCEEDS FROM SALE OF COUNTY PROPERTY		APPRAISAL	(\$7,800,000) POTENTIAL FOR ALL COUNTY HILLTOP PARCELS TO BE SURPLUSED
<b>TOTAL PROJECT COST</b>			<b>\$313,455,701</b> County portion = \$313,455,701 - \$32,514,963 = \$280,940,738

Note: 1. The Total Project Cost for the City of Olympia Municipal Space = \$32,514,963



TOTAL PROJECT COSTS (PROGRAM DRIVEN): HARRISON WEST

Thurston County Courthouse and Civic Center  
Comprehensive Comparative Feasibility Study

Total Project Cost Summary  
Option C1 - Program Driven

ITEM DESCRIPTION	UNITS	UNIT PRICE	COST	NOTES
<b>CONSTRUCTION COST</b>				
SITE PREP & IMPROVEMENTS			\$14,615,000	FROM 12-5-18 ACKER ESTIMATE (REPORT SECTION 13)
OFFSITE INFRASTRUCTURE			\$4,800,000	
5 LEVEL COURTHOUSE BUILDING			\$61,230,000	
LOBBY WING			\$3,805,000	
6 LEVEL ADMINISTRATION BUILDING			\$50,170,000	
SUB TOTAL			<b>\$134,620,000</b>	
25% CONTRACTORS MARKUP	\$134,620,000	25%	\$33,655,000	GEN REQ, OH&P, BOND & INS, B&O TAX, DESIGN CONT.
SUB TOTAL			\$168,275,000	HARD CONSTRUCTION COSTS IN 2018 DOLLARS
17% ESCALATION PREMIUM	\$168,275,000	17.0%	\$28,606,750	CONSTRUCTION WORK START IN SPRING OF 2022
<b>TOTAL CONSTRUCTION COST</b>			<b>\$196,881,750</b>	
<b>OTHER PROJECT EXPENSES</b>				
FURNISHINGS (FF&E)	1	ALLOWANCE	\$6,100,000	
WA STATE SALES TAX	\$196,881,750	8.90%	\$17,522,476	
OWNER'S CONTINGENCY	\$196,881,750	5.00%	\$9,844,088	OWNER ONLY CONTINGENCY (NEW CONST)
CITY OF OLYMPIA PERMITTING & IMPACT FEES	1	ALLOWANCE	\$4,305,500	(EST.) SEE DETAILED LIST OF FEES (REPORT SECTION 11)
CONST. MANAGEMENT SERVICES	\$196,881,750	1.50%	\$2,953,226	SURVEY, GEOTECH, TESTING, COMMISSIONING, CRITERIA DOCS
MOVING EXPENSES	1	ALLOWANCE	\$300,000	400 STAFF X \$750/STAFF
A&E STIPENDS		ALLOWANCE	\$500,000	FOR DESIGN-BUILD FINALISTS (2 UNSUCCESSFUL FIRMS)
ARCHITECT & ENGINEERING	\$196,881,750	7.0%	\$13,781,723	BASIC SERVICES
ADDITIONAL CONSULTANT SERVICES	\$196,881,750	2.5%	\$4,922,044	CIVIL, COST EST., I.D., L.A., ACCOUST., ENVELOPE, LEED
DESIGN-BUILD MARKUP ON A&E FEES	\$18,703,766	15.0%	\$2,805,565	TAXES & OVERHEAD COSTS ON A&E FEES FOR DESIGN-BUILDER
PROPERTY ACQUISITION	1	APPRAISAL	\$5,070,000	FOR 20.2 ACRES FROM 11-27-18 K.M. APPRAISAL
OWNER FINANCING COSTS (EST. @ 1% OF TOTAL)	1	ALLOWANCE	\$2,600,000	BOND UNDERWRITING, LEGAL, BOND PLACEMENT FEES
OWNER INTERNAL MANAGEMENT EXPENSES	1	ALLOWANCE	\$500,000	INTERNAL COSTS DIRECTLY ASSOCIATED WITH THIS PROJECT
<b>SUB TOTAL</b>			<b>\$268,086,371</b>	
PROCEEDS FROM SALE OF COUNTY PROPERTY		APPRAISAL	(\$7,800,000)	POTENTIAL FOR ALL COUNTY HILLTOP PARCELS TO BE SURPLUSED
<b>TOTAL PROJECT COST</b>			<b>\$260,286,371</b>	

Thurston County Courthouse and Civic Center  
Comprehensive Comparative Feasibility Study

Total Project Cost Summary  
Option C2 - Program Driven

ITEM DESCRIPTION	UNITS	UNIT PRICE	COST	NOTES
<b>CONSTRUCTION COST</b>				
SITE PREP & IMPROVEMENTS			\$13,060,000	FROM 12-5-18 ACKER ESTIMATE (REPORT SECTION 13)
OFFSITE INFRASTRUCTURE			\$4,800,000	
5 LEVEL COURTHOUSE BUILDING			\$61,230,000	
LOBBY WING			\$3,765,000	
6 LEVEL ADMINISTRATION BUILDING			\$50,170,000	
STRUCTURED PARKING			\$9,828,000	4 LEVELS
<b>SUB TOTAL</b>			<b>\$142,853,000</b>	
25% CONTRACTORS MARKUP	\$142,853,000	25%	\$35,713,250	GEN REQ, OH&P, BOND & INS, B&O TAX, DESIGN CONT.
<b>SUB TOTAL</b>			<b>\$178,566,250</b>	HARD CONSTRUCTION COSTS IN 2018 DOLLARS
17% ESCALATION PREMIUM	\$178,566,250	17.0%	\$30,356,263	CONSTRUCTION WORK START IN SPRING OF 2022
<b>TOTAL CONSTRUCTION COST</b>			<b>\$208,922,513</b>	
<b>OTHER PROJECT EXPENSES</b>				
FURNISHINGS (FF&E)	1	ALLOWANCE	\$6,100,000	
WA STATE SALES TAX	\$208,922,513	8.90%	\$18,594,104	
OWNER'S CONTINGENCY	\$208,922,513	5.00%	\$10,446,126	OWNER ONLY CONTINGENCY (NEW CONST)
CITY OF OLYMPIA PERMITTING & IMPACT FEES	1	ALLOWANCE	\$4,305,500	(EST.) SEE DETAILED LIST OF FEES (REPORT SECTION 11)
CONST. MANAGEMENT SERVICES	\$208,922,513	1.50%	\$3,133,838	SURVEY, GEOTECH, TESTING, COMMISSIONING, CRITERIA DOCS
MOVING EXPENSES	1	ALLOWANCE	\$300,000	400 STAFF X \$750/STAFF
A&E STIPENDS		ALLOWANCE	\$500,000	FOR DESIGN-BUILD FINALISTS (2 UNSUCCESSFUL FIRMS)
ARCHITECT & ENGINEERING	\$208,922,513	7.0%	\$14,624,576	BASIC SERVICES
ADDITIONAL CONSULTANT SERVICES	\$208,922,513	2.5%	\$5,223,063	CIVIL, COST EST., I.D., L.A., ACCOUST., ENVELOPE, LEED
DESIGN-BUILD MARKUP ON A&E FEES	\$19,847,639	15.0%	\$2,977,146	TAXES & OVERHEAD COSTS ON A&E FEES FOR DESIGN-BUILDER
PROPERTY ACQUISITION	1	APPRAISAL	\$5,070,000	FOR 20.2 ACRES FROM 11-27-18 K.M. APPRAISAL
OWNER FINANCING COSTS (EST. @ 1% OF TOTAL)	1	ALLOWANCE	\$2,700,000	BOND UNDERWRITING, LEGAL, BOND PLACEMENT FEES
OWNER INTERNAL MANAGEMENT EXPENSES	1	ALLOWANCE	\$500,000	INTERNAL COSTS DIRECTLY ASSOCIATED WITH THIS PROJECT
<b>SUB TOTAL</b>			<b>\$283,396,864</b>	
PROCEEDS FROM SALE OF COUNTY PROPERTY		APPRAISAL	(\$7,800,000)	POTENTIAL FOR ALL COUNTY HILLTOP PARCELS TO BE SURPLUSED
<b>TOTAL PROJECT COST</b>			<b>\$275,596,864</b>	



FURNISHING COST

# Thurston County Courthouse and Civic Center Comprehensive Comparative Feasibility Study

## Furnishings Cost Estimate

		Year 2050	
Thurston County Staff		Open Office	Private Office
1.0	Courthouse Lobby and Common Spaces	5	0
2.0	Superior Courts	34	21
2.5	Pre Trial Services	0	1
3.0	County Clerk	42	5
4.0	District Courts	22	33
5.0	Sheriff - Court Support Area	4	1
6.0	Prosecutor	59	58
7.0	Thurston County Public Defense	34	37
8.0	Drug Court (Outside Courthouse)	1	9
9.0	Sheriff's Office - Administration	27	23
10.0	Sheriff's Office - FOB	6	23
11.0	Assessor	34	7
12.0	Auditor	31	17
13.0	Treasurer	16	3
14.0	Environmental Health	10	3
15.0	Commissioners	12	14
16.0	Development Review	9	1
17.0	Community Planning and Economic Development	49	14
18.0	Information Services	15	1
19.0	Human Resources	21	5
20.0	Central Services	42	12
Staff Subtotal		473	288
Total # of Staff		761	
Cost per staff		\$6,500	\$10,500
Total		\$3,074,500	\$3,024,000
Total Furnishings Cost for Thurston County Staff			\$6,098,500

City of Olympia Staff		Open Office	Private Office
21.0	City of Olympia Municipal Court	34	36
Total # of Staff		70	
Cost per staff		\$6,500	\$10,500
Total		\$221,000	\$378,000
Total Furnishings Cost for City of Olympia Staff			\$599,000

Combined Total Staff		831
Combined Total Furnishings Cost		\$6,697,500

PERMIT FEES



Total City of Olympia Fee Summary					
City of Olympia Fees	Hill-Top Site	Plum St. Site	Plum St. Site City Functions	Harrison West Site	
Impact Fees	\$2,030,100	\$1,403,650	\$187,200		\$2,030,100
Building Permit Fees	\$1,228,380	\$1,414,730	\$2,303		\$1,241,079
Land Use Fees	\$13,630	\$13,630	\$0		\$13,630
Fire Permit Fees	\$28,657.28	\$31,591.73	\$8,845.88		\$26,857.93
MEP fees	\$3,155	\$0	-		\$3,155
Engineering Fees	\$426,819	\$449,379	\$21,629		\$723,811
Total City of Olympia Fees	\$3,730,741	\$3,312,981	\$219,978		\$4,038,632
		\$3,532,959			

Thurston County Courthouse  
Comparative Feasibility Study

City of Olympia Fees

Impact Fees - Transportation				
Impact Fees	Hill-Top Site	Plum St. Site	Plum St. Site City Functions	Harrison West Site
Courthouse Building				
Gross Floor Area	147000 s.f.	147000 s.f.	5000 s.f.	147000 s.f.
Impact Fee Rate	\$6.06	\$4.19	\$4.16	\$6.06
Sub-Total Impact Fees	\$890,820.00	\$615,930.00	\$20,800.00	\$890,820.00
Administrative Building				
Gross Floor Area	188000 s.f.	188000 s.f.	40000 s.f.	188000 s.f.
Impact Fee Rate	\$6.06	\$4.19	\$4.16	\$6.06
Sub-Total Impact Fees	\$1,139,280.00	\$787,720.00	\$166,400.00	\$1,139,280.00
Parking Garage				
Gross Floor Area	190000 s.f.	264000 s.f.	52800 s.f.	151200 s.f.
Impact Fee Rate	\$0.00	\$0.00	\$0.00	\$0.00
Sub-Total Impact Fees	\$0.00	\$0.00	\$0.00	\$0.00
Total Impact Fees	\$2,030,100.00	\$1,403,650.00	\$187,200.00	\$2,030,100.00
		\$1,590,850.00		

Building Permit Fees				
Building Permit Fees	Hill-Top Site	Plum St. Site	Plum St. Site City Functions	Harrison West Site
Construction Value	\$206,781,050	\$238,206,048	\$22,771	\$208,922,513
Permit fee for first fraction	\$8,098	\$8,098	\$8,098	\$8,098
Rate for each additional \$1,000	\$5.93	\$5.93	\$5.93	\$5.93
Variable Rate Subtotal	\$1,220,282	\$1,406,632	-\$5,795	\$1,232,981
Building Permit Fee Total	\$1,228,380	\$1,414,730	\$2,303	\$1,241,079



Land Use & Planning Application Fees

Land Use fees	Hill-Top Site	Plum St. Site	Plum St. Site City Functions	Harrison West Site
Pre-Submission Conference	\$240	\$240	-	\$240
Site Plan Review Fee	\$11,500	\$11,500	-	\$11,500
Design Review - Concept	\$900	\$900	-	\$900
Design Review - Details	\$900	\$900	-	\$900
Sign Review (\$55/ sign)	\$330	\$330	-	\$330
Land Use Fee Totals	\$13,630	\$13,630	\$0	\$13,630
		\$13,630		

Fire System Permit Fees				
Fire System Permit Fees	Hill-Top Site	Plum St. Site	Plum St. Site City Functions	Harrison West Site
Courthouse Building				
Gross Floor Area	147000 s.f.	147000 s.f.	5000 s.f.	147000 s.f.
System Value \$2.50/s.f.	\$367,500	\$367,500	\$12,500	\$367,500
Permit fee for first factor	\$3,292	\$3,292	\$3,292	\$3,292
Rate for each additional \$1,000	\$18.55	\$18.55	\$18.55	\$18.55
Sub-Total Fire Permit Fees	\$4,962	\$4,962	\$1,623	\$4,962
Courthouse Building sub-total	\$8,254	\$8,254	\$1,669	\$8,254
Administrative Building				
Gross Floor Area	188000 s.f.	188000 s.f.	40000 s.f.	188000 s.f.
System Value \$2.50/s.f.	\$470,000	\$470,000	\$100,000	\$470,000
Permit fee for first factor	\$3,292	\$3,292	\$3,292	\$3,292
Rate for each additional \$1,000	\$18.55	\$18.55	\$18.55	\$18.55
Sub-Total Fire Permit Fees	\$6,864	\$6,864	\$0	\$6,864
Admin Building sub-total	\$10,155	\$10,155	\$3,292	\$10,155
Parking Garage				
Gross Floor Area	190000 s.f.	264000 s.f.	52800 s.f.	151200 s.f.
System Value \$2.50/s.f.	\$475,000	\$660,000	\$132,000	\$378,000
Permit fee for first factor	\$3,292	\$10,664	\$3,292	\$3,292
Rate for each additional \$1,000	\$18.55	\$15.74	\$18.55	\$18.55
Sub-Total Fire Permit Fees	\$6,956	\$2,518	\$594	\$5,157
Admin Building sub-total	\$10,248	\$13,183	\$3,885	\$8,449
Total Fire System Permit Fees	\$28,657.28	\$31,591.73	\$8,845.88	\$26,857.93
		\$40,437.60		

Mechanical/Electrical/Plumbing Permit Fees				
MEP Permit Fees	Hill-Top Site	Plum St. Site	Plum St. Site City Functions	Harrison West Site
Electrical	\$1,755	\$1,755	-	\$1,755
Mechanical	\$600	\$600	-	\$600
Plumbing	\$800	\$800	-	\$800
MEP Permit Fee Totals	\$3,155	\$3,155	\$0	\$3,155
		\$3,155		



Engineering / General Facilities Fees				
Engineering Permit Fees	Hill-Top Site	Plum St. Site	Plum St. Site City Functions	Harrison West Site
Areas by Sq.ft.				
Courthouse	147000 s.f.	147000 s.f.	5000 s.f.	147000 s.f.
Admin Building	188000 s.f.	188000 s.f.	40000 s.f.	188000 s.f.
Parking Garage	190000 s.f.	264000 s.f.	52800 s.f.	151200 s.f.
Total	525000 s.f.	599000 s.f.	97800 s.f.	486200 s.f.
ERU Calculation Office(area/6,509)	80.66	92.03	15.03	74.70
Sewer General Facilities Fees rate	\$1,440	\$1,440	\$1,440	\$3,342
Total Charges \$3,342.44*ERU	\$116,109	\$132,475	\$21,629	\$249,669
Water GFC Charges				
Assume 8" meter	\$196,706	\$196,706	-	\$196,706
Stormwater Charges				
(1,190 per 2,538 sq.ft. of imperv.)	\$87,802	\$93,553	-	\$251,466
Plus \$2.10 per avg. daily trip.	\$2.10 x 2.85	\$2.10 x 2.85	-	\$2.10 x 2.85
Assume 2.85 trips per 1,000 s.f.	\$3,142	\$3,585	-	\$2,910
Water service Installation				
\$3,500 (assumed 2" service)	\$3,500	\$3,500	-	\$3,500
Water meter fees				
2" meter (assumed)	\$877	\$877	-	\$877
Engineering Plan Check Fee				
Curb & Sidewalk	\$877	\$877	-	\$877
\$452.00 + \$0.50 per linear foot	\$1,000	\$1,000	-	\$1,000
Driveway - Commercial				
\$678.00	\$678	\$678	-	\$678
Landscape Plan Review				
\$250.00	\$250	\$250	-	\$250
Solid Waste Pad or Enclosure				
\$125.00	\$125	\$125	-	\$125
Street Lighting				
\$452.00 + \$0.50 per linear foot	\$1,000	\$1,000	-	\$1,000
Traffic Signal				
\$1,355.00 each	\$1,355	\$1,355	-	\$1,355
Trees				
New Commercial - \$1,575.00	\$1,575	\$1,575	-	\$1,575

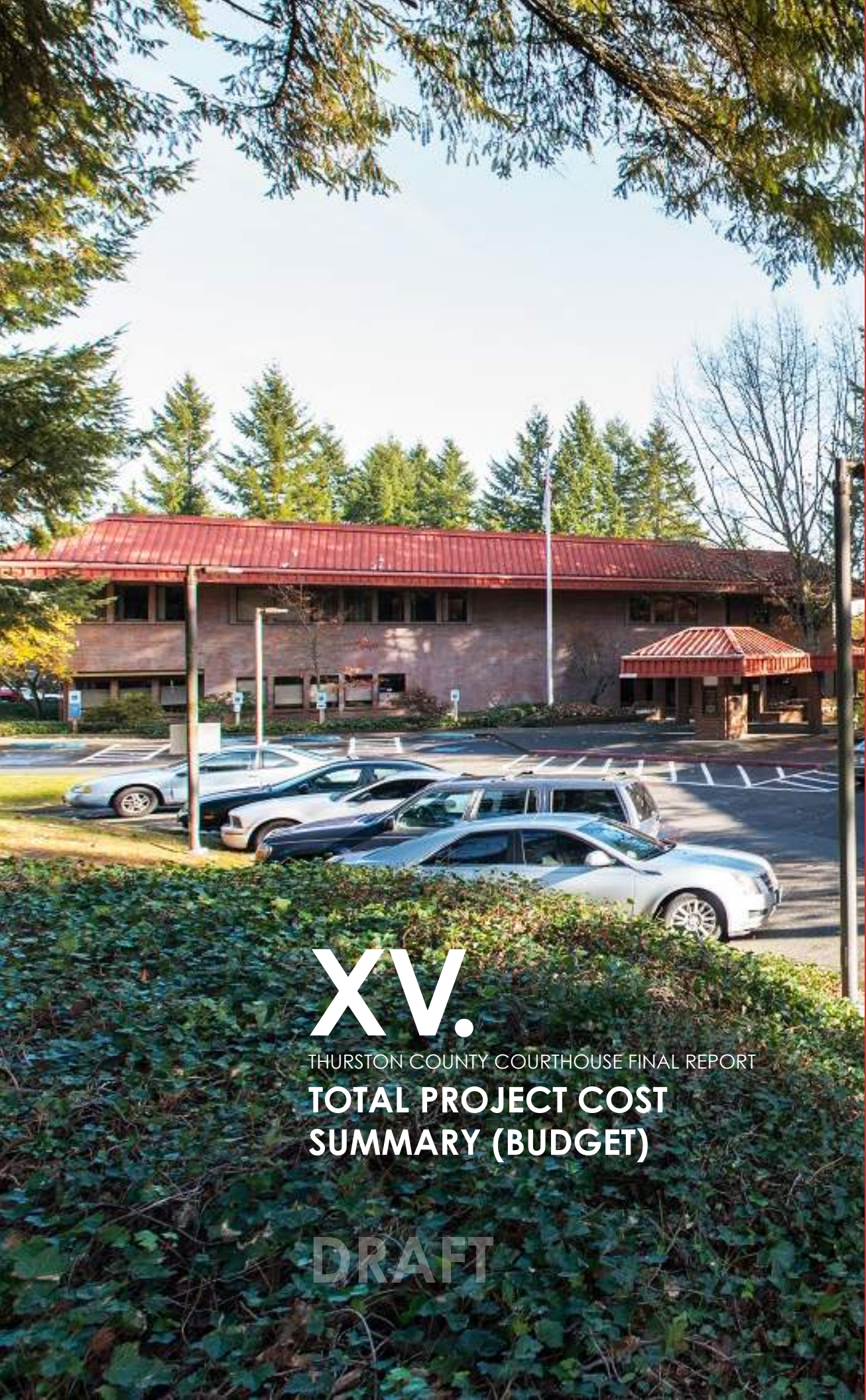
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Thurston County Courthouse  
ComprehensiveComparative Feasibility Study

City of Olympia Fees

Engineering Plan Check Fee subtotal	\$6,860	\$6,860	\$0	\$6,860
Engineering Permit/ Inspection Fees				
Bicycle Parking				
	\$125.00	\$125	-	\$125
Curb and/ or walk				
	\$2.30 per linear foot	\$2,300	-	\$2,300
Parking Lots				
	\$0.06 per square foot	\$1,500	-	\$1,500
Driveway - Commercial				
	\$788.00	\$788	-	\$788
Landscape				
	\$375.00	\$375	-	\$375
Right of Way				
	Obstruction-Traffic control - \$562	\$562	-	\$562
Sewer Lateral				
	New Connection@ main - \$386	\$386	-	\$386
	Connection on property - \$147	\$147	-	\$147
Storm Sewer Main				
	\$3.10 per linear foot	\$300	-	\$300
Storm On-site System				
	\$677.00	\$677	-	\$677
Streets and/ or Alley				
	\$2.30 per linear foot	\$500	-	\$500
Trees				
	New Commercial - \$1,575.00	\$1,575	-	\$1,575
Street Lighting				
	Inside City Limits - \$1.66 per linear ft.	\$1,660	-	\$1,660
Engineering Permit/ Insp. Fees Subtotal		\$10,895	-	\$10,895
Engineering Miscellaneous Fees				
Permit Fee				
	Base = \$250.00	\$250	-	\$250
	\$0.01 per cubic yard	\$100	-	\$100
Grading Permit Fee				
	Equals 165% of the permit fee	\$578	-	\$578
Engineering Misc. Fees Subtotal		\$928	-	\$928
Total Engineering Permits Fees		\$426,819	\$21,629	\$723,811
		\$449,379	\$471,008	





# TOTAL PROJECT COST SUMMARY (BUDGET)

**XV.**

THURSTON COUNTY COURTHOUSE FINAL REPORT

## TOTAL PROJECT COST SUMMARY (BUDGET)

**DRAFT**

TOTAL COSTS  
(BUDGET)



TOTAL PROJECT COST (BUDGET DRIVEN): HILLTOP CAMPUS

Thurston County Courthouse and Civic Center  
Comprehensive Comparative Feasibility Study

Total Project Cost Summary  
Option A1 - Budget Driven

ITEM DESCRIPTION	UNITS	UNIT PRICE	COST	NOTES
<b>CONSTRUCTION COST</b>				
SOUTH CAMPUS BUILDING DEMOLITION				Edits to 12-5-18 Acker Estimate
SOUTH CAMPUS SITE DEVELOPMENT			\$254,200	
4 LEVEL COURTHOUSE BUILDING			\$1,984,000	Reduced to 900 parking spaces and reduced scope by 10%
LOBBY WING			\$45,582,000	Seet Note 1
SOUTH PARKING STRUCTURE			\$2,700,000	Reduced scope by 10% + reduced cost/sf by 10%
BUILDING 3 DEMOLITION			\$10,204,000	3 levels (reduced by 1 level)
NORTH CAMPUS SITE DEVELOPMENT			\$865,000	Adaptive reuse of Buildings 1 & 2 (partial demo only)
NORTH PARKING STRUCTURE			\$3,416,800	Reduced scope by 10% + reduced cost/sf by 10%
NEW ADMINISTRATION BUILDING			\$4,180,000	3 levels (reduced by 1 level)
ADAPTIVE REUSE OF BLDG 1 & 2			\$23,280,000	81,000 SF of new admin building construction (scope - 10%)
SUB TOTAL			\$15,420,000	78,000 SF of adaptive reuse of buildings 1 & 2 (scope - 10%)
25% CONTRACTORS MARKUP	\$107,886,000	25%	\$107,886,000	
SUB TOTAL			\$26,971,500	GEN REQ, OH&P, BOND & INS, B&O TAX, DESIGN CONT.
ESCALATION PREMIUM (12% & 24%)	\$134,857,500	18.0%	\$134,857,500	HARD CONSTRUCTION COSTS IN 2018 DOLLARS
<b>TOTAL CONSTRUCTION COST</b>			\$24,274,350	Average 18% escalation premium (Const start in 2021)
<b>OTHER PROJECT EXPENSES</b>			\$159,131,850	
FURNISHINGS (FF&E)	1	ALLOWANCE	\$6,100,000	
WA STATE SALES TAX	\$159,131,850	8.90%	\$14,162,735	
OWNER'S CONTINGENCY	\$159,131,850	5.00%	\$7,956,593	OWNER ONLY CONTINGENCY (NEW CONST)
CITY OF OLYMPIA PERMITTING & IMPACT FEES	1	ALLOWANCE	\$4,180,850	(EST.) SEE DETAILED LIST OF FEES (REPORT SECTION 11)
CONST. MANAGEMENT SERVICES	\$159,131,850	1.00%	\$1,591,319	SURVEY, GEOTECH, TESTING, COMMISSIONING, CRITERIA DOCS
MOVING EXPENSES	1	ALLOWANCE	\$300,000	400 STAFF X \$750/STAFF
A&E STIPENDS		ALLOWANCE	\$500,000	FOR DESIGN-BUILD FINALISTS (2 UNSUCCESSFUL FIRMS)
ARCHITECT & ENGINEERING	\$159,131,850	7.0%	\$11,139,230	BASIC SERVICES
ADDITIONAL CONSULTANT SERVICES	\$159,131,850	2.5%	\$3,978,296	CIVIL, COST EST., I.D., L.A., ACCOUST., ENVELOPE, LEED
DESIGN-BUILD MARKUP ON A&E FEES	\$15,117,526	15.0%	\$2,267,629	TAXES & OVERHEAD COSTS ON A&E FEES FOR DESIGN-BUILDER
PROPERTY ACQUISITION	1	APPRAISAL	\$800,000	FOR WESTERN BLDG. MAT. (FROM 11-27-18 K.M. APPRAISAL)
OWNER FINANCING COSTS (EST. @ 1% OF TOTAL)	1	ALLOWANCE	\$2,100,000	BOND UNDERWRITING, LEGAL, BOND PLACEMENT FEES
OWNER INTERNAL MANAGEMENT EXPENSES	1	ALLOWANCE	\$500,000	INTERNAL COSTS DIRECTLY ASSOCIATED WITH THIS PROJECT
LEASE EXPENSES FOR COUNTY (45,000 SF X \$20/SF)		ALLOWANCE	\$1,800,000	RELOCATE BUILDING 1 STAFF FOR 2 YEARS
COSTS ASSOCIATED WITH LEASING OFF SITE		ALLOWANCE	\$3,640,500	DESIGN, T.I. CONSTRUCTION, IT CABLE, FURNITURE, MOVING
<b>SUB TOTAL</b>			\$220,149,000	
PROCEEDS FROM SALE OF COUNTY PROPERTY		APPRAISAL	(\$2,000,000)	SELECT COUNTY HILLTOP PARCELS COULD BE SURPLUSED
<b>TOTAL PROJECT COST</b>			\$218,149,000	

Note: 1. Reduced program 10%, eliminated basement parking, reduced penthouse & holding area premiums \$/SF.

Thurston County Courthouse and Civic Center  
Comprehensive Comparative Feasibility Study

Total Project Cost Summary  
Option A2 - Budget Driven

ITEM DESCRIPTION	UNITS	UNIT PRICE	COST	NOTES
<b>CONSTRUCTION COST</b>				
BUILDING 3 DEMOLITION			\$640,000	
INITIAL SITE DEVELOPMENT			\$1,970,400	<i>Edits to 12-5-18 Acker estimate</i> <i>Reduced to 900 parking spaces and reduced scope by 10%</i>
5 LEVEL COURTHOUSE BUILDING			\$45,582,000	<i>See Note 1</i>
PARKING STRUCTURE			\$9,180,000	<i>5 levels of parking (4 levels of structure) (reduced by 2)</i>
BUILDING 2 DEMOLITION			\$260,000	
SOUTH SITE DEVELOPMENT			\$1,752,000	<i>Reduced scope by 10% + reduced cost/sf by 10%</i>
LOBBY WING			\$2,700,000	<i>Reduced scope by 10% + reduced cost/sf by 10% (+ no curve)</i>
7 LEVEL ADMINISTRATION BUILDING			\$43,530,000	<i>Similar to Note 1</i>
BUILDING 1 DEMO & NORTH SITE DEVELOPMENT			\$1,302,400	<i>Reduced scope by 10% + reduced cost/sf by 10%</i>
SUB TOTAL			<b>\$106,916,800</b>	
25% CONTRACTORS MARKUP	\$106,916,800	25%	\$26,729,200	GEN REQ, OH&P, BOND & INS, B&O TAX, DESIGN CONT.
SUB TOTAL			<b>\$133,646,000</b>	HARD CONSTRUCTION COSTS IN 2018 DOLLARS
ESCALATION PREMIUM (12% & 24%)	\$133,646,000	18.0%	\$24,056,280	Average 18% escalation premium (const. start in 2021)
<b>TOTAL CONSTRUCTION COST</b>			<b>\$157,702,280</b>	
<b>OTHER PROJECT EXPENSES</b>				
FURNISHINGS (FF&E)	1	ALLOWANCE	\$6,100,000	
WA STATE SALES TAX	\$157,702,280	8.90%	\$14,035,503	
OWNER'S CONTINGENCY	\$157,702,280	5.00%	\$7,885,114	OWNER ONLY CONTINGENCY (NEW CONST)
CITY OF OLYMPIA PERMITTING & IMPACT FEES	1	ALLOWANCE	\$4,180,850	(EST.) SEE DETAILED LIST OF FEES (REPORT SECTION 11)
CONST. MANAGEMENT SERVICES	\$157,702,280	<b>1.00%</b>	\$2,365,534	SURVEY, GEOTECH, TESTING, COMMISSIONING, CRITERIA DOCS
MOVING EXPENSES	1	ALLOWANCE	\$300,000	400 STAFF X \$750/STAFF
A&E STIPENDS		ALLOWANCE	\$500,000	FOR DESIGN-BUILD FINALISTS (2 UNSUCCESSFUL FIRMS)
ARCHITECT & ENGINEERING	\$157,702,280	7.0%	\$11,039,160	BASIC SERVICES
ADDITIONAL CONSULTANT SERVICES	\$157,702,280	2.5%	\$3,942,557	CIVIL, COST EST., I.D., L.A., ACCOUST., ENVELOPE, LEED
DESIGN-BUILD MARKUP ON A&E FEES	\$14,981,717	15.0%	\$2,247,257	TAXES & OVERHEAD COSTS ON A&E FEES FOR DESIGN-BUILDER
PROPERTY ACQUISITION	1	APPRAISAL	\$800,000	FOR WESTERN BLDG. MAT. (FROM 11-27-18 K.M. APPRAISAL)
OWNER FINANCING COSTS (EST. @ 1% OF TOTAL)	1	ALLOWANCE	\$2,700,000	BOND UNDERWRITING, LEGAL, BOND PLACEMENT FEES
OWNER INTERNAL MANAGEMENT EXPENSES	1	ALLOWANCE	\$500,000	INTERNAL COSTS DIRECTLY ASSOCIATED WITH THIS PROJECT
LEASE EXPENSES FOR COUNTY (22,000 SF X \$20/SF)	1	ALLOWANCE	\$880,000	RELOCATE BUILDING 3 OCCUPANTS FOR 2 YEARS
COSTS ASSOCIATED WITH LEASING OFF SITE	1	ALLOWANCE	\$1,768,250	DESIGN, T.I. CONSTRUCTION, IT CABLE, FURNITURE, MOVING
<b>SUB TOTAL</b>			<b>\$216,946,505</b>	
PROCEEDS FROM SALE OF COUNTY PROPERTY		APPRAISAL	(\$2,000,000)	SELECT COUNTY HILLTOP PARCELS COULD BE SURPLUSED
<b>TOTAL PROJECT COST</b>			<b>\$214,946,505</b>	

Note: 1. Reduced program 10%, eliminated basement parking, reduced penthouse & holding area premiums \$/SF.



ITEM DESCRIPTION	UNITS	UNIT PRICE	COST	NOTES
<b>CONSTRUCTION COST</b>				
BUILDING 3 DEMOLITION			\$640,000	<i>Edits to 12-5-18 Acker estimate</i>
INITIAL SITE DEVELOPMENT			\$4,260,000	<i>Reduced to 900 parking spaces and reduced scope by 10%</i>
4 LEVEL COURTHOUSE BUILDING			\$48,228,000	<i>See Note 1</i>
LOBBY WING			\$2,700,000	<i>Reduced scope by 10% + reduced cost/sf by 10%</i>
5 LEVEL ADMINISTRATION BUILDING			\$46,710,000	<i>Similar to Note 1</i>
NORTH PARKING STRUCTURE			\$7,776,000	<i>3 levels of parking (2 levels of structure) (1 level less)</i>
BUILDING 1 & 2 DEMOLITION			\$550,000	
BALANCE OF SITE DEVELOPMENT			\$2,405,200	<i>Reduced scope by 10% + reduced cost/sf by 10%</i>
SOUTH PARKING STRUCTURE			\$5,616,000	
SUB TOTAL			<b>\$118,885,200</b>	
25% CONTRACTORS MARKUP	\$118,885,200	25%	\$29,721,300	GEN REQ, OH&P, BOND & INS, B&O TAX, DESIGN CONT.
SUB TOTAL			\$148,606,500	HARD CONSTRUCTION COSTS IN 2018 DOLLARS
ESCALATION PREMIUM (12% & 24%)		18.0%	\$26,749,170	<i>Average 18% escalation premium (construction start in 2021)</i>
<b>TOTAL CONSTRUCTION COST</b>			<b>\$175,355,670</b>	
<b>OTHER PROJECT EXPENSES</b>				
FURNISHINGS (FF&E)	1	ALLOWANCE	\$6,100,000	
WA STATE SALES TAX	\$175,355,670	8.90%	\$15,606,655	
OWNER'S CONTINGENCY	\$175,355,670	5.00%	\$8,767,784	OWNER ONLY CONTINGENCY (NEW CONST)
CITY OF OLYMPIA PERMITTING & IMPACT FEES	1	ALLOWANCE	\$4,180,850	(EST.) SEE DETAILED LIST OF FEES (REPORT SECTION 11)
CONST. MANAGEMENT SERVICES	\$175,355,670	<b>1.00%</b>	\$1,753,557	SURVEY, GEOTECH, TESTING, COMMISSIONING, CRITERIA DOCS
MOVING EXPENSES	1	ALLOWANCE	\$300,000	400 STAFF X \$750/STAFF
A&E STIPENDS		ALLOWANCE	\$500,000	FOR DESIGN-BUILD FINALISTS (2 UNSUCCESSFUL FIRMS)
ARCHITECT & ENGINEERING	\$175,355,670	7.0%	\$12,274,897	BASIC SERVICES
ADDITIONAL CONSULTANT SERVICES	\$175,355,670	2.5%	\$4,383,892	CIVIL, COST EST., I.D., L.A., ACCOUST., ENVELOPE, LEED
DESIGN-BUILD MARKUP ON A&E FEES	\$16,658,789	15.0%	\$2,498,818	TAXES & OVERHEAD COSTS ON A&E FEES FOR DESIGN-BUILDER
OWNER FINANCING COSTS (EST. @ 1% OF TOTAL)	1	ALLOWANCE	\$2,300,000	BOND UNDERWRITING, LEGAL, BOND PLACEMENT FEES
OWNER INTERNAL MANAGEMENT EXPENSES	1	ALLOWANCE	\$500,000	INTERNAL COSTS DIRECTLY ASSOCIATED WITH THIS PROJECT
LEASE EXPENSES FOR COUNTY (22,000 SF X \$20/SF)	1	ALLOWANCE	\$880,000	RELOCATE BUILDING 3 OCCUPANTS FOR 2 YEARS
COSTS ASSOCIATED WITH LEASING OFF SITE	1	ALLOWANCE	\$1,768,250	DESIGN, T.I. CONSTRUCTION, IT CABLE, FURNITURE, MOVING
<b>SUB TOTAL</b>			<b>\$237,170,372</b>	
PROCEEDS FROM SALE OF COUNTY PROPERTY		APPRAISAL	(\$3,000,000)	SELECT COUNTY HILLTOP PARCELS COULD BE SURPLUSED
<b>TOTAL PROJECT COST</b>			<b>\$234,170,372</b>	

Notes:

1. Reduced program 10%, eliminated basement parking, reduced penthouse & LEED \$/SF, reduced courtroom & holding area premiums \$/SF.

Thomas Architecture Studios / HOK

12/6/2018

ITEM DESCRIPTION	UNITS	UNIT PRICE	COST NOTES
<b>CONSTRUCTION COST</b>			
BUILDING 3 DEMOLITION			<i>Edits to 12-5-18 Acker estimate</i>
INITIAL SITE DEVELOPMENT			\$670,000
7 LEVEL COURTHOUSE BUILDING			\$3,484,000 <i>Reduced to 900 parking spaces and reduced scope by 10%</i>
LOBBY WING			\$45,582,000 <i>See Note 1</i>
NORTH PARKING STRUCTURE			\$2,700,000 <i>Reduced scope by 10% + reduced cost/sf by 10%</i>
BUILDING 1 & 2 DEMOLITION (PARTIAL)			\$12,852,000 <i>5 levels of parking (4 levels of structure) (2 less levels)</i>
BALANCE OF NORTH CAMPUS SITE DEVELOPMENT			\$520,000 <i>Selective demolition for adaptive reuse</i>
NEW ADMINISTRATION BUILDING			\$1,880,000
ADAPTIVE REUSE OF BUILDINGS 1 & 2			\$23,280,000 <i>81,000 SF of new admin building construction (scope - 10%)</i>
SOUTH CAMPUS SITE DEVELOPMENT			\$15,420,000 <i>78,000 sf of adaptive reuse of buildings 1 &amp; 2 (scope - 10%)</i>
SUB TOTAL			\$0 <i>No work in the area of buildings 4 &amp; 6</i>
25% CONTRACTORS MARKUP	\$106,388,000	25%	<b>\$106,388,000</b>
SUB TOTAL			\$26,597,000 GEN REQ, OH&P, BOND & INS, B&O TAX, DESIGN CONT.
ESCALATION PREMIUM (12% & 24%)	\$132,985,000	18.0%	\$132,985,000 HARD CONSTRUCTION COSTS IN 2018 DOLLARS
<b>TOTAL CONSTRUCTION COST</b>			<b>\$23,937,300</b> <i>Average 18% escalation premium (construction start 2021)</i>
<b>OTHER PROJECT EXPENSES</b>			<b>\$156,922,300</b>
FURNISHINGS (FF&E)	1	ALLOWANCE	\$6,100,000
WA STATE SALES TAX	\$156,922,300	8.90%	\$13,966,085
OWNER'S CONTINGENCY	\$156,922,300	5.00%	\$7,846,115 OWNER ONLY CONTINGENCY (NEW CONST)
CITY OF OLYMPIA PERMITTING & IMPACT FEES	1	ALLOWANCE	\$4,180,850 (EST.) SEE DETAILED LIST OF FEES (REPORT SECTION 11)
CONST. MANAGEMENT SERVICES	\$156,922,300	1.00%	\$1,569,223 SURVEY, GEOTECH, TESTING, COMMISSIONING, CRITERIA DOCS
MOVING EXPENSES	1	ALLOWANCE	\$300,000 400 STAFF X \$750/STAFF
A&E STIPENDS		ALLOWANCE	\$500,000 FOR DESIGN-BUILD FINALISTS (2 UNSUCCESSFUL FIRMS)
ARCHITECT & ENGINEERING	\$156,922,300	7.0%	\$10,984,561 BASIC SERVICES
ADDITIONAL CONSULTANT SERVICES	\$156,922,300	2.5%	\$3,923,058 CIVIL, COST EST., I.D., L.A., ACCOUST., ENVELOPE, LEED
DESIGN-BUILD MARKUP ON A&E FEES	\$14,907,619	15.0%	\$2,236,143 TAXES & OVERHEAD COSTS ON A&E FEES FOR DESIGN-BUILDER
OWNER FINANCING COSTS (EST. @ 1% OF TOTAL)	1	ALLOWANCE	\$2,100,000 BOND UNDERWRITING, LEGAL, BOND PLACEMENT FEES
OWNER INTERNAL MANAGEMENT EXPENSES	1	ALLOWANCE	\$500,000 INTERNAL COSTS DIRECTLY ASSOCIATED WITH THIS PROJECT
LEASE EXPENSES FOR COUNTY (45,000 SF X \$20/SF)		ALLOWANCE	\$1,800,000 RELOCATE BUILDING 1 STAFF FOR 2 YEARS
COSTS ASSOCIATED WITH LEASING OFF SITE		ALLOWANCE	\$3,640,500 DESIGN, T.I. CONSTRUCTION, IT CABLE, FURNITURE, MOVING
<b>SUB TOTAL</b>			<b>\$216,568,834</b>
PROCEEDS FROM SALE OF COUNTY PROPERTY		APPRAISAL	(\$2,000,000) SELECT COUNTY HILLTOP PARCELS COULD BE SURPLUSED
<b>TOTAL PROJECT COST</b>			<b>\$214,568,834</b>

Notes: 1. Reduced program 10%, eliminated basement parking, reduced penthouse & LEED \$/SF, reduced courtroom & holding area premiums \$/SF.

ITEM DESCRIPTION	UNITS	UNIT PRICE	COST NOTES
<b>CONSTRUCTION COST</b>			
BUILDING 3 DEMOLITION			<i>Edits to 12-5-18 Acker estimate</i>
INITIAL SITE DEVELOPMENT			\$670,000
NORTH PARKING STRUCTURE			\$1,018,000 <i>Reduced to 900 parking spaces and reduced scope by 10%</i>
BUILDING 1 DEMOLITION			\$9,828,000 <i>4 levels of parking (3 levels of structure) (3 less levels)</i>
BALANCE OF NORTH CAMPUS SITE WORK			\$300,000
5 LEVEL COURTHOUSE BUILDING			\$3,796,800 <i>Reduced scope by 10% + reduced cost/sf by 10%</i>
LOBBY WING			\$45,582,000 <i>See Note 1</i>
7 LEVEL ADMINISTRATION BUILDING			\$2,700,000 <i>Reduced scope by 10% + reduced cost/sf by 10%</i>
BUILDING 2 DEMO & BALANCE OF SITE DEV.			\$43,530,000 <i>Similar to Note 1</i>
SOUTH PARKING STRUCTURE			\$1,652,000 <i>Reduced scope by 10% + reduced cost/sf by 10%</i>
SUB TOTAL			\$7,000,000
25% CONTRACTORS MARKUP	\$116,076,800	25%	<b>\$116,076,800</b>
SUB TOTAL			\$29,019,200 GEN REQ, OH&P, BOND & INS, B&O TAX, DESIGN CONT.
ESCALATION PREMIUM (12% & 24%)	\$145,096,000	18.0%	\$145,096,000 HARD CONSTRUCTION COSTS IN 2018 DOLLARS
<b>TOTAL CONSTRUCTION COST</b>			\$26,117,280 Average 18% escalation premium (construction start 2021)
			<b>\$171,213,280</b>
<b>OTHER PROJECT EXPENSES</b>			
FURNISHINGS (FF&E)	1	ALLOWANCE	\$6,100,000
WA STATE SALES TAX	\$171,213,280	8.90%	\$15,237,982
OWNER'S CONTINGENCY	\$171,213,280	5.00%	\$8,560,664 OWNER ONLY CONTINGENCY (NEW CONST)
CITY OF OLYMPIA PERMITTING & IMPACT FEES	1	ALLOWANCE	\$4,180,850 (EST.) SEE DETAILED LIST OF FEES (REPORT SECTION 11)
CONST. MANAGEMENT SERVICES	\$171,213,280	<b>1.00%</b>	\$1,712,133 SURVEY, GEOTECH, TESTING, COMMISSIONING, CRITERIA DOCS
MOVING EXPENSES	1	ALLOWANCE	\$300,000 400 STAFF X \$750/STAFF
A&E STIPENDS		ALLOWANCE	\$500,000 FOR DESIGN-BUILD FINALISTS (2 UNSUCCESSFUL FIRMS)
ARCHITECT & ENGINEERING	\$171,213,280	7.0%	\$11,984,930 BASIC SERVICES
ADDITIONAL CONSULTANT SERVICES	\$171,213,280	2.5%	\$4,280,332 CIVIL, COST EST., I.D., L.A., ACCOUST., ENVELOPE, LEED
DESIGN-BUILD MARKUP ON A&E FEES	\$16,265,262	15.0%	\$2,439,789 TAXES & OVERHEAD COSTS ON A&E FEES FOR DESIGN-BUILDER
OWNER FINANCING COSTS (EST. @ 1% OF TOTAL)	1	ALLOWANCE	\$2,300,000 BOND UNDERWRITING, LEGAL, BOND PLACEMENT FEES
OWNER INTERNAL MANAGEMENT EXPENSES	1	ALLOWANCE	\$500,000 INTERNAL COSTS DIRECTLY ASSOCIATED WITH THIS PROJECT
LEASE EXPENSES FOR COUNTY (45,000 SF X \$20/SF)	1	ALLOWANCE	\$1,800,000 RELOCATE BUILDING 1 OCCUPANTS FOR 2 YEARS
COSTS ASSOCIATED WITH LEASING OFF SITE	1	ALLOWANCE	\$3,640,500 DESIGN, I.T. CONSTRUCTION, TI CABLE, FURNITURE, MOVING
<b>SUB TOTAL</b>			<b>\$234,750,460</b>
PROCEEDS FROM SALE OF COUNTY PROPERTY		APPRAISAL	(\$3,000,000) SELECT COUNTY HILLTOP PARCELS COULD BE SURPLUSED
<b>TOTAL PROJECT COST</b>			<b>\$231,750,460</b>

Notes: 1. Reduced program 10%, eliminated basement parking, reduced penthouse & LEED \$/SF, reduced courtroom & holding area premiums \$/SF.



TOTAL PROJECT COST (BUDGET DRIVEN): PLUM STREET

Thurston County Courthouse and Civic Center  
Comprehensive Comparative Feasibility Study

Total Project Cost Summary  
Option B1 - Budget Driven

ITEM DESCRIPTION	UNITS	UNIT PRICE	COST NOTES
<b>CONSTRUCTION COST</b>			
SITE PREP & IMPROVEMENTS			<i>Edits to 12-5-18 ACKER ESTIMATE</i>
7 LEVEL COURTHOUSE BUILDING			\$6,400,000 Reduce to 900 parking spaces and reduce scope by 10%
LOBBY WING			\$53,358,000 See Note 1
7 LEVEL ADMINISTRATION BUILDING			\$2,950,000 Reduce scope by 10% + reduce cost/sf by 10%
STRUCTURED PARKING			\$37,783,500 Similar to Note 1
CITY OF OLYMPIA MUNICIPAL SPACE			\$15,840,000 4 LEVELS (reduced by 1 level)
SUB TOTAL			\$10,290,000 Reduced to 35k program + courts shared with County
			<b>\$126,621,500</b>
25% CONTRACTORS MARKUP	\$126,621,500	25%	\$31,655,375 GEN REQ, OH&P, BOND & INS, B&O TAX, DESIGN CONT.
SUB TOTAL			\$158,276,875
ESCALATION PREMIUM	\$158,276,875	12.0%	\$158,276,875 HARD CONSTRUCTION COSTS IN 2018 DOLLARS
			\$18,993,225 CONSTRUCTION WORK START IN SPRING OF 2021
<b>TOTAL CONSTRUCTION COST</b>			<b>\$177,270,100</b>
<b>OTHER PROJECT EXPENSES</b>			
FURNISHINGS (FF&E)	1	ALLOWANCE	\$6,700,000 \$6,100,000(COUNTY) + \$600,000 (CITY)
WA STATE SALES TAX	\$177,270,100	8.90%	\$15,777,039
OWNER'S CONTINGENCY	\$177,270,100	5.00%	\$8,863,505 OWNER ONLY CONTINGENCY (NEW CONST)
CITY OF OLYMPIA PERMITTING & IMPACT FEES	1	ALLOWANCE	\$4,094,900 (EST.) SEE DETAILED LIST OF FEES (REPORT SECTION 11)
CONST. MANAGEMENT SERVICES	\$177,270,100	1.00%	\$1,772,701 SURVEY, GEOTECH, TESTING, COMMISSIONING, CRITERIA DOCS
MOVING EXPENSES	1	ALLOWANCE	\$337,500 450 STAFF X \$750/STAFF
A&E STIPENDS		ALLOWANCE	\$500,000 FOR DESIGN-BUILD FINALISTS (2 UNSUCCESSFUL FIRMS)
ARCHITECT & ENGINEERING	\$177,270,100	7.0%	\$12,408,907 BASIC SERVICES
ADDITIONAL CONSULTANT SERVICES	\$177,270,100	2.5%	\$4,431,753 CIVIL, COST EST., I.D., L.A., ACCOUST., ENVELOPE, LEED
DESIGN-BUILD MARKUP ON A&E FEES	\$16,840,660	15.0%	\$2,526,099 TAXES & OVERHEAD COSTS ON A&E FEES FOR DESIGN-BUILDER
PROPERTY ACQUISITION	1	APPRAISAL	\$4,422,200 FOR 10.47 ACRES FROM 11-27-18 K.M. APPRAISAL
OWNER FINANCING COSTS (EST. @ 1% OF TOTAL)	1	ALLOWANCE	\$2,400,000 BOND UNDERWRITING, LEGAL, BOND PLACEMENT FEES
OWNER INTERNAL MANAGEMENT EXPENSES	1	ALLOWANCE	\$500,000 INTERNAL COSTS DIRECTLY ASSOCIATED WITH THIS PROJECT
LEASE EXPENSES FOR CITY (24,000 SF X \$20/SF)	1	ALLOWANCE	\$960,000 RELOCATE CREIGHTON JUSTICE CENTER STAFF FOR 2 YEARS
COSTS ASSOCIATED WITH LEASING OFF SITE	1	ALLOWANCE	\$1,952,500 DESIGN, T.I. CONSTRUCTION, IT CABLE, FURNITURE, MOVING
<b>SUB TOTAL</b>			<b>\$244,917,203</b>
PROCEEDS FROM SALE OF COUNTY PROPERTY		APPRAISAL	(\$7,800,000) POTENTIAL FOR ALL COUNTY HILLTOP PARCELS TO BE SURPLUSED
<b>TOTAL PROJECT COST</b>			<b>\$237,117,203</b> <i>County portion = \$237,117,203 - \$22,274,164 = \$214,843,039</i>

Notes: 1. Reduced program 10%, eliminated basement parking, reduced penthouse & LEED \$/SF, reduced courtroom & holding area premiums \$/SF.

2. The Total Project Cost for the City of Olympia Municipal Space = \$22,274,164

Thurston County Courthouse and Civic Center  
Comprehensive Comparative Feasibility Study

Total Project Cost Summary  
Option B2 - Budget Driven

ITEM DESCRIPTION	UNITS	UNIT PRICE	COST NOTES
<b>CONSTRUCTION COST</b>			
SITE PREP & IMPROVEMENTS			<i>Edits to 12-5-18 Acker Estimate</i>
7 LEVEL COURTHOUSE BUILDING			\$6,216,000 <i>Reduced to 900 parking spaces and reduced scope by 10%</i>
LOBBY WING			\$53,358,000 <i>See Note 1</i>
7 LEVEL ADMINISTRATION BUILDING			\$2,950,000 <i>Reduced scope by 10% + reduced cost/sf by 10%</i>
STRUCTURED PARKING			\$37,783,500 <i>Similar to Note 1</i>
CITY OF OLYMPIA MUNICIPAL SPACE			\$15,840,000 <i>4 levels (reduced by 1 level)</i>
SUB TOTAL			\$10,290,000 <i>Reduced to 35k program + courts shared with County</i>
25% CONTRACTORS MARKUP	\$126,437,500	25%	<b>\$126,437,500</b>
SUB TOTAL			\$31,609,375 GEN REQ, OH&P, BOND & INS, B&O TAX, DESIGN CONT.
ESCALATION PREMIUM	\$158,046,875	12.0%	\$158,046,875 HARD CONSTRUCTION COSTS IN 2018 DOLLARS
<b>TOTAL CONSTRUCTION COST</b>			\$18,965,625 CONSTRUCTION WORK START IN SPRING OF 2021
			<b>\$177,012,500</b>
<b>OTHER PROJECT EXPENSES</b>			
FURNISHINGS (FF&E)	1	ALLOWANCE	\$6,700,000 \$6,100,000 (COUNTY) + \$600,000 (CITY)
WA STATE SALES TAX	\$177,012,500	8.90%	\$15,754,113
OWNER'S CONTINGENCY	\$177,012,500	5.00%	\$8,850,625 OWNER ONLY CONTINGENCY (NEW CONST)
CITY OF OLYMPIA PERMITTING & IMPACT FEES	1	ALLOWANCE	\$4,094,900 (EST.) SEE DETAILED LIST OF FEES (REPORT SECTION 11)
CONST. MANAGEMENT SERVICES	\$177,012,500	1.00%	\$1,770,125 SURVEY, GEOTECH, TESTING, COMMISSIONING, CRITERIA DOCS
MOVING EXPENSES	1	ALLOWANCE	\$337,500 450 STAFF X \$750/STAFF
A&E STIPENDS		ALLOWANCE	\$500,000 FOR DESIGN-BUILD FINALISTS (2 UNSUCCESSFUL FIRMS)
ARCHITECT & ENGINEERING	\$177,012,500	7.0%	\$12,390,875 BASIC SERVICES
ADDITIONAL CONSULTANT SERVICES	\$177,012,500	2.5%	\$4,425,313 CIVIL, COST EST., I.D., L.A., ACCOUST., ENVELOPE, LEED
DESIGN-BUILD MARKUP ON A&E FEES	\$16,816,188	15.0%	\$2,522,428 TAXES & OVERHEAD COSTS ON A&E FEES FOR DESIGN-BUILDER
PROPERTY ACQUISITION	1	APPRAISAL	\$4,422,200 FOR 10.47 ACRES FROM 11-27-18 K.M. APPRAISAL
OWNER FINANCING COSTS (EST. @ 1% OF TOTAL)	1	ALLOWANCE	\$2,400,000 BOND UNDERWRITING, LEGAL, BOND PLACEMENT FEES
OWNER INTERNAL MANAGEMENT EXPENSES	1	ALLOWANCE	\$500,000 INTERNAL COSTS DIRECTLY ASSOCIATED WITH THIS PROJECT
LEASE EXPENSES FOR CITY (24,000 SF X \$20/SF)	1	ALLOWANCE	\$960,000 RELOCATE CREIGHTON JUSTICE CENTER STAFF FOR 2 YEARS
COSTS ASSOCIATED WITH LEASING OFF SITE	1	ALLOWANCE	\$1,952,500 DESIGN, T.I. CONSTRUCTION, IT CABLE, FURNITURE, MOVING
<b>SUB TOTAL</b>			<b>\$244,593,078</b>
PROCEEDS FROM SALE OF COUNTY PROPERTY		APPRAISAL	POTENTIAL FOR ALL COUNTY HILLTOP PARCELS TO BE SURPLUSED
<b>TOTAL PROJECT COST</b>			<b>\$236,793,078</b> <i>County portion = \$236,793,078 - \$22,274,164 = \$214,518,914</i>

Notes:

1. Reduced program 10%, eliminated basement parking, reduced penthouse & LEED \$/SF, reduced courtroom & holding area premiums \$/SF.
2. The Total Project Cost for the City of Olympia Municipal Space = *\$22,274,164*



TOTAL PROJECT COST (BUDGET DRIVEN): HARRISON WEST

ITEM DESCRIPTION	UNITS	UNIT PRICE	COST NOTES
<b>CONSTRUCTION COST</b>			
SITE PREP & IMPROVEMENTS			<i>Edits to 12-5-18 ACKER ESTIMATE</i>
OFFSITE INFRASTRUCTURE			\$11,452,500 <i>Reduce to 900 parking spaces and reduce scope by 10%</i>
5 LEVEL COURTHOUSE BUILDING			\$3,200,000 <i>Reduce scope by 33%</i>
LOBBY WING			\$45,582,000 <i>See Note 1</i>
6 LEVEL ADMINISTRATION BUILDING			\$2,860,000 <i>Reduce scope by 10% + reduce cost/sf by 10%</i>
SUB TOTAL			\$43,530,000 <i>Similar to Note 1</i>
25% CONTRACTORS MARKUP	\$106,624,500	25%	<b>\$106,624,500</b>
SUB TOTAL			\$26,656,125 GEN REQ, OH&P, BOND & INS, B&O TAX, DESIGN CONT.
ESCALATION PREMIUM	\$133,280,625		\$133,280,625 HARD CONSTRUCTION COSTS IN 2018 DOLLARS
<b>TOTAL CONSTRUCTION COST</b>		12.0%	<b>\$15,993,675</b> CONSTRUCTION WORK START IN SPRING OF 2021
<b>OTHER PROJECT EXPENSES</b>			
FURNISHINGS (FF&E)	1	ALLOWANCE	\$6,100,000
WA STATE SALES TAX	\$149,274,300	8.90%	\$13,285,413
OWNER'S CONTINGENCY	\$149,274,300	5.00%	\$7,463,715 OWNER ONLY CONTINGENCY (NEW CONST)
CITY OF OLYMPIA PERMITTING & IMPACT FEES	1	ALLOWANCE	\$4,305,500 (EST.) SEE DETAILED LIST OF FEES (REPORT SECTION 11)
CONST. MANAGEMENT SERVICES	\$149,274,300	1.00%	\$1,492,743 SURVEY, GEOTECH, TESTING, COMMISSIONING, CRITERIA DOCS
MOVING EXPENSES	1	ALLOWANCE	\$300,000 400 STAFF X \$750/STAFF
A&E STIPENDS		ALLOWANCE	\$500,000 FOR DESIGN-BUILD FINALISTS (2 UNSUCCESSFUL FIRMS)
ARCHITECT & ENGINEERING	\$149,274,300	7.0%	\$10,449,201 BASIC SERVICES
ADDITIONAL CONSULTANT SERVICES	\$149,274,300	2.5%	\$3,731,858 CIVIL, COST EST., I.D., L.A., ACCOUST., ENVELOPE, LEED
DESIGN-BUILD MARKUP ON A&E FEES	\$14,181,059	15.0%	\$2,127,159 TAXES & OVERHEAD COSTS ON A&E FEES FOR DESIGN-BUILDER
PROPERTY ACQUISITION	1	APPRAISAL	\$5,070,000 FOR 20.2 ACRES FROM 11-27-18 K.M. APPRAISAL
OWNER FINANCING COSTS (EST. @ 1% OF TOTAL)	1	ALLOWANCE	\$2,000,000 BOND UNDERWRITING, LEGAL, BOND PLACEMENT FEES
OWNER INTERNAL MANAGEMENT EXPENSES	1	ALLOWANCE	\$500,000 INTERNAL COSTS DIRECTLY ASSOCIATED WITH THIS PROJECT
<b>SUB TOTAL</b>			<b>\$206,599,888</b>
PROCEEDS FROM SALE OF COUNTY PROPERTY		APPRAISAL	(\$7,800,000) POTENTIAL FOR ALL COUNTY HILLTOP PARCELS TO BE SURPLUSED
<b>TOTAL PROJECT COST</b>			<b>\$198,799,888</b>

Notes:

1. Reduced program 10%, eliminated basement parking, reduced penthouse & LEED \$/SF, reduced courtroom & holding area premiums \$/SF

ITEM DESCRIPTION	UNITS	UNIT PRICE	COST NOTES
<b>CONSTRUCTION COST</b>			
SITE PREP & IMPROVEMENTS			<i>Edits to 12-5-18 ACKER ESTIMATE</i>
OFFSITE INFRASTRUCTURE			\$10,296,000 <i>Reduce to 900 spaces and reduce scope by 10%</i>
5 LEVEL COURTHOUSE BUILDING			\$3,200,000 <i>Reduce scope by 33%</i>
LOBBY WING			\$45,582,000 <i>See Note 1</i>
6 LEVEL ADMINISTRATION BUILDING			\$2,860,000 <i>Reduce scope by 10% + reduce cost/sf</i>
STRUCTURED PARKING			\$43,530,000 <i>Similar to Note 1</i>
SUB TOTAL			\$6,804,000 <i>3 LEVELS (1 less level)</i>
25% CONTRACTORS MARKUP	\$112,272,000	25%	<b>\$112,272,000</b>
SUB TOTAL			\$28,068,000 GEN REQ, OH&P, BOND & INS, B&O TAX, DESIGN CONT.
ESCALATION PREMIUM	\$140,340,000	12.0%	\$140,340,000 HARD CONSTRUCTION COSTS IN 2018 DOLLARS
<b>TOTAL CONSTRUCTION COST</b>			<b>\$16,840,800</b> CONSTRUCTION WORK START IN SPRING OF 2021
<b>OTHER PROJECT EXPENSES</b>			
FURNISHINGS (FF&E)	1	ALLOWANCE	\$6,100,000
WA STATE SALES TAX	\$157,180,800	8.90%	\$13,989,091
OWNER'S CONTINGENCY	\$157,180,800	5.00%	\$7,859,040 OWNER ONLY CONTINGENCY (NEW CONST)
CITY OF OLYMPIA PERMITTING & IMPACT FEES	1	ALLOWANCE	\$4,305,500 (EST.) SEE DETAILED LIST OF FEES (REPORT SECTION 11)
CONST. MANAGEMENT SERVICES	\$157,180,800	1.00%	\$1,571,808 SURVEY, GEOTECH, TESTING, COMMISSIONING, CRITERIA DOCS
MOVING EXPENSES	1	ALLOWANCE	\$200,000 400 STAFF X \$500/STAFF
A&E STIPENDS		ALLOWANCE	\$500,000 FOR DESIGN-BUILD FINALISTS (2 UNSUCCESSFUL FIRMS)
ARCHITECT & ENGINEERING	\$157,180,800	7.0%	\$11,002,656 BASIC SERVICES
ADDITIONAL CONSULTANT SERVICES	\$157,180,800	2.5%	\$3,929,520 CIVIL, COST EST., I.D., L.A., ACCOUST., ENVELOPE, LEED
DESIGN-BUILD MARKUP ON A&E FEES	\$14,932,176	15.0%	\$2,239,826 TAXES & OVERHEAD COSTS ON A&E FEES FOR DESIGN-BUILDER
PROPERTY ACQUISITION	1	APPRAISAL	\$4,258,800 FOR 17 ACRES FROM 11-27-18 K.M. APPRAISAL
OWNER FINANCING COSTS (EST. @ 1% OF TOTAL)	1	ALLOWANCE	\$2,700,000 BOND UNDERWRITING, LEGAL, BOND PLACEMENT FEES
OWNER INTERNAL MANAGEMENT EXPENSES	1	ALLOWANCE	\$500,000 INTERNAL COSTS DIRECTLY ASSOCIATED WITH THIS PROJECT
<b>SUB TOTAL</b>			<b>\$216,337,042</b>
PROCEEDS FROM SALE OF COUNTY PROPERTY		APPRAISAL	(\$7,800,000) POTENTIAL FOR ALL COUNTY HILLTOP PARCELS TO BE SURPLUSED
<b>TOTAL PROJECT COST</b>			<b>\$208,537,042</b>

Notes:

1. Reduced program 10%, eliminated basement parking, reduced penthouse & LEED \$/SF, reduced courtroom & holding area premiums \$/SF